

# NOTICE OF REQUIREMENT FOR ALTERATION TO A DESIGNATION

Waka Kotahi NZ Transport Agency  
Weigh Right Programme – (SH1) Rakaia

27 MAY 2022



# NOTICE OF REQUIREMENT FOR AN ALTERATION OF A DESIGNATION PURSUANT TO SECTION 181(3) OF THE RESOURCE MANAGEMENT ACT 1991

## FORM 18

To: Selwyn District Council  
PO Box 90  
Rolleston 7643

From: Waka Kotahi NZ Transport Agency  
PO Box 1479  
Christchurch 8140

Note: Different address for service.

Pursuant to Section 181(3) of the Resource Management Act 1991 (RMA) Waka Kotahi NZ Transport Agency (Waka Kotahi) gives notice of its requirement to alter a designation.

Waka Kotahi is a network utility operator approved as a requiring authority under Section 167 of the RMA. The relevant Gazette Notices are:

- Resource Management (Approval of NZ Transport Agency as a Requiring Authority) Order 1992 (NZ Gazette, Notice Number 1994-go1500) – and refer Schedule 2, Clause 29 of the Land Transport Management Act 2003 which confirms that the order applies to NZ Transport Agency
- Resource Management (Approval of NZ Transport Agency as a Requiring Authority) Notice 2015 (NZ Gazette, Notice Number 2015-go6742) – which confirms the NZ Transport Agency as a requiring authority for the purpose of constructing or operating (or proposing to construct or operate) and maintaining cycleways and shared paths.

### The designations to be altered:

The designations to be altered are Designation TR7 in the Operative Selwyn District Plan (Operative SDP), and corresponding Designation NZTA-7 Rakaia Commercial Vehicle Safety Centre (hereon 'NZTA-7') within the Proposed Selwyn District Plan (Proposed SDP).

### The nature of the alteration:

Waka Kotahi propose to undertake a minor alteration to the existing Designation TR7 in the Operative SDP. The proposed alteration seeks to remove the following condition from the designation:

*“General Accordance with details provided in the NOR and plans attached in D190066 Appendix C updated 17/02/2020; the Addendum to the NOR 17/02/2020; and the response to the request for further information 27/03/2020.”*

Additionally, Waka Kotahi proposed to undertake a minor alteration to the existing Designation NZTA-7 in the Proposed SDP. The proposed alteration seeks to remove the following condition from the designation:

### *“General Accordance*

1. *The designation shall be implemented in general accordance with the details provided with the Notice of Requirement:*
  - a. *The Plans attached in Appendix C of the Notice of Requirement dated 17/02/2020;*
  - b. *The Addendum to the Notice of Requirement dated 17/02/2020;*
  - c. *The response to the request to the further information dated 27/03/2020; and*
  - d. *The response to the request for further information dated 17/04/2020.”*

### **The site to which the requirement applies:**

The location of the site for which the proposed alteration to a designation relates to is shown on the plans attached as Appendix A of this Notice. This requirement applies to an area of land approximately 28,620m<sup>2</sup> or 2.86 hectares located at the junction of State Highway 1 and North Rakaia Road, Bankside. The requirement applies to the following land:

| Address                            | Legal Description   | Parcel Size                   | Landowner                                  |
|------------------------------------|---------------------|-------------------------------|--------------------------------------------|
| Junction SH1 and North Rakaia Road | Rural Section 33582 | 23,000m <sup>2</sup> / 2.3 ha | Waka Kotahi (New Zealand Transport Agency) |
| North Rakaia Road                  | Parcel ID 3576329   | N/A                           | Selwyn District Council                    |

### **The nature of the proposed public work:**

Waka Kotahi are proposing to undertake a minor alteration to existing Designation *TR7* in the Operative SDP, and corresponding Designation *NZTA-7* within the Proposed SDP.

The alteration to the designation conditions in both the Operative SDP and Proposed SDP will facilitate the proposed works associated within the construction, operation, and maintenance of a Commercial Vehicle Safety Centre (CVSC) at the site. The proposed CVSC is smaller in scale and intensity than that previously approved and shown in the plans lodged with the previous NOR and Outline Plan application referenced in the existing conditions of the designation.

The proposed works are described in Section 7 of the accompanying Assessment of Effects of the Environment (AEE) Report and Section 8 of the proposal description for the Outline Plan of Works.

In summary, the proposed works include:

- Construction of a CVSC to capture southbound commercial vehicles only.
- Construction of a Tier 1.5 Safety Building
- A CVSC with a significantly reduced size and scale compared with that approved in the Plans of the previous NOR application.

### **The nature of the proposed conditions that would apply are:**

The alteration to designation seeks to remove a condition on Designation *TR7* within the Operative SDP and Condition 1 within the Proposed SDP. The other conditions are proposed to remain the same for both designations. Section 4 of the accompanying Assessment of Environmental Effects (AEE) outlines the nature of the conditions which would apply to the site.

---

### **The effects of the proposal on the environment, and mitigation of adverse effects:**

The effects that removal of the general condition on Designation TR7 in the Operative SDP and Condition 1 of Designation NZTA-7 in the Proposed SDP will have on the environment, and the ways in which any adverse effects will be mitigated are provided in Section 6 of the AEE.

### **Alternative sites, routes, and methods:**

Alternative sites, routes, and methods have not been considered as part of this Notice for an alteration of the existing designation given the minor alternations are proposed to an existing designation.

### **The public work and alteration are reasonably necessary for achieving the objectives of the requiring authority because:**

The objective of Waka Kotahi under Section 94 of the Land Transport Management Act 2003 (LTMA) is to undertake its functions in a way that contributes to an effective, efficient and safe land transport system in the public interest.

The objectives of Waka Kotahi for the proposed CVSC are to:

- Effective enforcement for overweight and unsafe heavy vehicles on the road.
- Reduced wear and maintenance requirements for the road network.
- Fair market conditions for compliant heavy vehicle operators.
- Increased safety of other road network users.

The proposed alteration to the designation is necessary for achieving the objectives of Waka Kotahi because it will:

- Make the transport system safe.
- Keeps the land transport system well maintained to ensure a safer and more reliable network, helping to keep communities connected and supporting economic growth.

The proposed alteration of the designations is reasonably necessary as a planning tool, as it identifies and protects land required for the proposed work and will enable Waka Kotahi to construct, operate, and maintain a CVSC which differs from the original approved plans.

The principal reason for requiring a designation alteration to facilitate the work to which this requirement relates is:

- It will allow for the construction of a CVSC on the site which will provide increased safety for other road network users.

### **The following resource consents are needed for the proposed activity and have been applied for and granted:**

Canterbury Regional Council

- CRC202420 – Discharge Stormwater to Land
- CRC202421 – Discharge Greywater and Wastewater to Land
- CRC202422 – Earthworks over an Aquifer

Selwyn District Council

- RCD19005 – Notice of Requirement to Designate a site at the junction of State Highway 1 and North Rakaia Road, and Outline Plan for the construction of a CVSC

- 
- RC195641 – Construct and Operate a VMS outside of the State Highway 1 Designation TR1
  - RC195640 – Outline Plan Waiver to undertake minor works within the State Highway 1 Designation TR1
  - RC205208 – Outline Plan for the installation of infrastructure associated with a CVSC

**The following resource consents are being obtained for the proposed activity:**

Canterbury Regional Council

- S127 Variation to the conditions of CRC202420 and CRC202421

Selwyn District

- Resource consent for the construction and operation of a VMS outside of the State Highway 1 Designation.

**The following consultation has been undertaken:**

- Selwyn District Council - Transportation Asset Manager

**Supporting Information:**

Waka Kotahi attaches the following information in support of this Notice.

- An assessment of Environmental Effects on the Environment

Signed by:



Richard Shaw

**Team Lead South – Poutiaki Taiao | Environmental Planning  
Transport Services**

*Pursuant to authority delegated by Waka Kotahi NZ Transport Agency*

Address for Service:

WSP NZ Ltd  
12 Moorhouse Avenue  
Christchurch 8011

Attention: Samuel Hammond  
Phone: +64 3 367 0058  
E-mail: [Samuel.Hammond@wsp.com](mailto:Samuel.Hammond@wsp.com)

## Contents

|                                                                                                                               |    |
|-------------------------------------------------------------------------------------------------------------------------------|----|
| NOTICE OF REQUIREMENT FOR AN ALTERATION OF A DESIGNATION PURSUANT TO SECTION 181(3) OF THE RESOURCE MANAGEMENT ACT 1991 ..... | 2  |
| FORM 18 .....                                                                                                                 | 2  |
| 1 INTRODUCTION .....                                                                                                          | 9  |
| 1.1 Overview .....                                                                                                            | 9  |
| 1.2 Project Background .....                                                                                                  | 9  |
| 1.3 Project History .....                                                                                                     | 9  |
| 1.4 Purpose of Report .....                                                                                                   | 10 |
| 2 OTHER ACTIVITIES PERMITTED OR REQUIRING CONSENT OR REGULATION .....                                                         | 11 |
| 2.1 Overview .....                                                                                                            | 11 |
| 2.2 NESCS (2011) .....                                                                                                        | 11 |
| 2.3 Canterbury Land and Water Regional Plan .....                                                                             | 11 |
| 2.4 Operative Selwyn District Plan .....                                                                                      | 12 |
| 3 DESCRIPTION OF THE SITE AND SURROUNDING AREA .....                                                                          | 13 |
| 3.1 Site Location .....                                                                                                       | 13 |
| 3.2 Surrounding Environment .....                                                                                             | 14 |
| 3.3 Selwyn District Plan Zoning and Overlays .....                                                                            | 14 |
| 4 PROPOSED ALTERATION TO DESIGNATION .....                                                                                    | 16 |
| 4.1 Designation Condition Removal .....                                                                                       | 16 |
| 4.2 Need for Designation Condition Removal .....                                                                              | 16 |
| 4.3 Designation Purpose .....                                                                                                 | 17 |
| 5 CONSIDERATION OF ALTERNATIVES .....                                                                                         | 17 |
| 6 ASSESSMENT OF EFFECTS ON THE ENVIRONMENT .....                                                                              | 17 |
| 6.1 Positive Effects .....                                                                                                    | 17 |
| 6.2 Effects on Amenity and Character .....                                                                                    | 18 |
| 6.3 Cultural and Archaeological Effects .....                                                                                 | 19 |
| 6.4 Conclusion .....                                                                                                          | 20 |
| 7 CONSULTATION .....                                                                                                          | 20 |
| 7.1 Iwi .....                                                                                                                 | 20 |
| 7.2 Selwyn District Council .....                                                                                             | 20 |
| 7.3 Summary .....                                                                                                             | 20 |
| 8 STATUTORY ASSESSMENT .....                                                                                                  | 21 |
| 8.1 The Resource Management Act 1991 .....                                                                                    | 21 |

|             |                                                                           |           |
|-------------|---------------------------------------------------------------------------|-----------|
| <b>8.2</b>  | <b>Section 5: Purpose of the RMA .....</b>                                | <b>21</b> |
| <b>8.3</b>  | <b>RMA Part 8: Alteration to Designation.....</b>                         | <b>21</b> |
| 8.3.1       | Minor Change to Effects (s181(3)(a)(i)) .....                             | 22        |
| 8.3.2       | Minor Change to Boundaries (s181(3)(a)(ii)) .....                         | 22        |
| 8.3.3       | Notice of Proposed Alteration to Owners and Occupiers (s181(3)(b)) .....  | 22        |
| 8.3.4       | Territorial Authority and Requiring Authority to Agree (s181(3)(c)) ..... | 22        |
| <b>8.4</b>  | <b>Canterbury Regional Policy Statement .....</b>                         | <b>23</b> |
| <b>8.5</b>  | <b>Selwyn District Plan.....</b>                                          | <b>24</b> |
| <b>9</b>    | <b>PROPOSAL DESCRIPTION FOR OUTLINE PLAN.....</b>                         | <b>25</b> |
| <b>9.1</b>  | <b>Overview.....</b>                                                      | <b>25</b> |
| <b>9.2</b>  | <b>Commercial Vehicle Safety Centre .....</b>                             | <b>25</b> |
| <b>9.3</b>  | <b>Site of Works.....</b>                                                 | <b>25</b> |
| <b>9.4</b>  | <b>Operative Selwyn District Plan.....</b>                                | <b>26</b> |
| <b>10</b>   | <b>OUTLINE PLAN S176A ASSESSMENT .....</b>                                | <b>29</b> |
| <b>10.1</b> | <b>Height, Shape, and Bulk of Work .....</b>                              | <b>29</b> |
| 10.1.1      | Designation TR1 – State Highway 1.....                                    | 29        |
| 10.1.2      | Designation TR7 – North Rakaia Road .....                                 | 30        |
| <b>10.2</b> | <b>Likely Finished Contour of Site.....</b>                               | <b>32</b> |
| <b>10.3</b> | <b>Vehicular Access, Circulation, and Provision for Parking .....</b>     | <b>33</b> |
| 10.3.1      | Vehicular Access .....                                                    | 33        |
| 10.3.2      | Circulation.....                                                          | 33        |
| 10.3.3      | Parking.....                                                              | 34        |
| <b>10.4</b> | <b>Landscaping Proposed.....</b>                                          | <b>34</b> |
| 10.4.1      | Designation TR1 – State Highway 1.....                                    | 34        |
| 10.4.2      | Designation TR7 – North Rakaia Road .....                                 | 34        |
| <b>10.5</b> | <b>Any Other Matters .....</b>                                            | <b>34</b> |
| <b>11</b>   | <b>CONCLUSION.....</b>                                                    | <b>36</b> |
|             | <b>APPENDIX A .....</b>                                                   | <b>37</b> |
|             | <b>APPENDIX B .....</b>                                                   | <b>38</b> |
|             | <b>APPENDIX C .....</b>                                                   | <b>39</b> |
|             | <b>APPENDIX D .....</b>                                                   | <b>40</b> |
|             | <b>APPENDIX E .....</b>                                                   | <b>41</b> |
|             | <b>APPENDIX F .....</b>                                                   | <b>42</b> |
|             | <b>APPENDIX G .....</b>                                                   | <b>43</b> |
|             | <b>APPENDIX H .....</b>                                                   | <b>44</b> |



---

# 1 INTRODUCTION

## 1.1 Overview

Waka Kotahi is a Crown entity with its functions, powers and responsibilities set out in the Land Transport Management Act 2003 (LTMA) and the Government Rounding Powers Act 1989. Waka Kotahi is also a Requiring Authority under Section 167(3) of the Resource Management Act 1991 (the RMA).

The Waka Kotahi statutory objective as set under the LTMA is to undertake its functions in a way that contributes to an effective, efficient, and safe land transport system in the public interest.

An integrated approach to transport planning, funding, and delivery is taken by Waka Kotahi. This includes exhibiting a sense of cultural, social, and environment responsibility when undertaking its functions.

Waka Kotahi is proposing to alter existing designations to facilitate the construction, operation, and maintenance of a commercial vehicle safety centre (CVSC) within the boundary of Designation TR7 as shown in the Operative Selwyn District Plan (Operative SDP) and Designation NZTA-7 Rakaia Commercial Vehicle Safety Centre (hereon 'NZTA-7') as shown in the Proposed Selwyn District Plan (Proposed SDP).

## 1.2 Project Background

The Weigh Right Programme (the Programme) combines roadside technology and intelligent software developed to screen heavy vehicles and to direct potentially overweight vehicles into a CVSC for further inspection. The Programme supports Waka Kotahi – the NZ transport Agency 'Road to Zero' strategy which aims to create a road system free of death and serious injury. Waka Kotahi also seek to enable a productive land transport system which supports a level playing field for the heavy vehicle industry. The Weigh Right programme is one way that Waka Kotahi is delivering this.

Vehicle screening involves weigh-in-motion or in-road scales, automatic number plate recognition cameras and electronic signs all working together to identify potentially overweight vehicles. Weigh-in-motion sensors are installed within the road on the approach to CVSCs. The weigh-in-motion sensors and automatic number plate recognition (ANPR) cameras send information to a connected information technology system (ITS) to determine whether a vehicle is potentially overweight by either gross vehicle mass or by axle weight distribution. Information is then displayed on the variable messaging sign(s) (VMS). Vehicles identified as potentially overweight are directed to pull into the CVSC while compliant vehicles continue uninterrupted.

Expected benefits of the Weigh Right Programme are:

- Effective enforcement for overweight and unsafe heavy vehicles on the road.
- Reduced wear and maintenance requirements for the road network
- Fair market conditions for compliant heavy vehicle operators
- Increased safety of other road network users

## 1.3 Project History

Waka Kotahi previously proposed to construct and operate a CVSC at the junction of State Highway 1 and North Rakaia Road which would capture potentially overweight heavy vehicles travelling in both a

---

north bound and south bound direction. As part of this project, the following approvals were obtained in 2019/2020.

#### **Canterbury Regional Council**

- CRC202420 – Discharge Stormwater to Land
- CRC202421 – Discharge Greywater and wastewater to Land
- CRC202422 – Earthworks over an Aquifer

#### **Selwyn District Council**

- RC D19005 – Notice of Requirement to Designate the site at the junction of State Highway 1 and North Rakaia Road, and Outline Plan for the construction and operation of a CVSC
- RC195641 – Construct and Operate a VMS outside of the State Highway 1 Designation
- RC195640 – Outline Plan Waiver to undertake minor works within the State Highway 1 Designation
- RC205208 – Outline Plan for the installation of infrastructure associated with a CVSC

#### **Project Change**

Since obtaining the approvals listed above, Traffic Planning Consultants Ltd (TPC) were engaged by Waka Kotahi to carry out a road safety audit to identify safety concerns that may affect the safety and performance of the proposed commercial vehicle safety centre. The audit found that commercial vehicles crossing State Highway 1 posed an unacceptable safety risk. As a result, separate north and south bound CVSC sites are proposed which do not require commercial vehicles to cross over lanes of oncoming traffic.

Waka Kotahi now propose to reduce the size and scale of the previously approved development at the junction of State Highway 1 and North Rakaia Road. The development will now be used as a commercial vehicle safety centre for southbound vehicles only. A separate project will be undertaken in the future to provide a CVSC to capture northbound vehicles at the junction of State Highway 1 and Weavers Road. Waka Kotahi will prepare a separate Notice of Requirement, Outline Plan, and relevant resource consents for this project.

Therefore, for this CVSC southbound project, variations are required to the existing Canterbury Regional Council consents obtained as well as a Notice of Requirement (NOR) to alter the existing designation conditions within both the Operative and Proposed SDP's and an Outline Plan.

## **1.4 Purpose of Report**

This is a combined Notice of Requirement and Outline Plan application for the following:

- A minor alteration to a designation (Designation TR7) in the Operative SDP, and to designation (Designation NZTA-7) in the Proposed SDP pursuant to Section 181(3) of the Resource Management Act 1991 (RMA).
- An Outline Plan associated with works to construct, operate, and maintain and CVSC within Designation TR1 and Designation TR7 in the Operative Selwyn District plan under Section 176A of the Resource Management Act 1991 (RMA).

---

## 2 OTHER ACTIVITIES PERMITTED OR REQUIRING CONSENT OR REGULATION

### 2.1 Overview

The following RMA planning provisions have been considered in determining if additional resource consents are required:

- Resource Management (National Environmental Standard for Assessing and Managing Contaminants in Soil to Protect Human Health) Regulations 2011 (NЕСS)
- Canterbury Land and Water Regional Plan (Plan Change 7 Inclusive)
- The Operative Selwyn District Plan

### 2.2 NESCS (2011)

The National Environmental Standard for Assessing and Managing Contaminants in Soil to Protect Human Health (NESCS) 2011 is a national consistent set of planning controls and soil contaminant values. It ensures that land affected by contaminants in soil is appropriately identified and assessed before it is developed – and if necessary, the land is remediated, or the contaminants contained to make the land safe for human use. Territorial authorities are responsible for implementing the NESCS. It only applies if there is a risk to human health and does not require consideration of effects on the environment. The NESCS applies only to a ‘piece of land’ on which an activity that is listed on the Minister for the Environment’s (MfE) Hazardous Activities and Industries List (HAIL) is occurring currently or has been carried out historically.

The land contained within designation TR7 in the Operative SDP and within designation NZTA-7 in the Proposed SDP is not identified on Canterbury Regional Council’s Listed Land Use Register (LLUR) as being a HAIL site. Given the long-term use of the designation as a roading corridor and primary production farmland, as shown on historic aerial imagery on Canterbury Maps, it is unlikely that any HAIL activity has occurred onsite. Therefore, the NESCS does not apply.

### 2.3 Canterbury Land and Water Regional Plan

Waka Kotahi obtained three resource consents from Canterbury Regional Council in 2020 for the project. This related to the discharge of stormwater, discharge of grey water and wastewater, and earthworks over an aquifer. These consents were obtained specifically for this project and additional detail is provided below:

- CRC202420 – Discharge Stormwater to Land
- CRC202421 – Discharge Greywater and Wastewater to Land
- CRC202422 – Earthworks over an Aquifer

Following the safety audit as discussed in Section 1.3 of this report, Waka Kotahi are now proposing to reduce the size and scale of this CVSC. As a result, a Section 127 (Change of Conditions) application will be lodged with Canterbury Regional Council.

The Section 127 application will be a joint application for both CRC202420 and CRC202421 to change the general accordance conditions. This Section 127 application seeks to alter the conditions on the consent allowing stormwater to originate from site different than that referenced within the conditions of

---

consent, and greywater and stormwater to originate from a site different than that reference within the conditions of consent.

The Section 127 application will be lodged separate and concurrently with this NOR and Outline Plan application.

## **2.4 Operative Selwyn District Plan**

Waka Kotahi have obtained several outline plans, outline plan waivers, and resource consents under the Operative SDP for the project prior to the safety audit being undertaken. Details of these approvals is provided below:

- RC D19005 – Notice of Requirement to Designate the site at the junction of State Highway 1 and North Rakaia Road, and Outline Plan for the construction and operation of a CVSC
- RC195640 – Outline Plan Waiver to undertake minor works within the State Highway 1 Designation
- RC195641 – Construct and Operate a VMS outside of the State Highway 1 Designation
- RC205208 – Outline Plan for the installation of infrastructure associated with a CVSC

Following the safety audit, Waka Kotahi are now proposing to reduce the size and scale of the CVSC, and this project will provide a southbound CSCV facility. As a result, a VMS sign will be located outside of the State Highway 1 designation (Designation TR1) which does not comply with the standards of the Operative Selwyn District Plan. A separate resource consent application seeks to permit the construction and operation of the VMS.

In addition to the above resource consent application for a VMS outside of the State Highway 1 designation, this application is a joint NOR seeking to remove Condition 1 of Designation TR7 within the Operative SDP and Condition 1 of Designation NZTA-7 within the Proposed SDP, and an Outline Plan to allow the construction of a CVSC and associated operational features within Designation TR1 and TR7.

## 3 DESCRIPTION OF THE SITE AND SURROUNDING AREA

### 3.1 Site Location

Designations TR1 and TR7 and Designation NZTA-7 are located at the junction of State Highway 1 and North Rakaia Road, Bankside. The location of these designations are shown in Figure 1 and Figure 2 below.

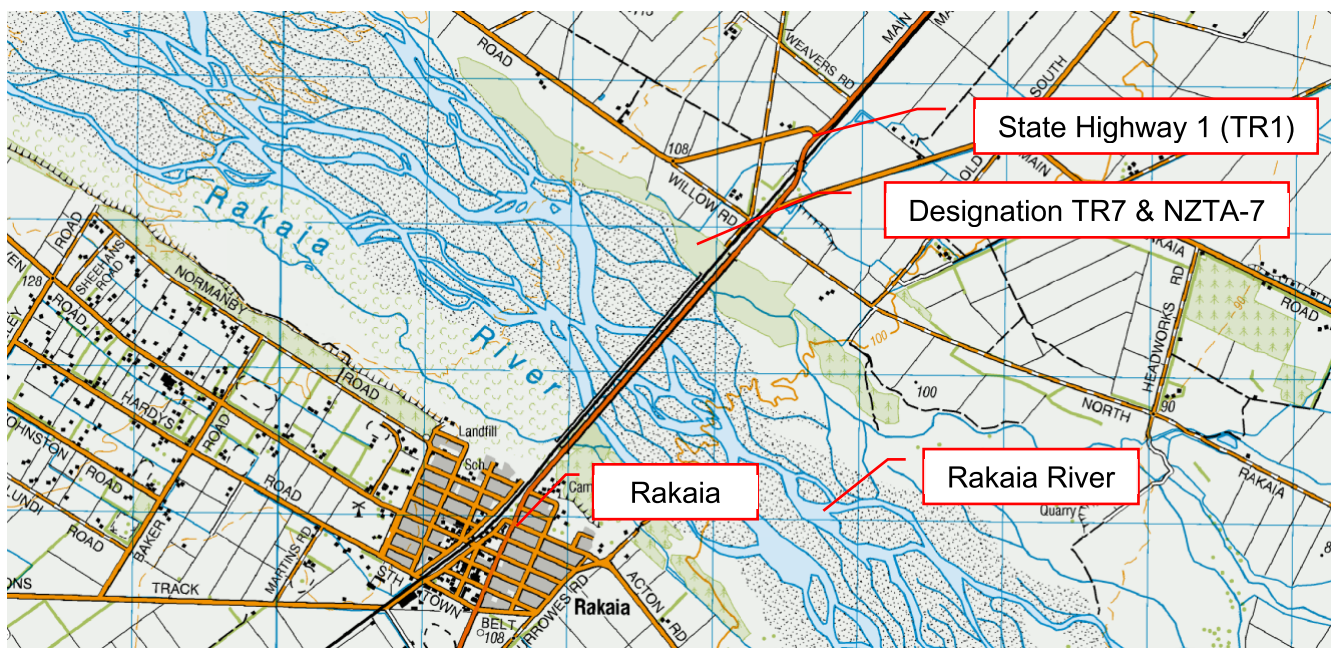


Figure 1: Location of Designation TR7 and Designation NZTA-7. (Source: NZ TopoMap)

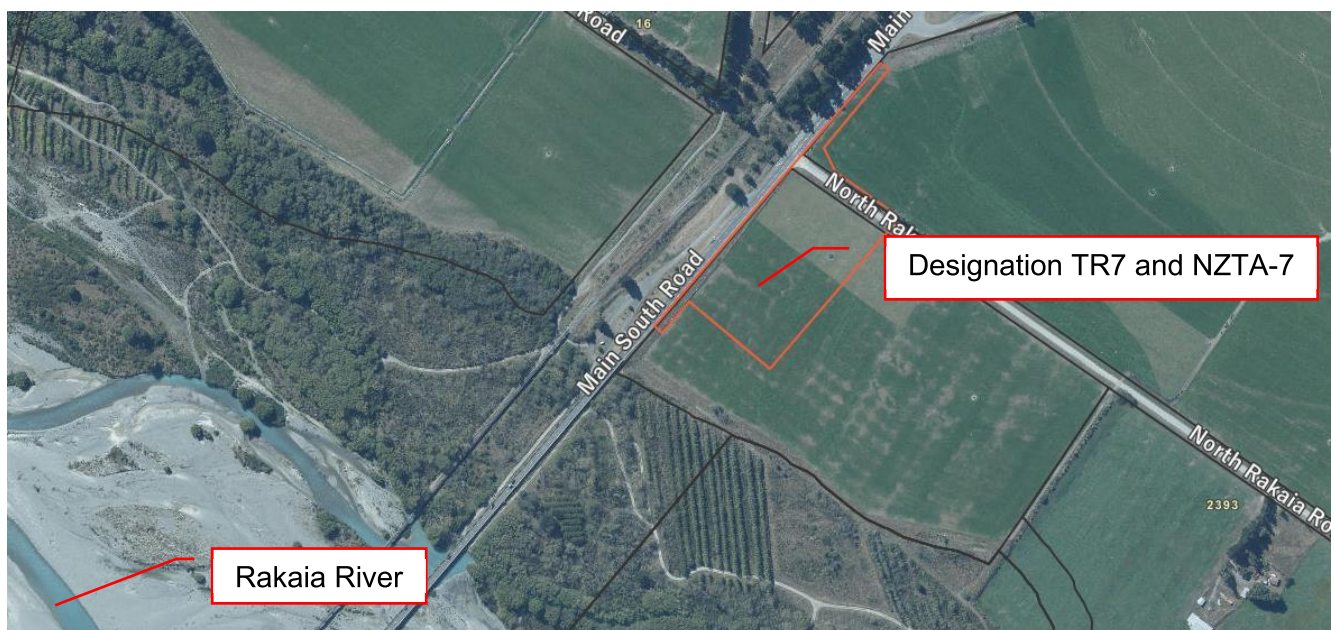


Figure 2: Aerial photo of Designation TR7 and Designation NZTA-7. (Source: Canterbury Maps)

## 3.2 Surrounding Environment

The surrounding environment is characterised by high intensity primary production farming to the north, east, and west. The Rakaia Township is located approximately 2.5km south of the site and is connected via bridge over the Rakaia River.

The topography of the site is flat and there are no surface water bodies in proximity to the site apart from the Rakaia River (0.4km approx.). No reticulated water, sewage, or stormwater services are provided onsite. State Highway 1 is adjacent to the site and is designated (TR1) within the Operative Selwyn District Plan.

## 3.3 Selwyn District Plan Zoning and Overlays

### Operative Selwyn District Plan

Designation TR7 is located on the corner of Main South Road and North Rakaia Road. The underlying zoning to the designation is the Outer Plains Zone. The designated site is also within the 'Lower Plains Flood Area'. No other planning overlays are present on or near the Designation TR7. Designation TR7 in the Operative SDP is shown in Figure 3 below.

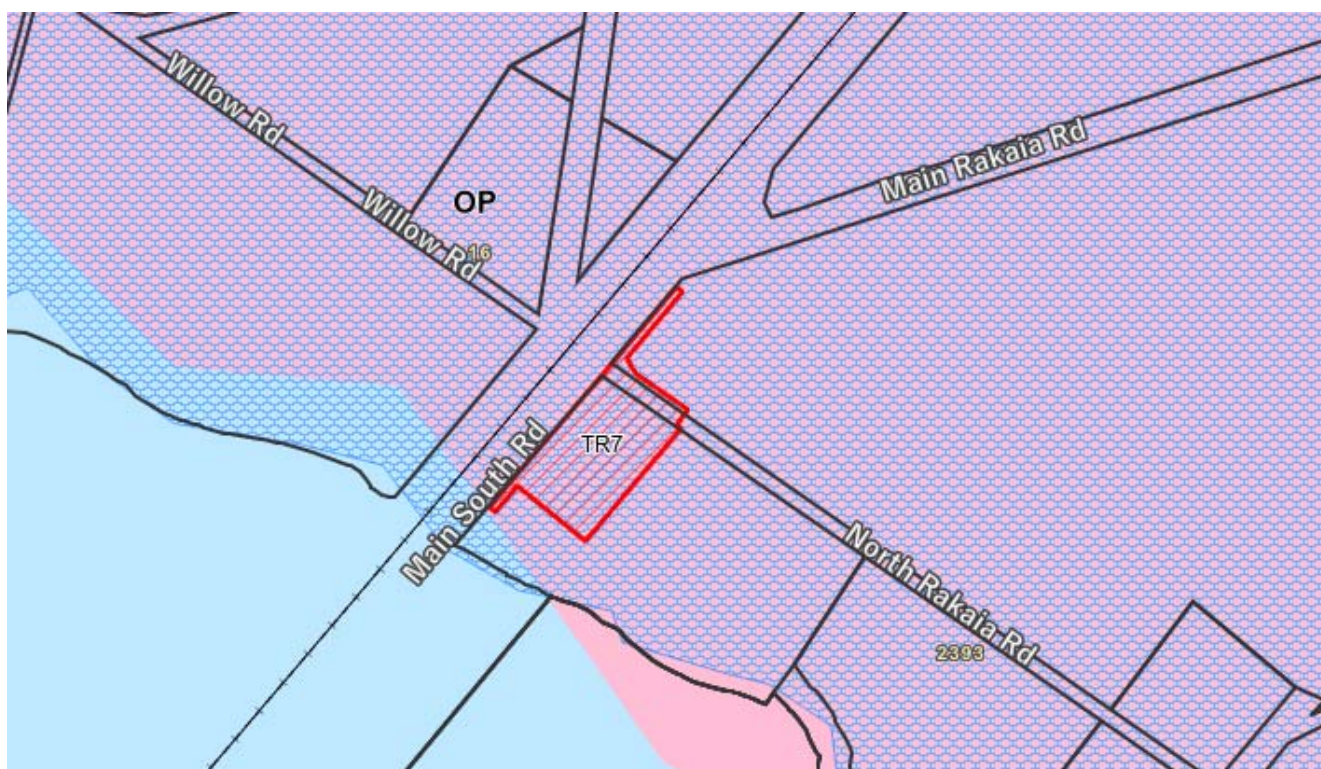


Figure 3: Designation TR7 as shown in the Operative SDP. (Source: Canterbury Maps).

Designation TR7 contains several conditions. These are outlined below:

*'Conditions including (but not limited to) the following:*

- *General Accordance with details provided in the NOR and plans attached in D190066 Appendix C updated 17/02/2020; the Addendum to the NOR 17/02/2020; and the response to the request for further information 27/03/2020;*

- 10yr Lapse;
- Prior to construction works being undertaken, engineering plans and specifications relating to the upgrade of the local road to be reviewed and approved by Council's Asset Manager, Transportation;
- Prior to commencement of operations, the operational management plan shall be provided to Council's Asset Manager, Transportation'.

### Proposed Selwyn District Plan

The Proposed SDP was publicly notified on 5 October 2020. SDC have been holding hearings on the Proposed SDP since August 2021 and the hearing regarding Designations were held in May 2022.

Designation NZTA-7 is located within the planning maps as shown in Figure 4 below. The underlying zoning is the General Rural Zone, and the site is also located within the Plains Flood Management Overlay. No other planning overlays are present within the boundary of Designation NZTA-7.



Figure 4: Designation NZTA-7 as shown in the Proposed SDP. (Source: SDC).

Designation NZTA-7 contains several conditions, and these are outlined below:

#### Conditions:

##### 'General Accordance

1. The designation shall be implanted in general accordance with the details provided within the Notice of Requirement:
  - a. The Plans attached in Appendix C to the Notice of Requirement updated 17/02/2020;
  - b. The Addendum to the Notice of Requirement dated 17/02/2020;
  - c. The response to the request for further information dated 27/03/2020; and
  - d. The response to the request for further information dated 17/04/2020.

##### Outline Plan

- 
2. Works undertaken in accordance with the NOR will not require an Outline Plan.

*Lapse Date*

3. The designation shall lapse on the expiry of 10 years from the date on which it is included in the District Plan if it has not been given effect to before the end of that period.

*Advice Note:*

*For the avoidance of doubt, none of these conditions prevent or apply to works required for the ongoing operation or maintenance of the Project following construction, such as routine maintenance or changes to signage over time. Depending upon the nature of such works, Outline Plans or Outline Plan waivers may be required for any such works’.*

## 4 PROPOSED ALTERATION TO DESIGNATION

### 4.1 Designation Condition Removal

Waka Kotahi seeks to remove the first general condition of Designation TR7 within the Operative SDP. The condition states:

*“General Accordance with details provided in the NOR and plans attached in D190066 Appendix C updated 17/02/2020; the Addendum to the NOR 17/02/2020; and the response to the request for further information 27/03/2020”.*

This Notice of Requirement seeks to remove this condition in its entirety.

Waka Kotahi seeks to remove Condition 1 of Designation NZTA-7 within the Proposed SDP. Condition 1 states:

*“General Accordance*

1. *The designation shall be implemented in general accordance with the details provided with the Notice of Requirement:*
  - a. *The Plans attached in Appendix C of the Notice of Requirement dated 17/02/2020;*
  - b. *The Addendum to the Notice of Requirement dated 17/02/2020;*
  - c. *The response to the request to the further information dated 27/03/2020; and*
  - d. *The response to the request for further information dated 17/04/2020.”*

### 4.2 Need for Designation Condition Removal

Waka Kotahi seek to remove the general condition of Designation TR7 to enable the construction, operation, and maintenance of a CVSC which is not in general accordance with the plans referenced above. As outlined in Section 1.3, the safety audit required the project to be altered from the approved plans. The project will now provide a southbound CVSC, which differs from the previously approved plans which provided a combined northbound and southbound CVSC. A combined northbound and southbound CVSC facility was not considered to be feasible. Hence, the requirement to remove the condition limiting the development to be in accordance with previously approved plans. This condition lacks flexibility and is impractical when changes to the approved plans need to be undertaken.

---

In conjunction with the removal of the general condition of Designation TR7 in the Operative SDP, the removal of Condition 1 of Designation NZTA-7 in the Proposed SDP is also proposed. This will enable onsite development on Designation NZTA-7 once the Proposed SDP becomes operative.

### 4.3 Designation Purpose

The purpose of Designation TR7 is to ‘*undertake maintenance, operation, use, and improvement to the state highway network*’. No change to the purpose of the designation is proposed. The existing designation purpose supports the development of a CVSC as discussed further within this report.

The removal of the first general condition of Designation TR7, will enable the construction, operation, and maintenance of a CVSC which is not in general accordance with the referenced plans to remain consistent with the purpose of the designation.

Likewise, the purpose of Designation NZTA-7 in the Proposed SDP is ‘*to undertake construction, maintenance, operation, use and improvement of the state highway network and associated infrastructure*’. As outlined above, the proposal is consistent with the purpose of the Designation NZTA-7.

## 5 CONSIDERATION OF ALTERNATIVES

Section 171(1)(b) of the RMA requires that a consideration of alternative sites, routes, or methods of undertaking the work is relevant in certain specific respects, including whether adequate consideration has been given to the alternative sites, routes or methods of undertaking the work (where a Requiring Authority does not have an interest in the land sufficient for undertaking the work, or it is likely that the work will have a significant adverse effect on the environment).

It is not considered that the proposal will result in significant environmental effects. Furthermore, it is considered that the requiring authority has sufficient interest in the land to undertake the works. As such, a consideration of alternative sites and/or methods is not considered to be required.

## 6 ASSESSMENT OF EFFECTS ON THE ENVIRONMENT

To satisfy Section 181(3)(a)(i) of the RMA, it is necessary to demonstrate that the:

*“alteration involves no more than a minor change to the effects on the environment associated with the use of proposed use of the land or any water concerned”*

The assessment of actual and potential effects on the environment from the proposed designation alteration is outlined below and summarised in the categories below:

- Positive Effects
- Effects on Amenity and Character
- Cultural and Archaeological Effects

### 6.1 Positive Effects

The construction, operation, and maintenance of the CVSC will contribute to the safe and efficient operation of the State Highway network through means of creating a productive land transport system that supports a level playing field in the heavy vehicle industry. In assessing the positive effects of

---

removing the general condition from Designation TR7 and Condition 1 from NZTA-7, the following are considered relevant.

The removal of the general condition in Designation TR7 and condition 1 of NZTA-7 will allow onsite development to occur which is not in general accordance with plans lodged with a previous NOR and Outline Plan application. The previous plans are no longer practical as a result of a safety audit, and an unacceptable safety risk which would have been created.

The removal of the general condition firstly allows the development to occur, and secondly, contributes to the effective enforcement of overweight and unsafe vehicles, reduced wear and maintenance on the road network, fair market conditions for compliant heavy vehicle operators, and increasing the safety of other road network users.

The removal of the general condition on Designation TR7 and condition 1 on NZTA-7 allows Waka Kotahi to meet their statutory objectives as outlined in the Land Transport Management Act 2003.

## **6.2 Effects on Amenity and Character**

### **6.2.1 Visual**

The removal of the general condition from TR7 and condition 1 from NZTA-7 will enable the development of a CVSC which is smaller in scale and intensity than that permitted by the existing conditions on the designation. A smaller CVSC is expected to have lesser adverse effects on existing amenity values and character of the rural area. On the basis that the proposed site is smaller in scale and intensity, adverse effects on visual amenity will be lesser than those already considered within the previous NOR application, and for the purpose of this application, insignificant.

No sensitive receivers are located within proximity to the proposed CVSC. The nearest residential dwelling is located approximately 280m north-west of the CVSC and is separated by the Main South Line (Railway) and State Highway 1. The next closest residential dwelling is located 650m south-east of the CVSC. Large vegetative planting provides good visual screening.

All sensitive visual receivers have good separation and features which provide visual screening. As above, the removal of the general condition from Designation TR7 and condition 1 from NZTA-7 to allow a site smaller in scale and intensity will create effects no greater than that already considered acceptable within the previous NOR. Therefore, any adverse effects on amenity and character are considered to be less than minor.

### **6.2.2 Noise**

Noise is expected to arise from construction activities and from the ongoing operation of the proposed CVSC. Construction noise is expected to be generated during the construction of buildings, through the operation of machinery such as excavators, compactors, loaders and the likes. Operational noise will result from the movement of commercial vehicles throughout the site when the CVSC becomes operational.

All onsite construction activities associated with the development of the CVSC is expected to occur in accordance with NZS6803:1999 *Acoustics – Construction Noise* to ensure acceptable construction noise effects. This will mean avoiding noisy construction activities at night, limiting construction to between the hours of 7:30am and 6pm during weekdays and Saturdays and no construction activity occurring on Sunday's and public holidays. This will also mean adopting the best practical option to

---

minimise construction noise where possible. Overall, any noise effects from construction related activities will be less than minor and of a temporary nature.

The CVSC will likely be operational for less than 7 days per month. The operation of the CVSC will result in the movement of heavy commercial vehicles into, around, and off the site. These vehicle movements are expected to result in minimal periodic operational noise. The overall effects of noise as a result of the operation of the CVSC will be lessened due to the absence of sensitive noise receivers in close proximity to the site. The closest dwelling is located approximately 270m north west of the proposed site. Given this large separation distance and the limited duration in which the CVSC will be in operation, any adverse effects noise effects as a result of the operation of the CVSC facility will be less than minor.

In addition to the separation distance provided between the site and the closest dwelling is State Highway 1 and the Main South Line (railway). It is likely that the existing ambient noise generated by the operation of State Highway 1 and Main South Line would likely exceed that of vehicles manoeuvring onsite with the operation of the CVSC. For these reasons, operational noise is not expected to create any adverse effects on the closest residential dwelling beyond that already present.

Overall, any noise effects from the operation of the CVSC will be less than minor.

## **6.3 Cultural and Archaeological Effects**

### **6.3.1 Cultural Effects**

The nearest culturally significant feature is the Rakaia River which is located approximately 300 – 400m south-west of Designation TR7 and NZTA-7. The Rakaia River is a Runanga Sensitive Area and is recognised for mahinga Kai (food gathering) and ara tawhito (tradition travel routes).

No effects above and beyond those already addressed within the previous NOR are anticipated from the removal of the general condition on Designation TR7 and condition 1 from NTZA-7.

### **6.3.2 Archaeological Effects**

There are no archaeologically significant sites in the vicinity of the application site. Additionally, a review of the New Zealand Archaeological Society's ArchSite carried out as part of the previous NOR application did not identify any archaeological sites in the vicinity. Therefore, any adverse effects on archaeological values are negligible.

### **6.3.3 Summary**

In managing risk, Waka Kotahi contractors will follow an Accidental Discovery Protocol. During works if any archaeological sites or remains are discovered, the works at that place of discovery will cease immediately. Heritage New Zealand, Mahaanui Kurataiao on behalf of Te Taumutu Runanga and Te Ngāi Tūāhuriri Rūnanga, and Selwyn District Council shall be contacted, and works shall only recommence in the affected areas when any necessary statutory authorisations or consents are obtained.

Adverse effects arising from the removal of the general condition from Designation TR7 and condition 1 from NZTA-7 are no greater than those already considered in the previous NOR application. Therefore, the effects on cultural values, and archaeological sites are expected to be less than minor.

---

## 6.4 Conclusion

No adverse effects above and beyond those already consented and considered acceptable and anticipated to arise from the removal of Condition 1 of Designation TR7 and NZTA-7. The proposed CVSC is smaller in scale and intensity and is therefore indicated to have lesser effects on the receiving environment than the previously approved site.

Adverse effects on the environment from the proposed alteration to a designation(s) is less than minor and satisfies Section 181(3)(a)(i) of the RMA.

## 7 CONSULTATION

### 7.1 Iwi

Waka Kotahi is currently undertaking consultation with Te Taumutu Rūnanga and Te Ngāi Tūāhuriri Rūnanga. Waka Kotahi will undertake this consultation at a hui scheduled for early June 2022.

Further details on the outcome of this consultation can be provided in June 2022 following the conclusion of the hui.

### 7.2 Selwyn District Council

In accordance with an existing condition of the designation (TR7 and NZTA7) and listed below, Waka Kotahi are required to obtain approval from Selwyn District Council's Asset Manager – Transportation, for the upgrade works to North Rakaia Road.

8. *'That prior to the undertaking of construction works, the engineering plans and specifications relating to the upgrades to the local road to be reviewed and approved by Council's Asset Manager, Transportation'.*

Waka Kotahi are in the process of obtaining engineering approval from Selwyn District Council for the upgrade works to North Rakaia Road. This approval is required as Selwyn District Council are the landowner of North Rakaia Road.

In addition to the above-mentioned approval, Waka Kotahi are required to provide Council's Asset Manager – Transportation with an operational management plan. This condition of the designation is shown below:

9. *'That prior to the commencement of operations, the operational management plan shall be provided to Council's Asset Manager, Transportation'.*

Note: A recommendation on this application for a Notice of Requirement and Outline Plan can be provided without engineering approval from Council's Asset Manager – Transportation. Engineering approval from Council's Asset Manager – Transportation is only required prior to the construction works being undertaken for the upgrade of North Rakaia Road to fulfil condition 8 of the designation.

### 7.3 Summary

Waka Kotahi are currently consulting with Iwi and Selwyn District Council's Asset Management – Transportation. The outcome of this can be provided, following the completion of the consultation.

No other parties are considered to be directly affected by the proposed alteration to designation.

---

## 8 STATUTORY ASSESSMENT

### 8.1 The Resource Management Act 1991

The RMA governs the use and development of New Zealand's natural and physical resources through:

- Part 2 (Purposes and Principles) which establishes the purposes and principles applying to resource management
- Section 171, which prescribes matters to be taken into account in confirming designations.

The following sections of the RMA are considered the most relevant to the project.

### 8.2 Section 5: Purpose of the RMA

The overriding purpose of the RMA is “to promote the sustainable management of natural and physical resources” (Section 5). The broader principles (Sections 6 to 8) are to inform the achieving of that purpose.

When considering an application, the consent authority, must subject to Part 2, have regard to those matters listed under Section 104 of the RMA.

With regards to the application of the subject to Part 2' under Section 104, case law findings have directed that decision makers / Commissioners may now only have recourse to Part 2 of the RMA if it is determined that one of three exceptions apply:

- If any part or the whole of the relevant plan(s) are invalid.
- If the relevant plan(s) did not provide complete coverage of the Part 2 matters.
- If there is uncertainty of the meaning of provisions as they affect Part 2.

In essence what this means is that decisions makers only need to 'go back to' Part 2 of the Act if the relevant planning documents have not fully addressed the Part 2 matters. If a Regional or District Plan has not fully addressed the Part 2 matters, then decision makers can 'go up the tree' to the RPS and then any relevant NPS in relation to any Part 2 matters.

Plans, which have to “give effect” to the higher order statutory planning documents (RPS and NPSs), should have appropriately addressed Part 2 of the RMA.

It is considered that none of the three exceptions listed above apply and that the Part 2 matters have adequately been addressed through the SDP. Based on the assessment of the proposal being consistent with the SDP as per below, the proposal is considered to be consistent with Part 2 of the RMA.

### 8.3 RMA Part 8: Alteration to Designation

Waka Kotahi is a Requiring Authority under section 166 of the RMA. Section 181(1) of the RMA provides that a Requiring Authority may give notice at any time to a territorial authority of its requirement to alter a designation or a designation requirement.

---

Under section 181(3), a territorial authority may at any time alter a designation in a district plan or a requirement for a designation in a proposed district plan, if:

*(a) The alteration-*

- (i) involves no more than a minor change to the effects on the environment associated with the use or proposed use of land or any water concerned; or*
- (ii) involves only minor changes or adjustments to the boundaries of the designation or requirement; and*

*(b) written notice of the proposed alteration has been given to every owner or occupier of the land directly affects and those owners or occupiers agree with the alteration; and*

*(c) both the territorial authority and the requiring authority agree with the alteration-and sections 168 to 179 and 198AA to 198AD shall not apply to any such alteration.*

If these criteria are satisfied, the alteration to designation may occur without further formality (ie on a non-notified basis).

The “effects” criteria provided for under s181(3)(a)(i) is one of two options available to the Requiring Authority. The other relates to a determination of the extent of change or adjustment to the “boundaries” of the designation under s181(3)(a)(ii). It is important to note it is not necessary to satisfy both the “effects” and the “boundaries” tests.

An assessment of the criteria under s181(3) is outlined below:

### **8.3.1 Minor Change to Effects (s181(3)(a)(i))**

In order to satisfy s.181(3)(a)(i), it is necessary to demonstrate the proposal will involve a no more than minor change to the effects on the environment associated with the use of the land.

Section 6 of this report outlines that any effects of the proposed alteration to designation with the removal of conditions will be less than minor. There will be no change in the level of effects currently associated with the existing designations.

It is considered the proposal meets the ‘change in effects’ test of s.181(3)(a)(i).

### **8.3.2 Minor Change to Boundaries (s181(3)(a)(ii))**

In order to satisfy s.181(3)(a)(ii), it is necessary to demonstrate the proposal involves no more than a minor change or adjustment to the boundaries of the designation or requirement. No changes are proposed to be undertaken to the boundaries of the existing designations TR7 in the Operative SDP and NZTA-7 in the Proposed District Plan.

### **8.3.3 Notice of Proposed Alteration to Owners and Occupiers (s181(3)(b))**

Waka Kotahi own and occupy all the land for the proposed development. Therefore, there are no directly affected owners or occupiers affected by the proposed alteration to designation.

### **8.3.4 Territorial Authority and Requiring Authority to Agree (s181(3)(c))**

As the Requiring Authority, Waka Kotahi agrees to this alteration, thereby partially satisfying s.181(3)(c) of the RMA. Selwyn District Council has discretion on this matter.

---

It is considered the alteration to designation satisfies the criteria set out in Section 181(3) of the RMA and can proceed on the basis as a minor alteration to designation and be processed non-notified.

Waka Kotahi therefore seeks that SDC agrees with the alteration to Designation TR7 in the Operative SDP and Designation NZTA-7 in the Proposed SDP through the s.181 notice process.

## **8.4 Canterbury Regional Policy Statement**

The Canterbury Regional Policy Statement 2013 (RPS) (revised February 2017) provides an overview of the resource management issues of the region. It sets out how natural and physical resources are to be managed in an integrated way to promote sustainable management.

The provisions contained in Sections 5 - 8 of the RMA provide the framework for the objectives and policies of the RPS. The RPS in turn provides the framework for the issues, objectives, policies and methods of the underlying Regional Plans. The Regional Plans have to be consistent with the RPS.

The following objectives and policies contained within Chapter 5 of the Canterbury Regional Policy Statement are considered relevant to the proposal and have been assessed below.

- Objective 5.2.3: Transport Network (Wider Region)
- Policy 5.3.1: Regional Growth (Wider Region)
- Policy 5.3.2: Development Conditions (Wider Region)
- Policy 5.3.3: Management of Development (Wider Region)
- Policy 5.3.7: Strategic Land Transport Network and Arterial Roads (Entire Region)
- Policy 5.3.8: Land Use and Transport Integration (Wider Region)

The proposal is consistent and not contrary to the above objectives and policies contained within the Canterbury Regional Policy Statement. The proposal is specifically consistent with Objective 5.2.3 and Policies 5.3.7 and 5.3.8 as they relate to transportation networks and transportation land uses.

In addition to Chapter 5, Chapter 11 is considered relevant to the proposal. Chapter 11 relates to natural hazards, with the relevant objectives being assessed below.

- Objective 11.2.2: Adverse Effects from Hazard Mitigation are avoided or mitigated
- Objective 11.2.3: Climate Change and Natural Hazards
- Policy 11.3.7: Physical Mitigation Works
- Policy 11.3.8: Climate Change

The proposal has taken into account the flood hazard at the site which has potential to cause adverse effects on built structures at the CVSC. The Tier 1.5 Compliance Building has been raised in accordance with a flood hazard report provided by Canterbury Regional Council. This report is attached as Appendix F of this application. The application is therefore considered consistent with the objectives and policies as they relate to natural hazards.

In addition, the proposal has considered the actual and potential adverse effects of climate change. The stormwater design onsite allows for increases in rainfall as a result of climate change.

Overall, the proposal is consistent with the objectives and policies of the Canterbury Regional Policy Statement.

## 8.5 Selwyn District Plan

The Operative and Proposed Selwyn District Plans contain objectives and policies which relate to the proposed activity. The following objectives and policies have been recognised as relevant to the proposal.

### Operative Selwyn District Plan

Objective B2.1.2: An integrated approach to land use and transport planning to manage and minimise adverse effects of transport networks on adjoining land uses, and to avoid “reverse sensitivity” effects on the operation of transport networks.

Policy B2.1.3: Recognise and protect the primary function of roads classified as State Highways or Arterial Roads in Appendix 9, to ensure the safe and efficient flow of through traffic en route to its destination.

Objective B3.1.2: Measures to mitigate natural hazards do not cause or exacerbate adverse effects on the environment.

Policy B3.1.3: Ensure dwellings or other principal buildings located in the flood area shown on the Planning Maps have minimum floor heights 300mm (freeboard) above the projected flood level for a 2% AEP (Annual Exceedance Probability) flood event so as to minimise the risk of inundation.

The proposal provides appropriate methods of mitigating natural hazard risk and adverse effects on the environment. Additionally, the CVSC contributes to the safe operation of the roading network. The proposal is consistent with the relevant objectives and policies of the Operative Selwyn District Plan.

### Proposed Selwyn District Plan

TRAN-01: People and places are connected through safe, efficient, and convenient land transport corridors and land transport infrastructure which is well integrated with land use activities and subdivision development.

TRAN-03: Land transport corridors and land transport infrastructure support the needs of people and freight, while ensuring adverse effects on the surrounding environment from their establishment and operation are managed.

TRAN-P1: The safety and efficiency of the district’s land transport network and systems are enabled through integrated land use and subdivision development [...]

TRAN-P7: Recognise and protect the function of the district's land transport network and systems by managing land use activities and subdivision development to ensure the safe and efficient movement of people and goods [...]

NH-O1: New subdivision, use, and development, other than new important infrastructure and land transport infrastructure [...]

NH-O2: Important infrastructure and land transport infrastructure is only located within areas of significant natural hazard risk where there is no reasonable alternative and the important infrastructure or land transport infrastructure is designed so as not to exacerbate natural hazard risk to people and property.

NH-O3: Methods to mitigate natural hazards do not create or exacerbate adverse effects on other people, property, infrastructure, or the environment.

NH-P2: Avoid the development or use of land, buildings or structures in high hazard areas for any important infrastructure or land transport infrastructure, unless the activity [...]

NH-P10: In areas within the Plains Flood Management Overlay that are not a high hazard area, provide for any new subdivision, use, and development (other than important infrastructure and land transport infrastructure) only where every new residential unit or principal building has an appropriate floor level above the 200-year Average Return Interval (ARI) design flood level.

The proposal is consistent with the above objectives and policies of the Proposed Selwyn District Plan. The proposal contributes to the safe and efficient movement of people and freight within the district while ensuring that adverse effects are appropriately managed. The proposal also has appropriate measures of mitigating the actual and potential effects of flood hazard.

Overall, the proposal is consistent and not contrary to the objectives and policies of the Proposed Selwyn District Plan.

## 9 PROPOSAL DESCRIPTION FOR OUTLINE PLAN

### 9.1 Overview

Waka Kotahi seek to construct, operate, and maintain a CVSC on Designation TR7 which has a reduced size and scale to that previously approved. Further details on the CVSC are provided below.

### 9.2 Commercial Vehicle Safety Centre

Works within Designation TR1 and TR7 for the construction and operation of a commercial vehicle safety centre (CVSC) are proposed as part of the Waka Kotahi national Weigh Right Programme. Appendix A contains plans which provide a general overview of the entire Rakaia CVSC.

The proposed CVSC north of the Rakaia River within the Selwyn District will capture a significant portion of heavy vehicles travelling south out of Christchurch along State Highway 1. The CVSV will contribute to the operation of an effective, efficient, and safe land transport system.

### 9.3 Site of Works

The works are proposed to occur within the boundaries of Designation TR1 and TR7 as listed within the Operative Selwyn District Plan. Different aspects of the proposal are scattered over a 1.5km length of State Highway 1 (TR1), with the CVSC concentrated in the North Rakaia Road site (TR7). A separate resource consent application for a variable message sign (VMS) located outside of Designation TR1 will be lodged with Council concurrent to this application. The general location of each aspect of the proposal are shown in Figure 5 below.

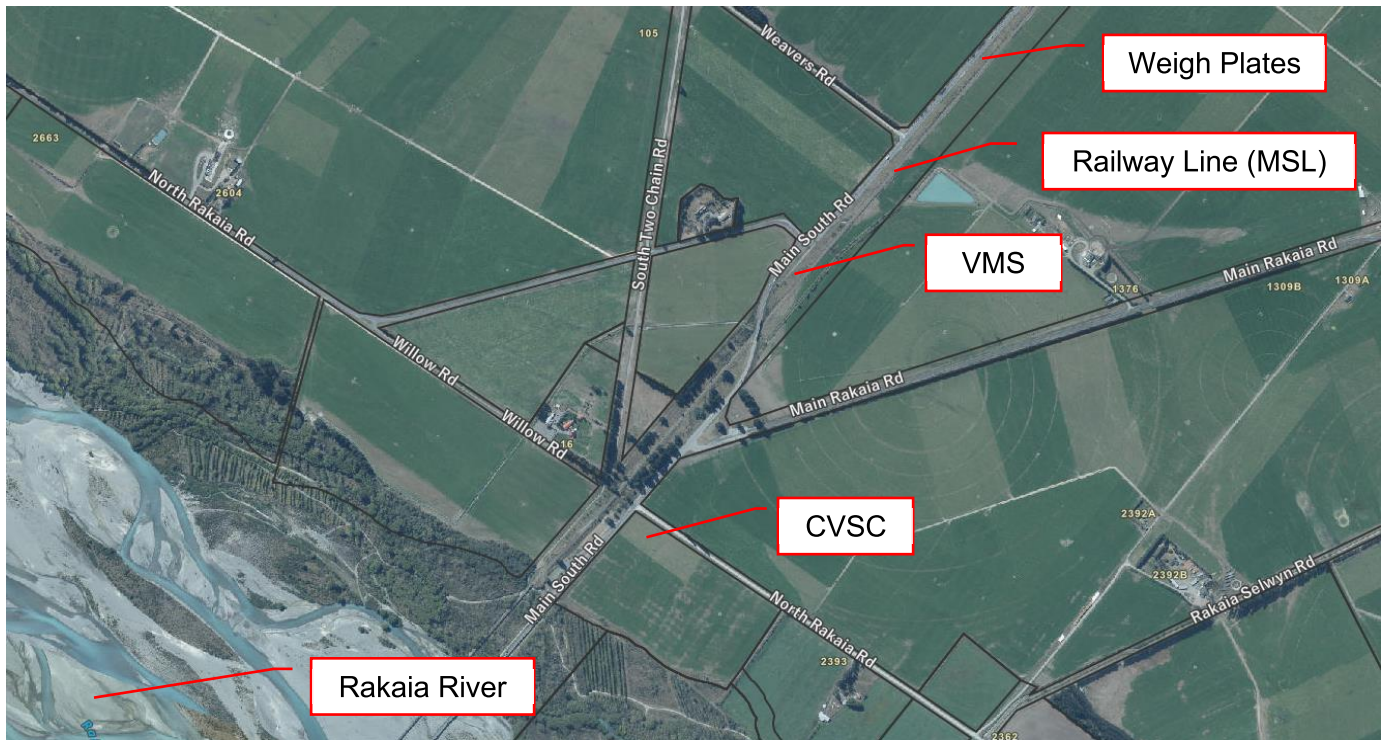


Figure 5: Location of works. (Source: Canterbury Maps)

## 9.4 Operative Selwyn District Plan

Works will be carried out in Designation TR1 and TR7. Both designations are listed within the Operative Selwyn District Plan and discussed further below.

### 9.4.1.1 Designation TR1

Designation TR1 as listed in the Operative Selwyn District Plan is shown in Figure 6 below.

| Des No. | Map Ref | Site Name                       | Designation Purpose | Location                                                                                                                                                                                                                                                                                                                                                                                                                                                                       | Zone    | Legal Description and Gazette (where known) | Comments/ Conditions |
|---------|---------|---------------------------------|---------------------|--------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|---------|---------------------------------------------|----------------------|
| TR1     | Various | State Highways 1, 73, 75 and 77 | State Highway       | <p>SH1: From the intersection of SH1 with the centerline of the Rakaia River. Note: there is a short section of SH1 south of Marshs Road that is within Christchurch City.</p> <p>Note: For that part of TR1 affected by the CSM2 and MSRFL proposal, separate conditions apply. The areas where these apply have been noted in the District Plan as either TR5 or TR6 (see below).</p> <p>SH73: From the Dawsons Road / SH 73 intersection to the summit of Arthurs Pass.</p> | Various | Various                                     | Various              |

*Figure 6: Designation TR1 as shown in the Operative Selwyn District Plan. (Source: Selwyn District Plan)*

As shown in Figure 6 above, TR1 is designated for State Highway purposes. The designation has full effect and overrides any rules in the Selwyn District Plan. There are no conditions on the designation that apply to this section of State Highway 1 (TR1).

Designation TR1 covers a significant portion of State Highway 1 within the Selwyn District. For the purpose of this application, the applicable section of Designation TR1 is shown in Figure 7 below.

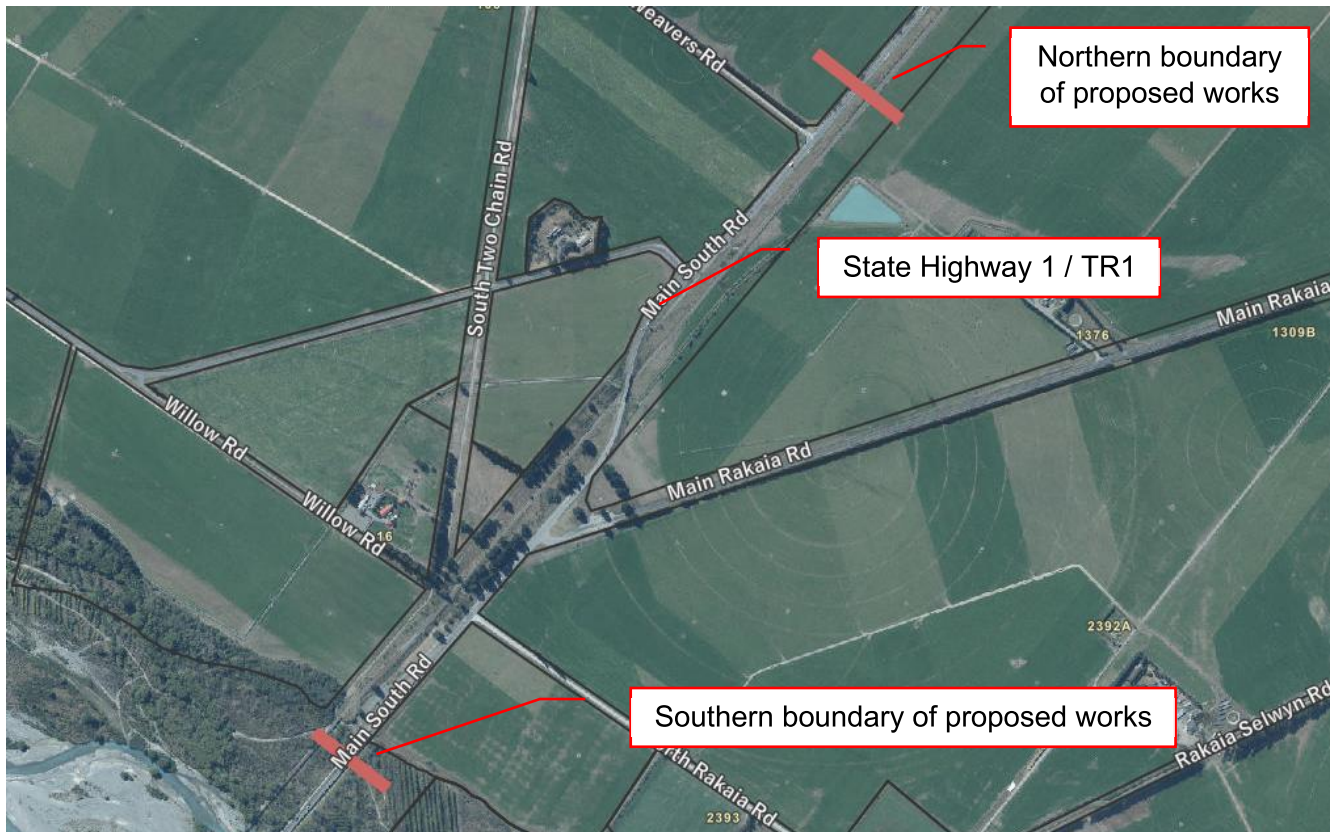


Figure 7: Applicable section of Designation TR1 which applies to this application. (Source: Canterbury Maps).

The purpose of the designation is for 'State Highway' and the proposal to construct and operate a CVSC is therefore in accordance with the designated State Highway purpose of the site.

#### 9.4.1.2 Designation TR7

Designation TR7 is listed within the Operative Selwyn District Plan and is shown in Figure 8 below.

|     |         |                   |                                                                                     |                   |              |                                                                                                                                                     |                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                 |
|-----|---------|-------------------|-------------------------------------------------------------------------------------|-------------------|--------------|-----------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| TR7 | Various | North Rakaia Road | Undertake maintenance, operation, use, and improvement to the state highway network | North Rakaia Road | Outer Plains | Rural Section 33582 (23,000m <sup>2</sup> ); Rural Section 33583 (1660M <sup>2</sup> ); Rural Section 34454 (240m <sup>2</sup> ); Parcel ID 3576329 | <p><b>Conditions including (but not limited to) the following:</b></p> <p>General Accordance with details provided in the NOR and plans attached in D190066 Appendix C updated 17/02/2020; the Addendum to the NOR 17/02/2020; and the response to the request for further information 27/03/2020;</p> <p>10yr Lapse;</p> <p>Prior to construction works being undertaken, engineering plans and specifications relating to the upgrade of the local road to be reviewed and approved by Council's Asset Manager, Transportation;</p> <p>Prior to commencement of operations, the operational management plan shall be provided to Council's Asset Manager, Transportation.</p> |
|-----|---------|-------------------|-------------------------------------------------------------------------------------|-------------------|--------------|-----------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|

Figure 8: Designation TR7 as shown in the Operative Selwyn District Plan. (Source: Selwyn District Plan)

As shown above, the site is designated for the purpose of ‘undertaking maintenance, operation, use and improvement to the State Highway network’.

The site where the CVSC is to be located is at the junction of State Highway 1 and North Rakaia Road. The site is legally described as RS 33582 contained within Record of Title CB20B/482. This land parcel has an area of 11.15 hectares. The site is currently vacant and used for primary production purposes by Rakaia Dairy Limited and has recently been purchased by Waka Kotahi. A copy of the purchase agreement has been included as Appendix H.

As shown in Section 3.3 of this application, Designation TR7 is located within the Outer Plains Zone and is subject to the Lower Plains Flood Area overlay. No other overlays or management zones apply to the site as shown in the Selwyn District Council Planning Maps.

## 10 OUTLINE PLAN S176A ASSESSMENT

Section 176A of the Resource Management Act 1991 (RMA) requires an Outline Plan of works to be constructed on designated land to be submitted to a territorial authority. This provides opportunity for the territorial authority to request changes before construction commences.

Section 176A(3) of the RMA states:

*“An outline plan must show –*

- (a) The height, shape, and bulk of the public works, project, or work; and*
- (b) The location on the site of the public works, project, or work; and*
- (c) The likely finished contour of the site; and*
- (d) The vehicular access, circulation, and the provision for parking; and*
- (e) The landscaping proposed; and*
- (f) Any other matters to avoid, remedy, or mitigate any adverse effects on the environment.”*

A territorial authority does not have the discretion to approve or decline a development under an Outline Plan. Under Section 176A(4) of the RMA, the sole discretion of a territorial authority is to request changes to an Outline Plan within 20 working days of its receipt.

Assessment under Section 176A(3) of the RMA is discussed under the relevant headings below.

### 10.1 Height, Shape, and Bulk of Work

#### 10.1.1 Designation TR1 – State Highway 1

Works within designation TR1 are shown on Plans attached as Appendix A. Works include the following:

- Automatic Number Plate Reading (ANPR) Camera
- Weigh-In-Motion (WIN) Plates
- ITS Cabinets
- Metal Side Guard
- Road Resurfacing

Plan showing the location and scale of the above listed features are contained within Appendix A.

### 10.1.2 Designation TR7 – North Rakaia Road

The general layout of the proposed CVSC to be located within Designation TR7 is shown in Figure 9 below and attached within Appendix A of this application.

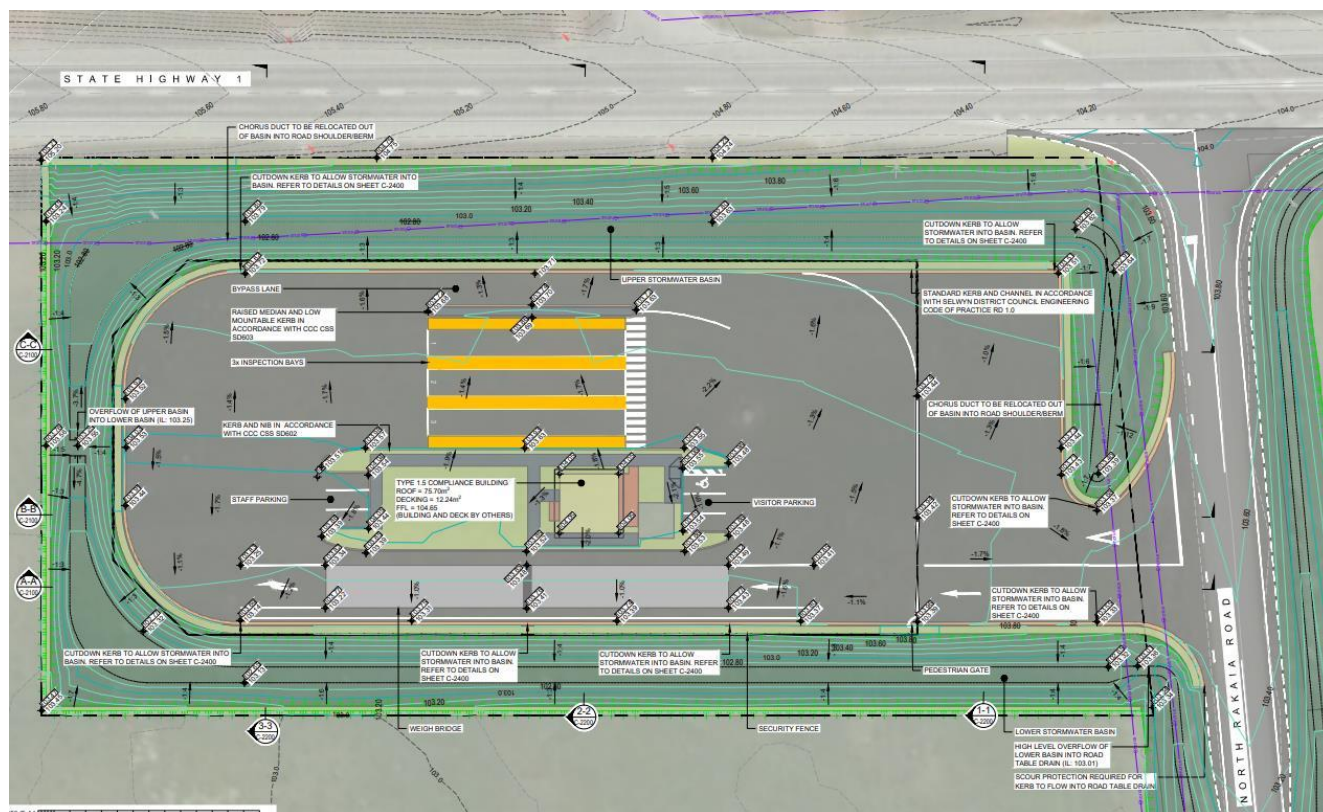


Figure 9: Proposed general layout of the CVSC to be located in Designation TR7. (Source: WSP)

The proposed CVSC when complete will comprise of:

- A weigh bridge
- Tier 1.5 compliance building
- Bypass lane
- Inspection bays
- Staff and visitor parking
- Lighting
- Onsite greywater and wastewater disposal
- Onsite stormwater disposal
- Site fencing

#### 10.1.2.1 Weigh Bridge

The onsite weigh bridge comprises of two 5m wide by 25m long weigh plates. These plates are located on the east of the CVSC site and will require all heavy vehicles entering the site to pass over the plates. The weigh bridge will be integrated into the site at ground level.

### 10.1.2.2 Tier 1.5 Compliance Building

The tier 1.5 compliance building is a 7.5m wide by 7.5m long (75m<sup>2</sup> in area) building located adjacent to the weigh bridge on the CVSC site. The building will be situated on wooden piles 600mm above ground level, giving the building a total approximate height of 4.5m.

The building contains an observation area, two information and communication technology (ICT) rooms, interview room, kitchenette, and toilet facilities. The internal layout of the building is shown in Figure 10 below.

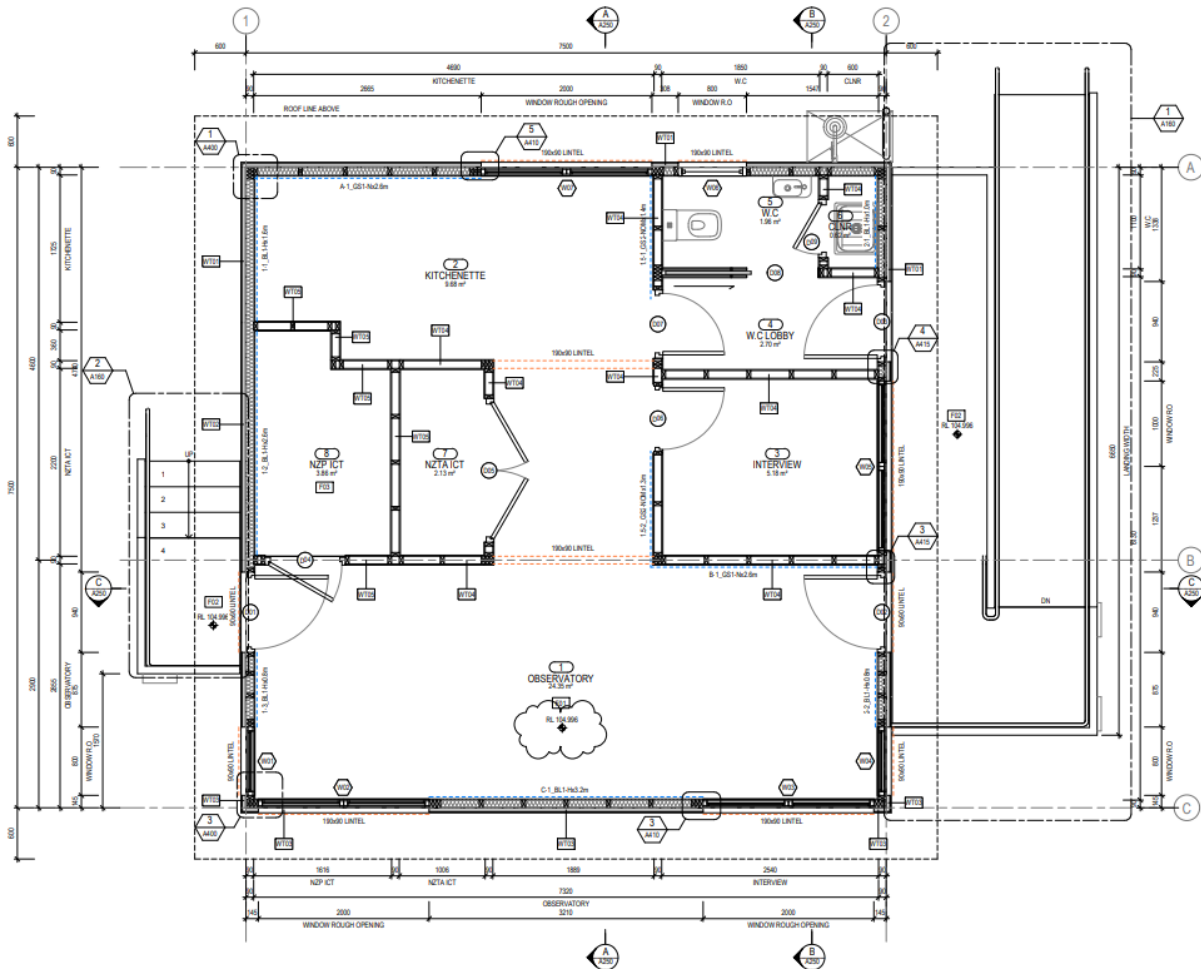


Figure 10: Internal layout of Tier 1.5 Compliance Building. (Source: GHD Woodhead)

Access to the building is obtained either by a wooden ramp on the buildings north-east elevation, or wooden steps on the south-west elevation. The exterior of the building is characterised by dark weatherboard. A complete set of building plans for the Tier 1.5 Compliance Building have been attached as Appendix B of this application.

### 10.1.2.3 Bypass Lane

A bypass lane is provided for heavy vehicles which are either weight compliant or do not require inspection. This bypass lane is located on the north-west portion of the site and runs adjacent to State Highway 1. The bypass lane is approximately 4m in width and is separated from the inspection bays by a raised median and low mountable kerb.

---

#### *10.1.2.4 Inspection Bays*

Three inspection bays will be provided on the CVSC site. Heavy vehicles will be directed to these if they are either overweight or require further inspection. Each bay is approximately 3m in width and 25m in length. The inspection bays are adjacent to the bypass lane and are separated by a raised median and low mountable kerb.

#### *10.1.2.5 Staff and Visitor Parking*

Six car parks will be provided onsite. These are located at the front and rear of the Tier 1.5 Compliance Building.

#### *10.1.2.6 Lighting*

Onsite lighting has been designed to facilitate the safe and efficient movement of vehicles entering, exiting, and manoeuvring on the CVSC. Onsite lighting will be mounted on poles and will be pointed towards the ground (0-degree angle). Lighting which is angled directly at the ground will ensure that light spill onto surrounding properties and the state highway network is minimised. A lighting plan is attached to this application as Appendix G.

#### *10.1.2.7 Onsite Services*

No reticulated water or wastewater services are available onsite.

In lieu of this, the Tier 1.5 Compliance Building will obtain drinking water from a roof water collection tank located on a concrete pad to the north-east.

Wastewater generated from the Tier 1.5 Compliance Building will be disposed of onsite via a secondary aerated treatment system discharge to a sand trench land application system.

Stormwater generated onsite will be disposed of into onsite stormwater basins. These stormwater basins form the perimeter of the CVSC and are shown on Plan C-2010 within Appendix C of this application.

The discharge of wastewater to land, and discharge of stormwater to land are permitted under previously obtained resource consent from Canterbury Regional Council.

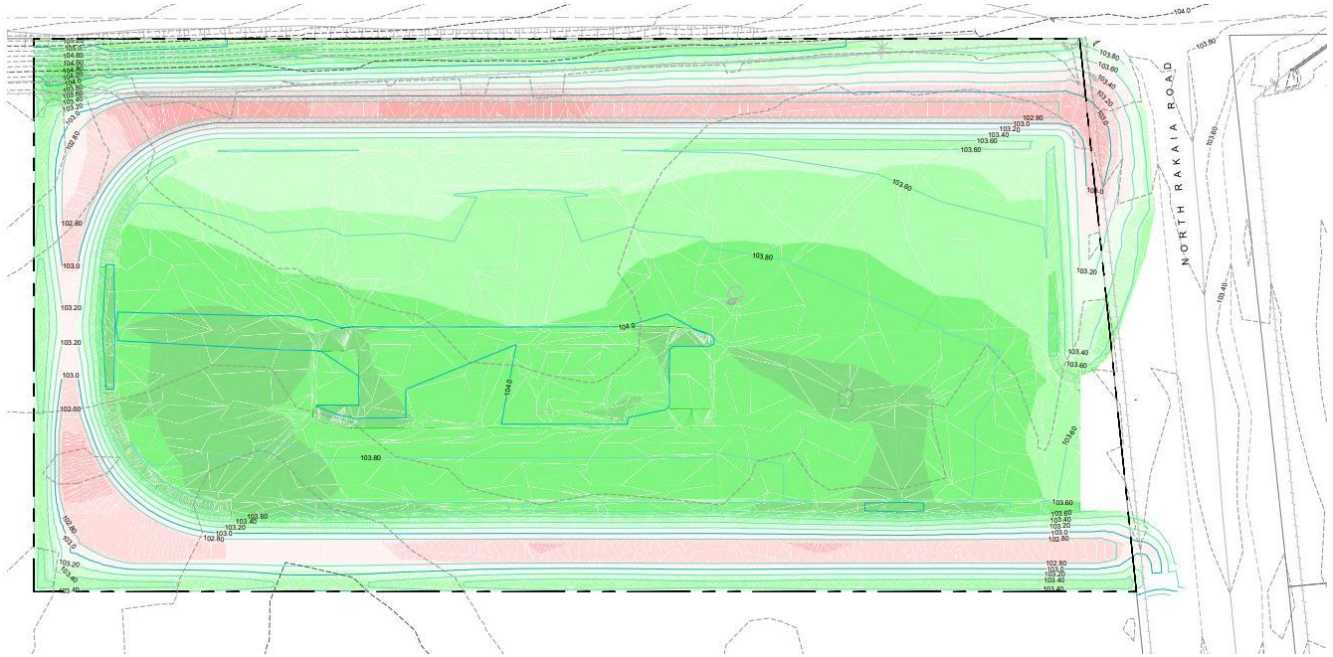
#### *10.1.2.8 Site Fencing*

Security fencing will be installed around the perimeter of the CVSC site. This will be located between the edge of the hardstand area and stormwater basins. An electric gate will close the site off from North Rakaia Road when the CVSC is not operating.

### **10.2 Likely Finished Contour of Site**

The entire site will be stripped of topsoil before works commence onsite. This is expected to result in approximately 3,000m<sup>3</sup> of topsoil to be stripped to a depth of 300mm below the existing ground level. It is assumed that 1,600m<sup>3</sup> of topsoil will be retained onsite for reuse. This will result in approximately 1,400m<sup>3</sup> of topsoil to be cut to waste.

Following the construction of the CVSC, the hardstand area will have a 1.1% to 2.2% slope to direct stormwater to the stormwater swales and basins which form the perimeter of the site. The proposed finish contour of the site is shown in Figure 11 below and shown in plan C-2010 attached as Appendix D of this application.



### 10.3 Vehicular Access, Circulation, and Provision for Parking

Vehicle access upgrade works are proposed to occur in Designation TR7 (North Rakaia Road) to support the construction, operation, and maintenance of the CVS. These works will only occur in Designation TR7.

No upgrade works associated with vehicular access, circulation, or provisions for parking will occur in Designation TR1 (State Highway 1).

### 10.3.1 Vehicular Access

As part of the development of the CVSC, North Rakaia Road will be upgraded. Upgrade works involve widening and sealing North Rakaia Road approximately 90m off State Highway 1. Permanent asphalt will be overlayed the existing gravel road surface.

Proposed upgrade works to North Rakaia Road are shown on the Plans attached in Appendix E of the application.

### 10.3.2 Circulation

The CVSC has been designed to direct entering vehicles over a weigh bridge and then either into an inspection lane or a bypass lane. Following, vehicles are able to exit the site and continue travelling south bound.

The CVSC can facilitate the circulation of a standard 17.9m truck + semi-trailer, and a 25m truck + single steer tri-drive + trailer capable of carrying a 10m wide load. The plans demonstrate how the above vehicles enter, circulate, and exit the CSVS. These plans are attached as Appendix E to this application.

### **10.3.3 Parking**

Parking for all vehicles expected to utilise the CVSC will be provided within the CVSC site. A total of five car parks and one mobility car park will be provided onsite. These car parks will be used by Road Safety Officers and visitors to the site. Adequate space is provided onsite such that cars can enter and exit the site in a forward direction.

Provisions for parking are shown in Plans attached in Appendix E of the application.

## **10.4 Landscaping Proposed**

### **10.4.1 Designation TR1 – State Highway 1**

Minor landscaping is proposed within Designation TR1. This is limited to the re-instatement of grassed areas upon the completion of earthworks for signs and foundations. No further landscaping is proposed to occur within Designation TR1.

### **10.4.2 Designation TR7 – North Rakaia Road**

Landscaping with Designation TR7 is limited to re-grassing stormwater basins following the completion of earthworks. No shrubs or trees are proposed to be planted onsite and they have potential to compromise sightlines resulting in increased crash risk and reduced safety onsite.

## **10.5 Any Other Matters**

### **10.5.1 Management and Mitigation Measures**

An approved erosion and sediment control will be prepared in accordance with Canterbury Regional Council's Erosion and Sediment Control Toolbox. This plan will be implemented during the construction of the CVSC. This plan is required as a condition of consent CRC202422 for earthworks activities.

A dust management plan will be prepared prior to the commencement of works on the CVSC. This plan will be implemented for the duration of works.

An erosion and sediment control plan, and dust management plan will mitigate potential adverse effects arising during the construction of the CVSC. If both plans are implemented and followed, construction activities are expected to have effects less than minor.

### **10.5.2 Permanent Traffic Impacts**

The CVSC site is not expected to generate any additional traffic as site operators, police vehicles, and trucks directed to the CVSC are already traveling along State Highway 1.

### **10.5.3 Flood Zones and Fixed Floor Levels**

The CVSC site is located within the Lower Plains Flood Area as shown on the Selwyn District Planning Maps. As part of the previous consenting process, a Flood Hazard Risk assessment was obtained from Canterbury Regional Council. This Flood Hazard Risk Assessment has been included as Appendix F of this application, and is summarised as follows:

- New buildings with floor levels of at least 104.3 m.a.s.l (Lyttleton 1937 Datum) are likely to provide some freeboard above the Rakaia River overflows. This equates to 600mm above the existing ground level.

The proposed Tier 1.5 Compliance Building will be placed on 600mm wooden piles and the building will have a fixed floor level of 104.99 m.a.s.l (Lyttleton 1937 Datum) as shown in the plans included in Appendix B of this application.

#### 10.5.4 Compliance with Designation Conditions

| Designation TR7 (OSDP)                                                                                                                                                                                                          |                                                                                                                                              |
|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|----------------------------------------------------------------------------------------------------------------------------------------------|
| <i>General Accordance with details provided in the NOR and plans attached in D190066 Appendix C updated 17/02/2020; the Addendum to the NOR 17/02/2020; and the response to the request for further information 27/03/2020;</i> | <b>Not Applicable:</b> Proposal seeks to remove this condition.                                                                              |
| <i>10yr Lapse;</i>                                                                                                                                                                                                              | <b>Compliant:</b> Proposal will give effect to Designation TR7.                                                                              |
| <i>Prior to construction works being undertaken, engineering plans and specifications relating to the upgrade of the local road to be reviewed and approved by Council's Asset Manager, Transportation;</i>                     | <b>Will Comply:</b> Council's Asset Manager – Transportation has been consulted and is pending approval.                                     |
| <i>Prior to commencement of operations, the operational management plan shall be provided to Council's Asset Manager, Transportation.</i>                                                                                       | <b>Will Comply:</b> Council's Asset Manager – Transportation will be provided with a copy of the operational management plan for the site. . |

The proposal has also been assessed against the conditions of Designation NZTA-7 of the Proposed Selwyn District Plan. This assessment is as follows:

| Designation NZTA-7 (PSDP)                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                                |                                                                                           |
|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|-------------------------------------------------------------------------------------------|
| <p>1. <i>The designation shall be implemented in general accordance with the details provided with the Notice of Requirement:</i></p> <ul style="list-style-type: none"> <li><i>a. the Plans attached in Appendix C to the Notice of Requirement updated 17/02/2020;</i></li> <li><i>b. the Addendum to the Notice of Requirement dated 17/02/2020;</i></li> <li><i>c. the response to the request for further information dated 27/03/2020; and</i></li> <li><i>d. the response to the request for further information dated 17/04/2020.</i></li> </ul> | <b>Not Applicable:</b> Proposal seeks to remove condition                                 |
| <p>2. <i>Works undertaken in accordance with the NOR will not require an Outline Plan.</i></p>                                                                                                                                                                                                                                                                                                                                                                                                                                                           | <b>Not Applicable:</b> Works will be undertaken in accordance with proposed Outline Plan. |

|                                                                                                                                                                                              |                                                                     |
|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------|
| 3. <i>The designation shall lapse on the expiry of 10 years from the date on which it is included in the District Plan if it has not been given effect to before the end of that period.</i> | <b>Will Comply:</b> Proposal will give effect to Designation NZTA-7 |
|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|---------------------------------------------------------------------|

## 11 CONCLUSION

Pursuant to Section 181(1) of the RMA, Waka Kotahi gives notice of its requirement to alter existing Designation TR7 in the Operative Selwyn District Plan and Designation NZTA-7 in the Proposed District Plan. The proposed alteration to Designation satisfies the criteria outlined in s181(3) of the RMA. The proposed alteration is considered to be minor and can be processed on a non-notified basis.

The proposed alteration to designation will have positive effects in allowing the development to occur and will contribute to the effective enforcement of overweight and unsafe vehicles, reduced wear and maintenance on the road network, establish fair market conditions for compliant heavy vehicle operators, and increase the safety of other road network users.

The assessment of actual and potential effects concludes that any adverse effects of the proposed alteration to designation will be less than minor.

The proposal is in accordance with the designated purpose of the site. The works will enable Waka Kotahi to continue to ensure the state highway network remains resilient and fit for purpose.

Waka Kotahi as the Requiring Authority and by virtue of giving Notice, agrees to the alteration of the designation.

The bulk and location of the proposed works has been confirmed in the Outline Plan that forms part of this application. It is therefore concluded that pursuant to Section 176A of the RMA, the Selwyn District Council can confirm the Outline Plan for the works within TR1, TR7 without any modification.



## APPENDIX A



---

**WAKA KOTAHI NZ TRANSPORT AGENCY  
RAKAIA SOUTHBOUND  
WEIGH RIGHT PROGRAMME**

---

**CIVIL**

**TENDER**

Project No: 5C4353-WSP-09-DR-  
Date: 2022.04.22



CIVIL

TRAIDED

WSP PROJECT NO. (SUB-PROJECT)

| SHEET NO. | REVISION |
|-----------|----------|
| 0000      | 0        |

[illegible][illegible]



| EXISTING FEATURES                                                                 |                             |
|-----------------------------------------------------------------------------------|-----------------------------|
|  | PROPERTY BOUNDARY           |
|  | FENCE                       |
|  | TOP OF BANK                 |
|  | EDGE OF SEAL                |
|  | CHORUS DUCT                 |
|  | VODAFONE DUCT               |
|  | FIBRE DUCT                  |
| PROPOSED FEATURES                                                                 |                             |
|  | 0.2m & 1m CONTOURS          |
|  | NEW SITE BOUNDARY           |
|  | TOP OF BANK                 |
|  | BASIN FLOWLINE              |
|  | STD. KERB & CHANNEL         |
|  | LOW MOUNTABLE KERB          |
|  | TRAFFICKED HARDESTAND       |
|  | PEDESTRIAN HARDESTAND       |
|  | GRASSLANDSCAPED AREA        |
|  | BASIN                       |
|  | BUILDING                    |
|  | PEDESTRIAN PAVEMENT MARKING |
|  | SECURITY FENCE              |
|  | EDGE OF SEAL                |



CL02  
LONGSECTION BETWEEN STA: 0.00 AND 139.10  
SCALE HORIZ 1:250 VERT 1:50



| PROVISION | AMENDMENT         | APPROVED | DATE       |
|-----------|-------------------|----------|------------|
| A         | FOR INFORMATION   | LS       | 2021.12.17 |
| B         | FOR REVIEW        | BS       | 2021.12.21 |
| C         | FOR COMMENT       | BS       | 2022.01.18 |
| D         | ISSUED FOR TENDER | BS       | 2022.04.22 |
|           |                   |          |            |
|           |                   |          |            |
|           |                   |          |            |
|           |                   |          |            |
|           |                   |          |            |



**Wellington Office**  
+64 4 471 7000

CIVIL

## TENDER

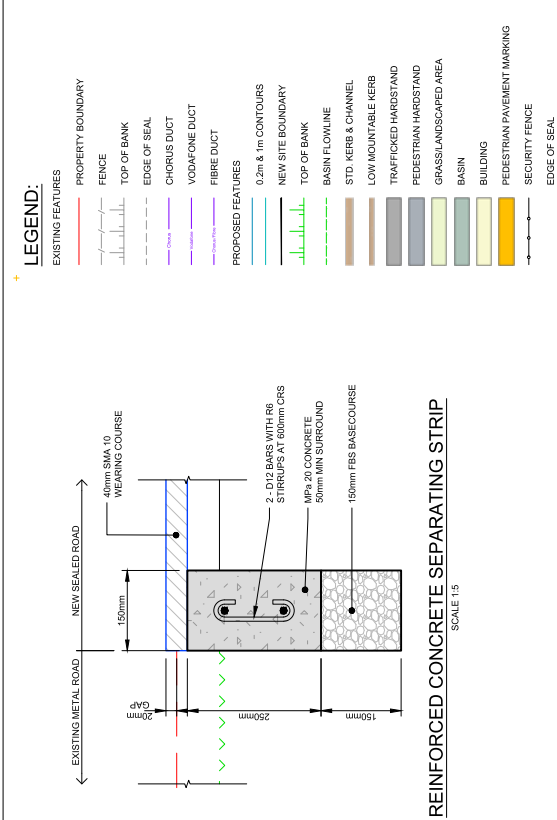
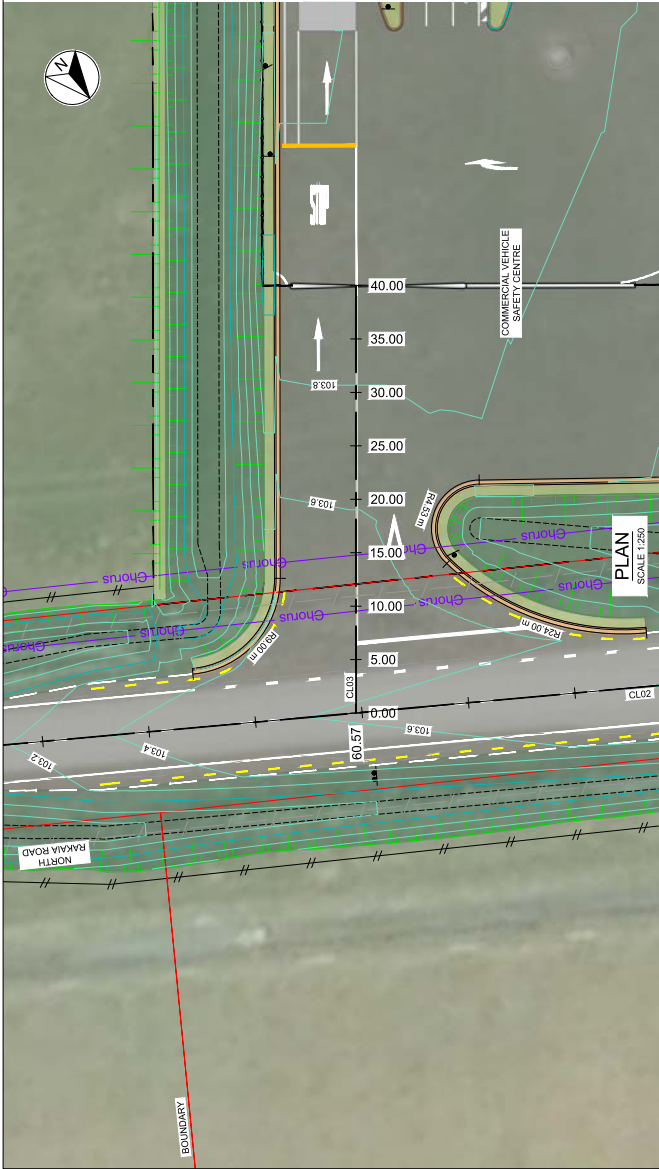
|                  |               |
|------------------|---------------|
| ORIGINAL SIZE    | A1            |
| DESIGNED         | M.A DONALDSON |
| DRAWN            | M.A DONALDSON |
| DESIGNED         | M.A DONALDSON |
| APPROVED         | B. STERLING   |
| DESIGN VERIFIED  | B. MICHELL    |
| DRAWING VERIFIED | B. MICHELL    |
| APPROVED DATE    | 2021.12.17    |

PROJECT  
WAKA KOTAHI NZ TRANSPORT AGENCY  
RAKAIA SOUTHBOUND  
WEIGH RIGHT PROGRAMME

**TITLE**  
**COMMERCIAL VEHICLE SAFETY CENTRE**  
**NORTH RAKAIA ROAD PLAN AND LONGITUDINAL SECTION**

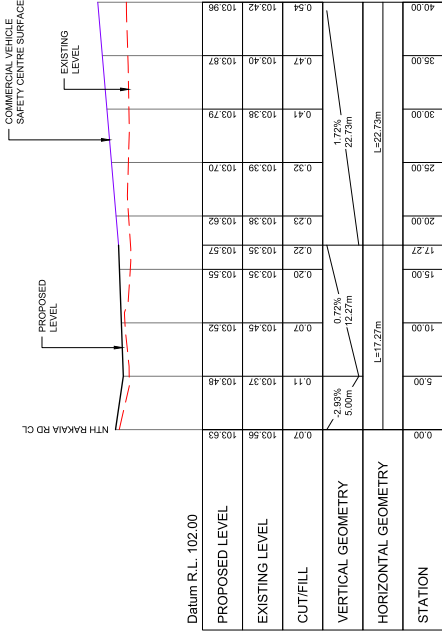
WSP PROJECT NO. (SUB-PROJECT)  
5C4353-WSP-09-DR-

SHEET NO. C-2250  
REVISION D



LEGEND:

- EXISTING FEATURES
  - PROPERTY BOUNDARY
  - FENCE
  - TOP OF BANK
  - EDGE OF SEAL
  - CHORUS DUCT
  - VODAFONE DUCT
  - FIBRE DUCT
- PROPOSED FEATURES
  - 0.2m & 1m COMTOURS
  - TOP OF BANK
  - NEW SITE BOUNDARY
  - MP3 20 CONCRETE
  - 50mm MIN SURROUND
  - 150mm FBS BASECOURSE
  - STD. KERB & CHANNEL
  - LOW MOUNTABLE KERB
  - PEDESTRIAN HARDSTAND
  - TRAFFICKED HARDSTAND
  - GRASSLANDSCAPED AREA
  - BASIN
  - BUILDING
  - PEDESTRIAN PAVEMENT MARKING
  - SECURITY FENCE
  - EDGE OF SEAL



CLO3  
LONG SECTION BETWEEN STA: 0.00 AND 57.79  
SCALE HORIZ 1:250 VERT 1:50



| REVISION | AMENDMENT         | APPROVED | DATE       |
|----------|-------------------|----------|------------|
| A        | FOR INFORMATION   | LS       | 2021.12.17 |
| B        | FOR REVIEW        | BS       | 2021.12.21 |
| C        | FOR PRELIM        | BS       | 2022.01.27 |
| D        | ISSUED FOR TENDER | BS       | 2022.04.22 |
|          |                   |          |            |
|          |                   |          |            |
|          |                   |          |            |



PO Box 12403  
Wellington  
New Zealand

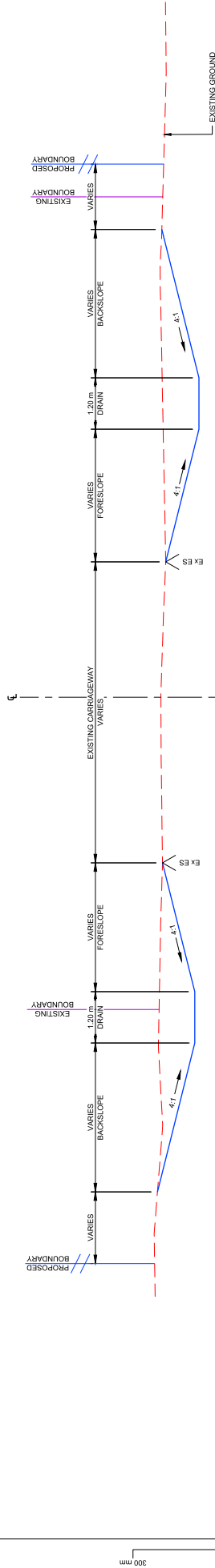
CIVIL

TENDER

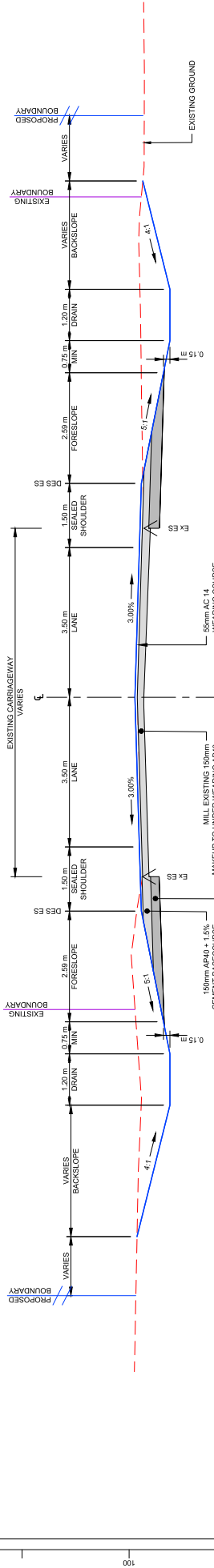
| SCALES | AS SHOWN         | DESIGNED        | DRAWN         | APPROVED | ORIGINAL SIZE |
|--------|------------------|-----------------|---------------|----------|---------------|
|        | M.A. DONALDSON   | M.A. DONALDSON  | B. STERLING   |          | A1            |
|        | DRAWING VERIFIED | DESIGN VERIFIED | APPROVED DATE |          |               |
|        | B. MCNEILL       | B. MCNEILL      | 2021.12.17    |          |               |

PROJECT  
WAKA KOTAHI NZ TRANSPORT AGENCY  
RAKAIA SOUTHBOUND  
WEIGH RIGHT PROGRAMME  
TITLE  
COMMERCIAL VEHICLE SAFETY CENTRE  
ACCESSWAY PLAN AND LONGITUDINAL SECTION & DETAILS  
WSP PROJECT NO. (SUB-PROJECT)  
5C4353-WSP-09-DR-  
SHEET NO.  
C-2251  
REVISION  
D

+



TYPICAL SECTION  
SIDE DRAINS



TYPICAL SECTION  
ROADING REHABILITATION

TYPICAL SECTIONS  
SCALE 1:50

1:50 @ A1  
1:100 @ A3

0 1 2 3 4 5 m

| REVISION | AMENDMENT         | APPROVED | DATE       |
|----------|-------------------|----------|------------|
| A        | FOR INFORMATION   | LS       | 2021.12.17 |
| B        | FOR REVIEW        | BS       | 2021.12.21 |
| C        | FOR PRELIM        | BS       | 2022.01.27 |
| D        | ISSUED FOR TENDER | BS       | 2022.04.22 |
|          |                   |          |            |
|          |                   |          |            |
|          |                   |          |            |



**wsp**  
Wellington Office  
PO Box 12003  
+64 4 471 7000  
New Zealand

CIVIL

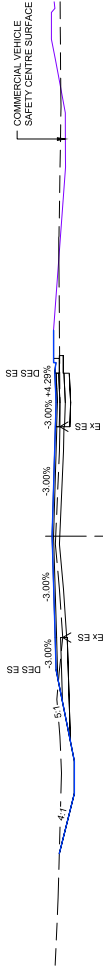
TENDER

| SCALES                |                 | ORIGINAL SIZE |  |
|-----------------------|-----------------|---------------|--|
| 1:50 @ A1, 1:100 @ A3 |                 | A1            |  |
| DRAWN                 | DESIGNED        | APPROVED      |  |
| M.A.DONALDSON         | M.A.DONALDSON   | B. STERLING   |  |
| DRAWING VERIFIED      | DESIGN VERIFIED | APPROVED DATE |  |
| B.McNEILL             | B.McNEILL       | 2021, 12, 17  |  |

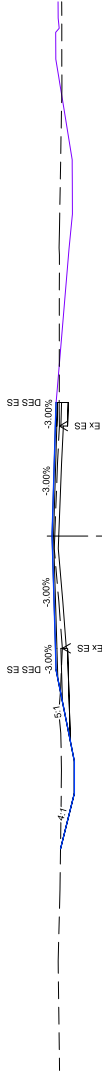
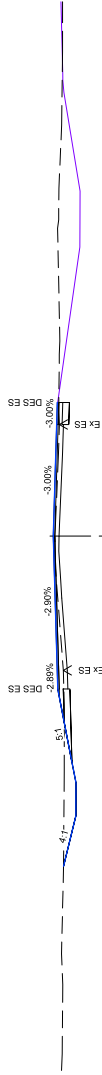
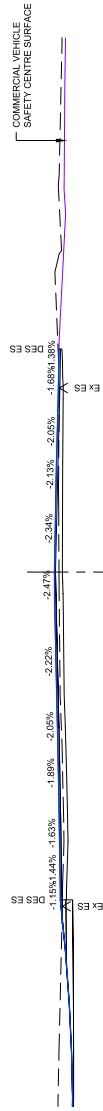
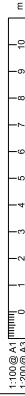
TENDER

PROJECT  
WAKA KOTAHI NZ TRANSPORT AGENCY  
RAKAIA SOUTHBOUND  
WEIGH RIGHT PROGRAMME  
TITLE  
COMMERCIAL VEHICLE SAFETY CENTRE  
NORTH RAKAIA ROAD TYPICAL SECTIONS  
WSP PROJECT NO. (SUB-PROJECT)  
5C4353-WSP-09-DR-

SHEET NO.  
C-2252  
REVISION  
D



| DGN. LEVEL                                     | DATUM: 100.00m |        |        |        |        |        |        |        |        |        |
|------------------------------------------------|----------------|--------|--------|--------|--------|--------|--------|--------|--------|--------|
| <span style="color: blue;">—</span> E.G. LEVEL | 103.46         | 103.48 | 103.47 | 103.46 | 103.46 | 103.46 | 103.46 | 103.46 | 103.46 | 103.46 |
| <span style="color: red;">—</span> CUT / FILL  | 0.00           | -0.50  | -0.38  | -0.50  | -0.50  | -0.50  | -0.50  | -0.50  | -0.50  | -0.50  |
| OFFSETS                                        | 4.10           | 5.00   | 6.10   | 5.52   | 0.16   | 0.14   | 0.17   | 0.22   | 0.22   | 0.22   |

[illegible][illegible][illegible]

| REVISION | AMENDMENT         | APPROVED | DATE       |
|----------|-------------------|----------|------------|
| A        | FOR INFORMATION   | LS       | 2021.12.17 |
| B        | FOR REVIEW        | BS       | 2021.12.21 |
| C        | FOR PRICING       | BS       | 2022.01.27 |
| D        | ISSUED FOR TENDER | B. S.    | 2022.04.29 |

WAKA KOTAHI  
isp

| ORIGINAL SIZE                           |                 | A1              |
|-----------------------------------------|-----------------|-----------------|
| <b>SCALES</b><br>1:100 @ A1, 1:200 @ A3 |                 |                 |
| <b>DRAWN</b>                            | <b>DESIGNED</b> | <b>APPROVED</b> |
| M.A. DONALDSON                          | M.A. DONALDSON  | B. STERLING     |

|                                                 |                               |                              |                             |
|-------------------------------------------------|-------------------------------|------------------------------|-----------------------------|
| PO Box 12-003<br>Wellington 6144<br>New Zealand | DRAWING VERIFIED<br>B McNEILL | DESIGN VERIFIED<br>B McNEILL | APPROVED DATE<br>2021.12.17 |
|-------------------------------------------------|-------------------------------|------------------------------|-----------------------------|

|                                                 |                               |                              |                             |
|-------------------------------------------------|-------------------------------|------------------------------|-----------------------------|
| PO Box 12-003<br>Wellington 6144<br>New Zealand | DRAWING VERIFIED<br>B McNEILL | DESIGN VERIFIED<br>B McNEILL | APPROVED DATE<br>2021.12.17 |
|-------------------------------------------------|-------------------------------|------------------------------|-----------------------------|

|                                                 |                               |                              |                             |
|-------------------------------------------------|-------------------------------|------------------------------|-----------------------------|
| PO Box 12-003<br>Wellington 6144<br>New Zealand | DRAWING VERIFIED<br>B McNEILL | DESIGN VERIFIED<br>B McNEILL | APPROVED DATE<br>2021.12.17 |
|-------------------------------------------------|-------------------------------|------------------------------|-----------------------------|

|                                                 |                               |                              |                             |
|-------------------------------------------------|-------------------------------|------------------------------|-----------------------------|
| PO Box 12-003<br>Wellington 6144<br>New Zealand | DRAWING VERIFIED<br>B McNEILL | DESIGN VERIFIED<br>B McNEILL | APPROVED DATE<br>2021.12.17 |
|-------------------------------------------------|-------------------------------|------------------------------|-----------------------------|

|                                                 |                               |                              |                             |
|-------------------------------------------------|-------------------------------|------------------------------|-----------------------------|
| PO Box 12-003<br>Wellington 6144<br>New Zealand | DRAWING VERIFIED<br>B McNEILL | DESIGN VERIFIED<br>B McNEILL | APPROVED DATE<br>2021.12.17 |
|-------------------------------------------------|-------------------------------|------------------------------|-----------------------------|

|                                                 |                               |                              |                             |
|-------------------------------------------------|-------------------------------|------------------------------|-----------------------------|
| PO Box 12-003<br>Wellington 6144<br>New Zealand | DRAWING VERIFIED<br>B McNEILL | DESIGN VERIFIED<br>B McNEILL | APPROVED DATE<br>2021.12.17 |
|-------------------------------------------------|-------------------------------|------------------------------|-----------------------------|

|                                                 |                               |                              |                             |
|-------------------------------------------------|-------------------------------|------------------------------|-----------------------------|
| PO Box 12-003<br>Wellington 6144<br>New Zealand | DRAWING VERIFIED<br>B McNEILL | DESIGN VERIFIED<br>B McNEILL | APPROVED DATE<br>2021.12.17 |
|-------------------------------------------------|-------------------------------|------------------------------|-----------------------------|

|                                                 |                               |                              |                             |
|-------------------------------------------------|-------------------------------|------------------------------|-----------------------------|
| PO Box 12-003<br>Wellington 6144<br>New Zealand | DRAWING VERIFIED<br>B McNEILL | DESIGN VERIFIED<br>B McNEILL | APPROVED DATE<br>2021.12.17 |
|-------------------------------------------------|-------------------------------|------------------------------|-----------------------------|

|                                                 |                               |                              |                             |
|-------------------------------------------------|-------------------------------|------------------------------|-----------------------------|
| PO Box 12-003<br>Wellington 6144<br>New Zealand | DRAWING VERIFIED<br>B McNEILL | DESIGN VERIFIED<br>B McNEILL | APPROVED DATE<br>2021.12.17 |
|-------------------------------------------------|-------------------------------|------------------------------|-----------------------------|

|                                                 |                               |                              |                             |
|-------------------------------------------------|-------------------------------|------------------------------|-----------------------------|
| PO Box 12-003<br>Wellington 6144<br>New Zealand | DRAWING VERIFIED<br>B McNEILL | DESIGN VERIFIED<br>B McNEILL | APPROVED DATE<br>2021.12.17 |
|-------------------------------------------------|-------------------------------|------------------------------|-----------------------------|

|                                                 |                               |                              |                             |
|-------------------------------------------------|-------------------------------|------------------------------|-----------------------------|
| PO Box 12-003<br>Wellington 6144<br>New Zealand | DRAWING VERIFIED<br>B McNEILL | DESIGN VERIFIED<br>B McNEILL | APPROVED DATE<br>2021.12.17 |
|-------------------------------------------------|-------------------------------|------------------------------|-----------------------------|

|                                                 |                               |                              |                             |
|-------------------------------------------------|-------------------------------|------------------------------|-----------------------------|
| PO Box 12-003<br>Wellington 6144<br>New Zealand | DRAWING VERIFIED<br>B McNEILL | DESIGN VERIFIED<br>B McNEILL | APPROVED DATE<br>2021.12.17 |
|-------------------------------------------------|-------------------------------|------------------------------|-----------------------------|

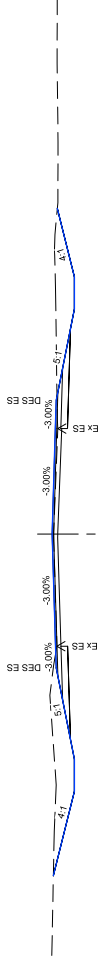
|                                                 |                               |                              |                             |
|-------------------------------------------------|-------------------------------|------------------------------|-----------------------------|
| PO Box 12-003<br>Wellington 6144<br>New Zealand | DRAWING VERIFIED<br>B McNEILL | DESIGN VERIFIED<br>B McNEILL | APPROVED DATE<br>2021.12.17 |
|-------------------------------------------------|-------------------------------|------------------------------|-----------------------------|

|                                                 |                               |                              |                             |
|-------------------------------------------------|-------------------------------|------------------------------|-----------------------------|
| PO Box 12-003<br>Wellington 6144<br>New Zealand | DRAWING VERIFIED<br>B McNEILL | DESIGN VERIFIED<br>B McNEILL | APPROVED DATE<br>2021.12.17 |
|-------------------------------------------------|-------------------------------|------------------------------|-----------------------------|

|                                                 |                               |                              |                             |
|-------------------------------------------------|-------------------------------|------------------------------|-----------------------------|
| PO Box 12-003<br>Wellington 6144<br>New Zealand | DRAWING VERIFIED<br>B McNEILL | DESIGN VERIFIED<br>B McNEILL | APPROVED DATE<br>2021.12.17 |
|-------------------------------------------------|-------------------------------|------------------------------|-----------------------------|

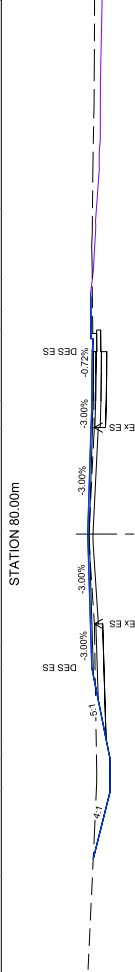
|                                                 |                               |                              |                             |
|-------------------------------------------------|-------------------------------|------------------------------|-----------------------------|
| PO Box 12-003<br>Wellington 6144<br>New Zealand | DRAWING VERIFIED<br>B McNEILL | DESIGN VERIFIED<br>B McNEILL | APPROVED DATE<br>2021.12.17 |
|-------------------------------------------------|-------------------------------|------------------------------|-----------------------------|

+



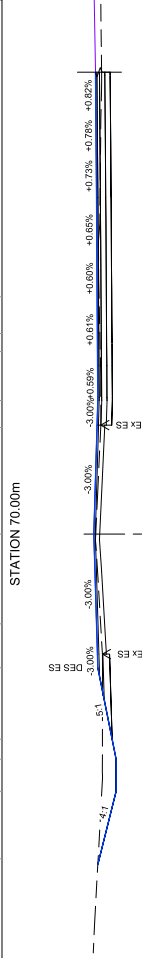
DATUM: 100.00m

|            |        |        |        |        |        |        |        |        |        |        |        |        |        |        |
|------------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|
| DGN. LEVEL | -12.78 | 0.00   | -9.63  | -8.43  | -7.68  | -5.00  | -4.20  | 0.00   | 3.95   | 5.00   | 7.68   | 8.43   | 9.63   | 12.19  |
| E.G. LEVEL | 103.41 | 103.41 | 102.62 | 102.77 | 103.30 | 103.41 | 103.30 | 103.38 | 103.24 | 103.30 | 103.32 | 103.34 | 103.26 | 103.26 |
| CUT / FILL |        |        |        |        |        |        |        |        |        |        |        |        |        |        |
| OFFSETS    |        |        |        |        |        |        |        |        |        |        |        |        |        |        |



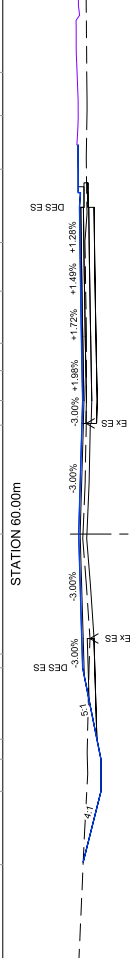
DATUM: 100.00m

|            |        |        |        |        |        |        |        |        |        |        |        |        |        |
|------------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|
| DGN. LEVEL | -12.15 | 0.00   | -9.63  | -8.43  | -7.68  | -5.00  | -3.36  | 0.00   | 3.99   | 5.00   | 6.83   | 7.71   | 8.88   |
| E.G. LEVEL | 103.36 | 103.36 | 102.73 | 102.74 | 103.24 | 103.29 | 103.32 | 103.57 | 103.45 | 103.42 | 103.40 | 103.38 | 103.39 |
| CUT / FILL |        |        |        |        |        |        |        |        |        |        |        |        |        |
| OFFSETS    |        |        |        |        |        |        |        |        |        |        |        |        |        |



DATUM: 100.00m

|            |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |
|------------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|
| DGN. LEVEL | -12.37 | 0.00   | -9.63  | -8.43  | -7.68  | -5.00  | -4.49  | 0.00   | 4.08   | 5.00   | 6.12   | 7.14   | 9.10   | 10.91  | 13.66  | 15.67  | 17.29  |
| E.G. LEVEL | 103.48 | 103.48 | 102.80 | 103.33 | 102.95 | 103.31 | 103.46 | 103.63 | 103.51 | 103.48 | 103.49 | 103.50 | 103.51 | 103.52 | 103.54 | 103.55 | 103.57 |
| CUT / FILL |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |
| OFFSETS    |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |



DATUM: 100.00m

|            |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |
|------------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|--------|
| DGN. LEVEL | -12.28 | 0.00   | -9.63  | -8.43  | -7.68  | -5.00  | -3.91  | 0.00   | 4.14   | 5.00   | 6.44   | 7.80   | 9.96   | 12.20  | 13.00  |
| E.G. LEVEL | 103.52 | 103.52 | 102.86 | 103.37 | 103.01 | 103.54 | 103.58 | 103.69 | 103.57 | 103.54 | 103.60 | 103.44 | 103.63 | 103.66 | 103.71 |
| CUT / FILL |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |
| OFFSETS    |        |        |        |        |        |        |        |        |        |        |        |        |        |        |        |

1:100 @ A1 (MINIMUM)  
1:200 @ A3



| REVISION | AMENDMENT         | APPROVED | DATE       |
|----------|-------------------|----------|------------|
| A        | FOR INFORMATION   | LS       | 2021.12.17 |
| B        | FOR REVIEW        | BS       | 2021.12.21 |
| C        | FOR PRELIM        | BS       | 2022.01.27 |
| D        | ISSUED FOR TENDER | BS       | 2022.04.26 |
|          |                   |          |            |
|          |                   |          |            |
|          |                   |          |            |



**wsp**  
Wellington Office  
PO Box 12003  
New Zealand  
+64 4 477 7000

CIVIL

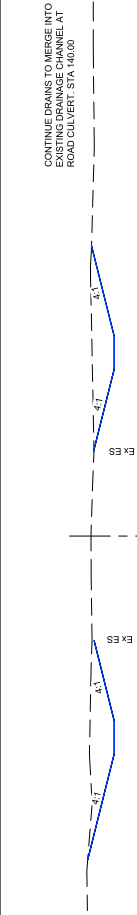
TENDER

| ORIGINAL SIZE          |                | A1              |               |
|------------------------|----------------|-----------------|---------------|
| SCALES                 |                |                 |               |
| 1:100 @ A1, 1:200 @ A3 |                | DESIGNED        | APPROVED      |
| DRAWN                  | M.A. DONALDSON | M.A. DONALDSON  | B. STERLING   |
| DRAWING VERIFIED       | B. MCNEILL     | DESIGN VERIFIED | APPROVED DATE |
|                        |                | B. MCNEILL      | 2021.12.17    |

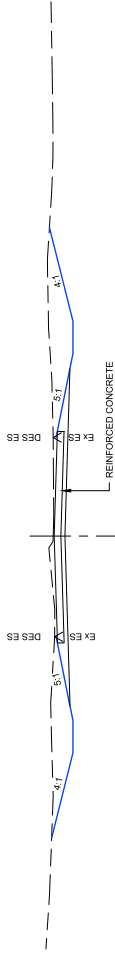
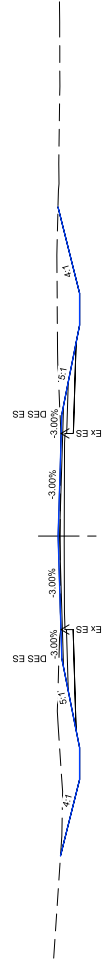
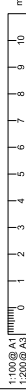
TENDER

PROJECT  
WAKA KOTAHI NZ TRANSPORT AGENCY  
RAKAIA SOUTHBOUND  
WEIGH RIGHT PROGRAMME  
TITLE  
COMMERCIAL VEHICLE SAFETY CENTRE  
NORTH RAKAIA ROAD CROSS SECTIONS STA 50.00 - 80.00  
WSP PROJECT NO. (SUB-PROJECT)  
5C4353-WSP-09-DR-

SHEET NO.  
C-2254  
REVISION  
D



|        | DGN. LEVEL | E.G. LEVEL | CUT / FILL | OFFSETS |
|--------|------------|------------|------------|---------|
| -12.11 | 0.00       | 103.32     | 103.32     | 103.32  |
| -8.90  | 0.73       | 103.22     | 102.49     | 102.49  |
| -6.19  | -0.90      | 103.25     | 102.49     | 102.49  |
| -9.93  | -0.91      | 103.23     | 102.32     | 102.32  |
| -6.26  | -0.73      | 103.22     | 102.49     | 102.49  |
| -3.92  | -0.08      | 103.16     | 103.08     | 103.08  |
| 3.16   | 0.02       | 103.06     | 103.10     | 103.10  |
| 5.59   | -0.64      | 103.13     | 102.49     | 102.49  |
| 6.27   | -0.82      | 103.14     | 102.32     | 102.32  |
| 8.14   | -0.69      | 103.16     | 102.32     | 102.32  |
| 10.87  | 0.00       | 103.16     | 103.16     | 103.16  |

[illegible][illegible][illegible]

| REVISION | MEDICAMENT        | APPROVED | DATE       |
|----------|-------------------|----------|------------|
| A        | FOR INFORMATION   | LS       | 2021.12.17 |
| B        | FOR REVIEW        | LS       | 2021.11.17 |
| C        | FOR PROG.         | BS       | 2021.11.22 |
| D        | ISSUED FOR TENDER | B.S.     | 2022.04.22 |
|          |                   |          |            |
|          |                   |          |            |
|          |                   |          |            |
|          |                   |          |            |



 Wellington Office  
+64 4 471 7000

PO Box 12-003  
Wellington 0144  
New Zealand

CIVIL

|                  |                        |                 |
|------------------|------------------------|-----------------|
| ORIGINAL SIZE    | A1                     |                 |
| SCALES           | 1:100 @ A1, 1:200 @ A3 |                 |
| DRAWN            | M.A.DONALDSON          | DESIGNED        |
| DRAWING VERIFIED | B.McNEILL              | DESIGN VERIFIED |
|                  |                        | APPROVED DATE   |
|                  |                        | 2021.12.17      |
|                  |                        | TENDER          |

|                               |                                                                                         |
|-------------------------------|-----------------------------------------------------------------------------------------|
| PROJECT                       | WAKA KOTAHI NZ TRANSPORT AGENCY<br>RAKAIA SOUTHBOND<br>WEIGH RIGHT PROGRAMME            |
| TITLE                         | COMMERCIAL VEHICLE SAFETY CENTRE<br>NORTH RAKAIA ROAD CROSS SECTIONS STA 90.00 - 120.00 |
| WSP PROJECT NO. (SUB-PROJECT) | 50C4353-WSP-09-DR-                                                                      |
| SHEET NO.                     | C-2255                                                                                  |
| REVISION                      | D                                                                                       |



## APPENDIX B

# NEW ZEALAND TRANSPORT AGENCY

## TIER 1.5 - CONTROL BUILDING

# TIER 1.5 - CONTROL BUILDING

**51-34037**



NZTA TIER 1.5 TYPE CONTROL BUILDING

THIS IS A STANDARDISED BUILDING TYPE THAT IS CONSTRUCTED AT SEVERAL SITES REFER TO SPECIFIC SITE PLAN FOR LOCATION OF CONTROL RIGGING WITHIN SITE

THIS BUILDING HAS BEEN DESIGNED TO THE FOLLOWING 'WORST CASE SCENARIO' SITE CONDITIONS

WINDZONE: VERY HIGH  
EARTHQUAKE ZONE: 3  
EXPOSURE ZONE: ZONE

WHERE THE PROPOSED LOCATION OF THE TIER 1.5 BUILDING TYPE EXCEEDS TO THE ABOVE SCENARIOS, THEN THE BUILDING WILL NEED TO BE DESIGNED SPECIFICALLY FOR THE SITE.

| DRAWING LIST | DRG NO.   | PROJECT NO. | SHEET | DRAWING TITLE                 | CURRENT REVISION |
|--------------|-----------|-------------|-------|-------------------------------|------------------|
|              | 51-340037 | A001        |       | ARCHITECTURAL DRAWING INDEX   | 1                |
|              | 51-340037 | A010        |       | GENERAL NOTES                 | 1                |
|              | 51-340037 | A100        |       | GENERAL ARRANGEMENT PLAN      | 1                |
|              | 51-340037 | A101        |       | SET OUT PLAN                  | 2                |
|              | 51-340037 | A102        |       | SECURITY PLAN                 | 1                |
|              | 51-340037 | A103        |       | CEILING PLAN                  | 1                |
|              | 51-340037 | A110        |       | ROOF PLAN                     | 1                |
|              | 51-340037 | A120        |       | SUBELOOR TRAINING PLAN        | 1                |
|              | 51-340037 | A121        |       | FLOOR TRAINING PLAN           | 1                |
|              | 51-340037 | A122        |       | ROOF TRAINING PLAN            | 1                |
|              | 51-340037 | A190        |       | EXTERIOR RAMP & STAIR DETAILS | 1                |
|              | 51-340037 | A200        |       | INTERIOR RAMP & STAIR DETAILS | 1                |
|              | 51-340037 | A201        |       | EXTERIOR ELEVATIONS           | 1                |
|              | 51-340037 | A250        |       | SECTIONS                      | 1                |
|              | 51-340037 | A300        |       | KITCHEN-JOINER SETOUT         | 1                |
|              | 51-340037 | A301        |       | MC ELEVATIONS                 | 1                |
|              | 51-340037 | A400        |       | PAVEMENTS                     | 1                |
|              | 51-340037 | A401        |       | PAVEMENT DETAILS              | 1                |
|              | 51-340037 | A410        |       | WINDOW DETAILS                | 1                |
|              | 51-340037 | A415        |       | DOOR DETAILS                  | 1                |
|              | 51-340037 | A500        |       | DOOR & WINDOW SCHEDULE        | 1                |
|              | 51-340037 | A600        |       | HYDRAULICS & PLUMBING PLAN    | 1                |
|              | 51-340037 | A601        |       | MECHANICAL LAYOUT PLAN        | 1                |
|              | 51-340037 | A602        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A603        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A604        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A605        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A606        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A607        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A608        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A609        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A610        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A611        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A612        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A613        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A614        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A615        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A616        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A617        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A618        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A619        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A620        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A621        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A622        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A623        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A624        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A625        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A626        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A627        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A628        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A629        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A630        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A631        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A632        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A633        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A634        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A635        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A636        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A637        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A638        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A639        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A640        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A641        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A642        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A643        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A644        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A645        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A646        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A647        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A648        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A649        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A650        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A651        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A652        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A653        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A654        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A655        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A656        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A657        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A658        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A659        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A660        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A661        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A662        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A663        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A664        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A665        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A666        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A667        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A668        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A669        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A670        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A671        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A672        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A673        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A674        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A675        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A676        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A677        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A678        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A679        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A680        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A681        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A682        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A683        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A684        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A685        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A686        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A687        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A688        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A689        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A690        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A691        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A692        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A693        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A694        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A695        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A696        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A697        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A698        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A699        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A700        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A701        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A702        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A703        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A704        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A705        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A706        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A707        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A708        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A709        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A710        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A711        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A712        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A713        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A714        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A715        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A716        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A717        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A718        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A719        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A720        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A721        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A722        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A723        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A724        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A725        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A726        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A727        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A728        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A729        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A730        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A731        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A732        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A733        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A734        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A735        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A736        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A737        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A738        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A739        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A740        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A741        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A742        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A743        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A744        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A745        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A746        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A747        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A748        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A749        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A750        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A751        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A752        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A753        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A754        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A755        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A756        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A757        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A758        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A759        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A760        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A761        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A762        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A763        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A764        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A765        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A766        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A767        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A768        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A769        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A770        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A771        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A772        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A773        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A774        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A775        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A776        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A777        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A778        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A779        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A780        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A781        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A782        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A783        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A784        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A785        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A786        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A787        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A788        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A789        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A790        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A791        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A792        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A793        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A794        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A795        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A796        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A797        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A798        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A799        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A800        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A801        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A802        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A803        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A804        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A805        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A806        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A807        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A808        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A809        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A810        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A811        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A812        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A813        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A814        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A815        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A816        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A817        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A818        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A819        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A820        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A821        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A822        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A823        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A824        |       | MECHANICAL LAYOUT             | 1                |
|              | 51-340037 | A825        |       |                               |                  |

| No | Revision | 1 | CONSENT ISSUE | Date     |
|----|----------|---|---------------|----------|
|    |          |   |               | 05.07.19 |

**GHDWOODHEAD**  
creativespaces

Level 1, Grant Thornton House  
215 Lambton Quay, Wellington New Zealand  
T 64 4 472 0799 F 64 4 72 0833  
E [vg@ghd.com](mailto:vg@ghd.com) W [www.ghd.com](http://www.ghd.com)

Conditions of Use.  
This document may only be used by  
GHD's client (and any other person who  
GHD has agreed can use this document  
for the purpose for which it was prepared  
and must not be used by any other  
person or for any other purpose.

**DO NOT SCALE**

Scale 1 : 1

**TENDER / CONSENT**

|                            |                                                                                                                   |
|----------------------------|-------------------------------------------------------------------------------------------------------------------|
| Client<br>Project<br>Title | NEW ZEALAND TRANSPORT AGENCY<br>TIER 1.5 - CONTROL BUILDING<br>ARCHITECTURAL WORKS<br>ARCHITECTURAL DRAWING INDEX |
|----------------------------|-------------------------------------------------------------------------------------------------------------------|

---

|              |                            |
|--------------|----------------------------|
| Drawing No:  | <b>A1    51-34037-A001</b> |
| Revision No: | <b>1</b>                   |

## TENDER / CONSENT

[illegible]

1. ALL TIMBER SHALL BE US12 RADII RATED.
2. TIMBER TREATMENT TO COMPLY WITH NZS 3602.
3. ALL TIMBER SHALL BE GALVANIZED TO PREVENT CORROSION. BEARS TO BE STAINLESS STEEL. ALL OTHER LOCATIONS THEY ARE TO BE GALVANIZED.
4. ALL NUTS AND BOLT HEADS THROUGH TIMBER ARE TO HAVE GALVANIZED OR STAINLESS WASHERS. 50x50mm OR 60x60mm BUT NOT EXCEEDING 12mm UNO.
5. BOLT SIZES SHALL BE TO THE SIZES AND POSITIONS AS SHOWN ON THE DRAWINGS. THE MINIMUM LENGTH OF THE BOLT SHALL BE 10 TIMES THE DIAMETER OF THE BOLT. THE MINIMUM PENETRATION OF THE BOLT SHALL BE 10 TIMES THE DIAMETER OF THE BOLT. THE MINIMUM PENETRATION OF THE BOLT SHALL BE 10 TIMES THE DIAMETER OF THE BOLT. THE MINIMUM PENETRATION OF THE BOLT SHALL BE 10 TIMES THE DIAMETER OF THE BOLT.
6. BRIDS, BRIDS AND SCREWS AND NUTS SHALL BE OF STEEL. GENERALLY IN ACCORDANCE WITH NZS 3602. MINIMUM PENETRATIONS OF NUTS, BRIDS AND SCREWS SHALL BE 10 TIMES THE FASTENER DIAMETER. ALL FRAMED METAL PENETRATION THROUGH PICKING WILL NOT COUNT.
7. ALL FRAMED METAL PENETRATION THROUGH PICKING WILL NOT COUNT.
8. MISCELLANEOUS TIMBER FASTENERS SHALL BE GALVANIZED 250 MLD. ALL TIMBER NUTS SHALL BE OF STEEL. ALL TIMBER NUTS SHALL BE OF STEEL. ALL TIMBER NUTS SHALL BE OF STEEL.

|  |                                 |
|--|---------------------------------|
|  | CENTRELINE OF ELEMENT           |
|  | BUILDING SETOUT POINT           |
|  | LEVEL DATUM - TOP OF ELEMENT    |
|  | LEVEL DATUM - BOTTOM OF ELEMENT |
|  | PLAN SPOT RELATIVE LEVEL        |
|  | FALL DIRECTION SYMBOL           |
|  | CEILING TAG                     |
|  | DOOR TAG                        |
|  | ROOM NUMBER                     |
|  | WALL TYPE TAG                   |
|  | WINDOW TAG                      |
|  | CURTAIN WALL TAG                |
|  | CONCRETE FLOOR SLAB SETDOWN     |
|  | CONCRETE FLOOR SLAB EDGE        |
|  | PLAN - FINISH FLOOR LEVEL       |
|  | PLAN - SLAB LEVEL               |

INTERIOR FRAMING - STUDS @  
400mm CENTRES  
R2.5 100x100mm WALL BATTS  
7mm ECOLY RAB - SCREW FIXED  
45x114 H2 TIMBER Cavity BATTENS  
CORRUGATED PROFILE CLADDING  
30mm STANDARD PLASTERBOARD  
CONCRETE  
INTERIOR LINING - LA FINISH  
90mm TIMBER FRAMING - STUDS @  
400mm CENTRES  
2mm GAUGED STEEL SHEET INSTALLABLE  
TO INSIDE FACE OF WALL FRAMING  
7mm ECOLY RAB - SCREW FIXED  
45x114 H2 TIMBER Cavity BATTENS  
CORRUGATED PROFILE CLADDING

'GREY FRANKS'  
• ROOF UNDERLAY  
• 70x45mm H3.2 TIMBER PURLINS @ 900mm  
• CTRS FIXED ON FLAT  
• 290x45mm H1.2 TIMBER RAFTERS AT 480mm CTRS NOTCHED AT SOFFIT

**STEEL UTTER - 125 BOX**  
• DIMOND BOX 125 GUTTER PROFILE  
• COLORSTEEL MAXX FINISH - COLOUR 'GREY FRANKS'

**100mmØ DOWN PIPE**  
• DIMOND Ø 55mm STEEL  
• FINISH TO MATCH ROOFING

**GU101**

**DP01**

1. ALL OTHER RAINWATER GOODS, FININGS AND SEALANTS TO MATCH ROOF SHEETING.
2. REFER TO SERVICES DRAWINGS FOR ALL ROOF SERVICE PENETRATIONS.
3. ALL ROOF PENETRATIONS TO BE APPROPRIATELY FLASHED/SEALED TO RIDGE LINE. REFER TO SPECIFICATION FOR DETAILS.
4. CONNECT ALL NEW RAINWATER DOWNPIPES TO STORM WATER RETICULATION AND LEGAL POINT OF DISCHARGE.

|                                                                                                                                                                                                                                                                                                               |                                                                                                                                                                                                                                                                                                                |
|---------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| <p><b>7</b></p> <p><b>FINISHES</b></p> <ul style="list-style-type: none"> <li>• FULFILL DEPTH JOISTS AT 180mm</li> <li>• ADJACENT FLOOR JOISTS AT 450mm</li> <li>• 6mm FIBRE CEMENT LINING TO CRACKS</li> <li>• PROPRIETARY UNIFLOOR</li> <li>• INSULATION R1.9</li> <li>• MAMMOET M1 UNIFLOOR COR</li> </ul> | <p><b>8</b></p> <p><b>FINISHES</b></p> <ul style="list-style-type: none"> <li>• FULFILL DEPTH JOISTS AT 180mm</li> <li>• ADJACENT FLOOR JOISTS AT 450mm</li> <li>• 6mm FIBRE CEMENT LINING TO CRACKS</li> <li>• PROPRIETARY UNIFLOOR</li> <li>• INSULATION R1.9</li> <li>• MAMMOET M1 UNIFLOOR COR</li> </ul>  |
| <p><b>9</b></p> <p><b>FINISHES</b></p> <ul style="list-style-type: none"> <li>• FULFILL DEPTH JOISTS AT 180mm</li> <li>• ADJACENT FLOOR JOISTS AT 450mm</li> <li>• 6mm FIBRE CEMENT LINING TO CRACKS</li> <li>• PROPRIETARY UNIFLOOR</li> <li>• INSULATION R1.9</li> <li>• MAMMOET M1 UNIFLOOR COR</li> </ul> | <p><b>10</b></p> <p><b>FINISHES</b></p> <ul style="list-style-type: none"> <li>• FULFILL DEPTH JOISTS AT 180mm</li> <li>• ADJACENT FLOOR JOISTS AT 450mm</li> <li>• 6mm FIBRE CEMENT LINING TO CRACKS</li> <li>• PROPRIETARY UNIFLOOR</li> <li>• INSULATION R1.9</li> <li>• MAMMOET M1 UNIFLOOR COR</li> </ul> |

**ROOF:**  
FIT R2.5 FIBERGLASS BATTS TO ALL CEILING SPACES. PINK BATTS R2.5 SKULLION ROOF INSULATION SHALL BE INSTALLED TO SKULLION ROOF TO ACHIEVE 25MM AIR GAP BETWEEN INSULATION AND ROOF UNDERLAY

**WALL INSULATION:**  
EXTERIOR WALLS ARE TO BE INSULATED WITH R2.8 PINK BATTS' ULTRA FIBERGLASS BATTS.

**UNDERFLOOR INSULATION:**  
MAMMOET POLYESTER R1.9 UNDERFLOOR SECTIONS FITTED BETWEEN FLOOR JOISTS

BRACING ELEMENT  
BRACING ELEMENT  
BRACING ELEMENT  
BRACING LINE ELEMENT  
BRACING LINE  
LINTEL ABOVE WINDOW

- THE CONTRACTOR IS OBLIGATED TO THEIR NEED TO UNDERSTAND AND ITS OBLIGATIONS OF SAFETY IN DESIGN UNDER SECTION 39, SAFETY IN DESIGN WITH PARTICULAR REFERENCE TO SECTION 39, SAFETY IN DESIGN.
- GLO HAS TAKEN ALL REASONABLE STEPS TO IDENTIFY HAZARDS ASSOCIATED WITH DESIGN VIA AN INTERNATIONALLY ACCREDITED SAFETY IN DESIGN REVIEW PROCESS AND HAS MADE THE RESULTING DESIGN REVIEW REPORT AVAILABLE TO ALL OF ITS EMPLOYEES FOR REFERENCE AND ACTION DURING THE CONSTRUCTION OF THIS PROJECT.
- IN THE EVENT THAT A DESIGN RELATED HAZARD BECOMES APPARENT TO THE CLIENT AND/OR CONTRACTOR, THEY ARE REQUIRED TO IMMEDIATELY NOTIFY GLO OF THESE ISSUES IN WRITING AND TO IDENTIFY THE APPROPRIATE RISK MITIGATION METHODS.
- GLO'S SAFETY IN DESIGN REVIEW RELIES ON THE CONTRACTOR UNDERSTANDING ALL WORKS IN ACCORDANCE WITH THE HEALTH AND SAFETY AT WORK ACT 2015, SUBSEQUENT HEALTH AND SAFETY REGULATIONS AND GUIDANCE, IN CONJUNCTION WITH IDENTIFIED CODES AND INDUSTRY STANDARDS.

- **PLASTERBOARD CEILING**
- 13mm PLASTERBOARD, L4 FINISH, PAINTED
- 308 RONDO CEILING BATTENS ON GIB Z26 CLIP FIXED TO ROOF FRAMING
- **SOFFIT LINING**
- 6mm THICK FIBRE CEMENT SHEET
- PROPRIETARY JOINTERS
- 45mm FRAMING

- REFER TO ELECTRICAL DRAWINGS TO COORDINATE LIGHTING AND FOR INSTALLATION DETAILS
- REFER TO SERVICES DRAWINGS FOR OTHER REQUIRED CEILING ACCESS POINTS
- PROVIDE ACCESS PANELS IN CEILING IN ACCORDANCE WITH BUILDING SERVICES REQUIREMENTS
- CEILING SHALL BE READ IN CONJUNCTION WITH BUILDING SERVICES AND RELEVANT SHOP DRAWINGS
- CEILING EQUIPMENT SETTINGS TO ALLOW WHERE POSSIBLE CENTRE LINES AND BE SPACED EVENLY BETWEEN ADJACENT WALLS
- PROVIDE ADDITIONAL SUPPORTS IN THE CEILING SUSPENSION SYSTEM WHERE CEILING MOUNTED CURTAIN TRAGERS ARE HUNG
- CEILING MOUNTED EQUIPMENT SHALL BE TO SUPPORT EQUIPMENT WEIGHTS AND VIBRATIONS

|                                                                                       |            |                                                                                                                                                        |
|---------------------------------------------------------------------------------------|------------|--------------------------------------------------------------------------------------------------------------------------------------------------------|
|  | <b>WC</b>  | SFC PAN CAROMA CUBE WALL FACED TOILET SUITE WITH WC CLOSE SET PRODUCT CODE: 624N15W                                                                    |
|  | <b>VHB</b> | BASIN CAROMA LUNA HAND WALL BASIN BLACK BOTTLE TRAP                                                                                                    |
|  | <b>BMT</b> | BASIN MIXER CAROMA LUNO BASIN MIXER                                                                                                                    |
|  | <b>CS</b>  | CLEANERS SINK CAROMES SINKS WITH SP/LASHBACK AND BRACKET GRID CODE: FRCS<br>CLEANERS SINK TAPWARE ZURN SINK FAUCET FOR CLEANERS SINK TAPWARE ZURN 2-3" |
|  | <b>SHR</b> | EXTERIOR SHOWER SPEKAMUM FEFASHER WALL MOUNTED EXTERIOR EXERCISER SHOWER HEAD WITH PULL DOWN ARM                                                       |

|                                                                                     |                                                                                 |
|-------------------------------------------------------------------------------------|---------------------------------------------------------------------------------|
|  | <b>MIRROR</b> - 1000mm H x 600mm W FRAMLESS MIRROR - GLUE FIXED TO WALL SURFACE |
|  | <b>TOILET ROLL HOLDER</b> - SINGLE JUMBO DISPENSER RESERVE, LHM-COTTONSOFT      |
|  | <b>SOAP DISPENSER</b> - AS1 SURFACE MOUNTED VERTICAL SOAP DISPENSER             |
|  | <b>PAPER TOWEL DISPENSER</b> - AS1 SURFACE MOUNTED PAPER TOWEL DISPENSER        |

Client **NEW ZEALAND TRANSPORT AGENCY**  
Project **TIER 1.5 - CONTROL BUILDING**  
Title **ARCHITECTURAL WORKS  
GENERAL NOTES**

ARCHITECTURAL SYMBOLS

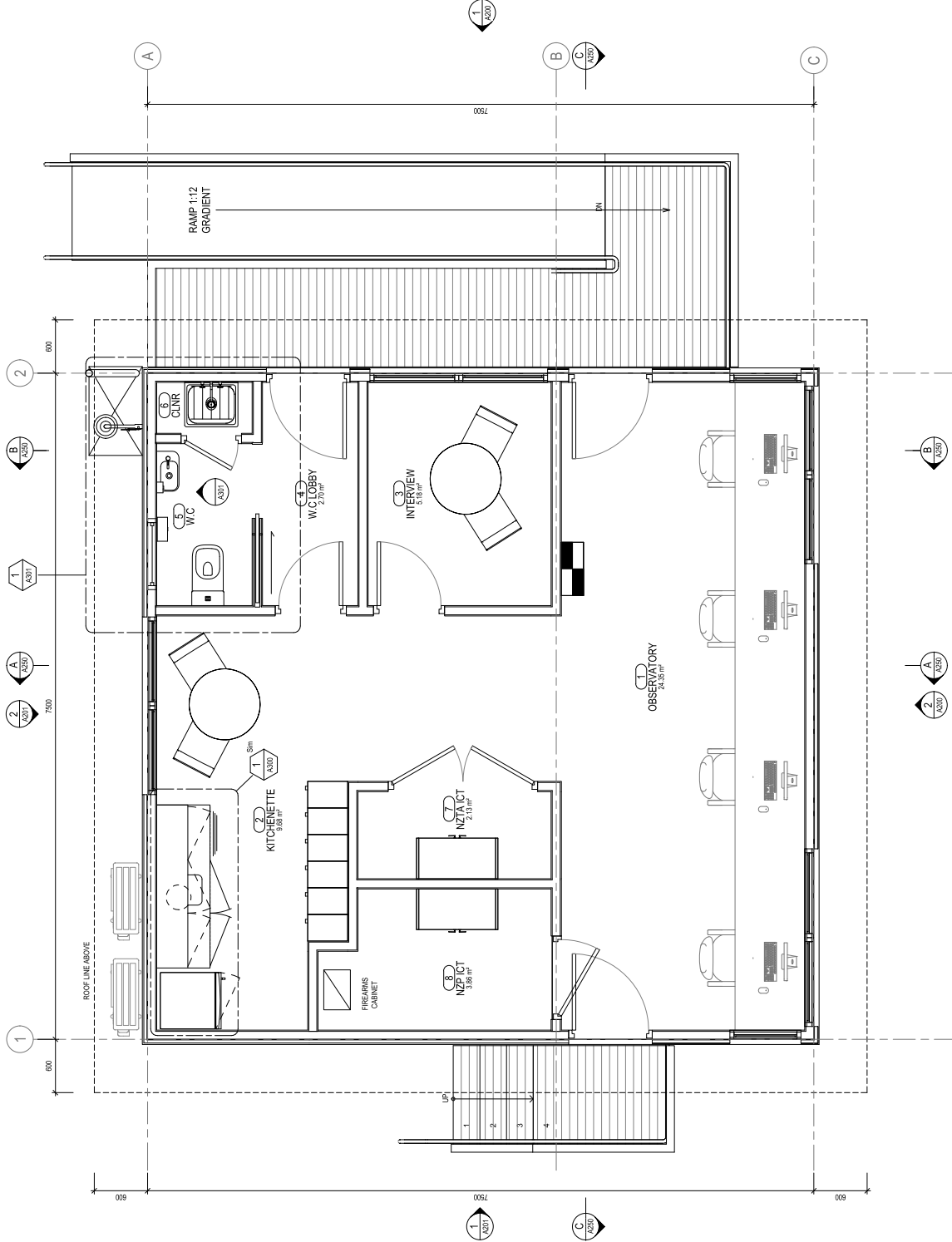
- CENTRELINE OF ELEMENT  
TILE SETOUT POINT  
BUILDING GRID SETOUT POINT  
LEVEL DATUM - TOP OF ELEMENT  
LEVEL DATUM - BOTTOM OF ELEMENT  
PLAN SPOT RELATIVE LEVEL  
FALL DIRECTION SYMBOL  
CEILING TAG  
DOOR TAG  
ROOM NUMBER  
WALL TYPE TAG  
WINDOW TAG  
CURTAIN WALL TAG  
CONCRETE FLOOR SLAB SETDOWN  
CONCRETE FLOOR SLAB EDGE  
PLAN - FINISH FLOOR LEVEL  
PLAN - SLAB LEVEL

EXTERNAL COLOUR SELECTIONS:

- CORRUGATE CLADDING: DULUX 'GREY FRANKS'  
HARDEFLEX CLADDING: RESINE 'GREEN LEAF'  
CLADDING: 'GREY FRANKS'  
FASCIA & BARGE: 'GREY FRANKS' PAINT FINISH  
GUTTERS: 'GREY FRANKS'  
DOWNPIPES: 'GREY FRANKS'  
SOFFIT PAINT COLOUR: 'GREY FRANKS'  
FLASHINGS: POWERCOAT 'GREY FRANKS'  
WINDOW & DOOR JOINERY: BLACK ANODISED ALUMINIUM  
INTERIOR WALLS: 'GREY FRANKS'  
EXTERIOR DECORATIVE STRAIGHTS: RESINE 'WOODSMAN'  
LINED GUM  
INTERIOR PAINT COLOURS: RESINE 'ALABASTER'  
GENERAL WALLS: RESINE 'GREY FRANKS'  
INTERIOR DOOR TRIMS: RESINE 'GREY FRANKS'  
WINDOW TRIMS: RESINE 'ALABASTER'  
SKINTING: RESINE 'ALABASTER'

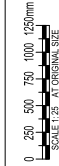
FLOOR FINISHES:

- FLOOR FINISHES:  
INSTALL FLOORING VINYL TO ALL SPACES  
FLOOR FINISHES: RESINE 'ALABASTER'  
COLOUR: 461  
SKINTING TRIM: 90x10 BELLEF PINE F3 SKINTING PROFILE  
WALL PAINT COLOUR: RESINE 'ALABASTER'  
CEILING PAINT COLOUR: RESINE 'ALABASTER'  
INTERIOR TRIM PAINT COLOUR: RESINE 'ALABASTER'



GENERAL ARRANGEMENT PLAN

SCALE 1:25



SCALE 1:25 AS SHOWN ON SITE

GHDWOODHEAD  
creativespaces

Level 1.5, Great Theatre Building  
215 Lambton Quay, Wellington New Zealand  
T 64 4 472 0989 F 64 4 472 0833  
E graham@ghd.com W www.ghd.com

Conditions of Use:  
This drawing is the property of GHD and is to be used only for the purposes for which it was prepared and must not be used by any other person without the written consent of GHD.

DO NOT SCALE

Scale As indicated

TENDER / CONSENT

Client NEW ZEALAND TRANSPORT AGENCY  
Project TIER 1.5 - CONTROL BUILDING  
Title ARCHITECTURAL WORKS  
General Arrangement Plan

Drawing No: 51-34037-A100  
Rev: A

| No | Revision      | Drawn | Date     |
|----|---------------|-------|----------|
| 1  | CONSENT ISSUE | RW    | 05/07/19 |
| A  | CLIENT REVIEW | RW    | WIP      |

**INSULATION NOTES:**

**ROOF:**

- FIBREGLASS BATTS TO ALL CEILING SPACES.
- PINK BATTS R/8 SKILLION ROOF INSULATION SHALL BE INSTALLED TO SKILLION ROOF TO ACHIEVE 20MM AIR GAP BETWEEN INSULATION AND ROOF UNDERLAY

**WALL INSULATION:**

- EXTERIOR WALLS ARE TO BE INSULATED WITH R2.8 PINK BATTS ULTIMA FIBROGLASS BATTS.

**UNDERFLOOR INSULATION:**

- PINK BATTS ULTIMA FIBROGLASS BATTS.
- FITTED BETWEEN FLOOR JOISTS

**ARCHITECTURAL SYMBOLS**

CENTRELINE OF ELEMENT

TILE SETOUT POINT

BUILDING GRID SETOUT POINT

LEVEL DATUM - TOP OF ELEMENT

LEVEL DATUM - BOTTOM OF ELEMENT

PLAN SPOT RELATIVE LEVEL

FALL

FALL DIRECTION SYMBOL

CEILING TAG

DOOR TAG

ROOM NUMBER

WALL TYPE TAG

WINDOW TAG

CURTAIN WALL TAG

CONCRETE FLOOR SLAB SETDOWN

PLAN - FINISH FLOOR LEVEL

PLAN - SLAB LEVEL

**BRACING LEGEND**

BRACING ELEMENT

1/2 BL-HK1-BK BRACING ELEMENT LENGTH

BRACING ELEMENT TYPE

BRACING LINE

LINTEL ABOVE WINDOW

| Number | Name        | Area                 |
|--------|-------------|----------------------|
| 1      | OBSERVATORY | 24.35 m <sup>2</sup> |
| 2      | KITCHENETTE | 1.88 m <sup>2</sup>  |
| 3      | INTERVIEW   | 5.18 m <sup>2</sup>  |
| 4      | W.C. LOBBY  | 2.70 m <sup>2</sup>  |
| 5      | W.C.        | 1.96 m <sup>2</sup>  |
| 6      | CLNR        | 0.02 m <sup>2</sup>  |
| 7      | NZTA CT     | 2.13 m <sup>2</sup>  |
| 8      | NZP CT      | 3.86 m <sup>2</sup>  |

**WALL TYPE LEGEND**

**WALL TYPES:**

**W101 CORRUGATED HORIZONTAL CLADDING**

- 13mm S FANBOARD PLASTERBOARD
- INTERIOR LINING - L4 FINISH
- 40mm CENTRES
- 40mm STUDS @ 400mm CENTRES
- R2.8 ULTRA WALL BATTS
- 13mm S FANBOARD PLASTERBOARD
- INTERIOR LINING - L4 FINISH
- 40mm CENTRES
- 40mm STUDS @ 400mm CENTRES
- 2mm GAUGED STEEL SHEET INSTALLED OVER PLASTERBOARD
- R2.8 ULTRA WALL BATTS
- 7mm ECDPLY R4B - SCREW FIXED
- 4521 R32 TIMBER CAVITY BATTENS
- 4521 R32 TIMBER CAVITY BATTENS
- CORRUGATED PROFILE CLADDING

**W102 CORRUGATED HORIZONTAL CLADDING**

- 13mm S FANBOARD PLASTERBOARD
- INTERIOR LINING - L4 FINISH
- 40mm CENTRES
- 40mm STUDS @ 400mm CENTRES
- R2.8 ULTRA WALL BATTS
- 13mm S FANBOARD PLASTERBOARD
- INTERIOR LINING - L4 FINISH
- 40mm CENTRES
- 40mm STUDS @ 400mm CENTRES
- 2mm GAUGED STEEL SHEET INSTALLED OVER PLASTERBOARD
- R2.8 ULTRA WALL BATTS
- 7mm ECDPLY R4B - SCREW FIXED
- 4521 R32 TIMBER CAVITY BATTENS
- 4521 R32 TIMBER CAVITY BATTENS
- CORRUGATED PROFILE CLADDING

**W103 JAMES HARDIE MURFORD PLASTERBOARD**

- 13mm S FANBOARD PLASTERBOARD
- INTERIOR LINING - L4 FINISH
- 40mm CENTRES
- 40mm STUDS @ 400mm CENTRES
- R2.8 ULTRA WALL BATTS
- 13mm S FANBOARD PLASTERBOARD
- INTERIOR LINING - L4 FINISH
- 40mm CENTRES
- 40mm STUDS @ 400mm CENTRES
- 2mm GAUGED STEEL SHEET INSTALLED OVER PLASTERBOARD
- R2.8 ULTRA WALL BATTS
- 7mm ECDPLY R4B - SCREW FIXED
- 4521 R32 TIMBER CAVITY BATTENS
- 4521 R32 TIMBER CAVITY BATTENS
- CORRUGATED PROFILE CLADDING

**W104 INTERIOR TIMBER WALL**

- 90mm TIMBER FRAMING - STUDS @ 400mm CENTRES
- 13mm GB PLASTERBOARD BOTH SIDES
- L4 FINISH - REFER TO BRACING PLAN FOR LOCATION BRACING UNITS

**W105 INTERIOR TIMBER WALL**

- 90mm TIMBER FRAMING - STUDS @ 400mm CENTRES
- 13mm GB PLASTERBOARD BOTH SIDES
- L4 FINISH - REFER TO BRACING PLAN FOR LOCATION BRACING UNITS

**FLOOR LEGEND**

**FLOOR TYPES:**

**F101 TIMBER FLOOR**

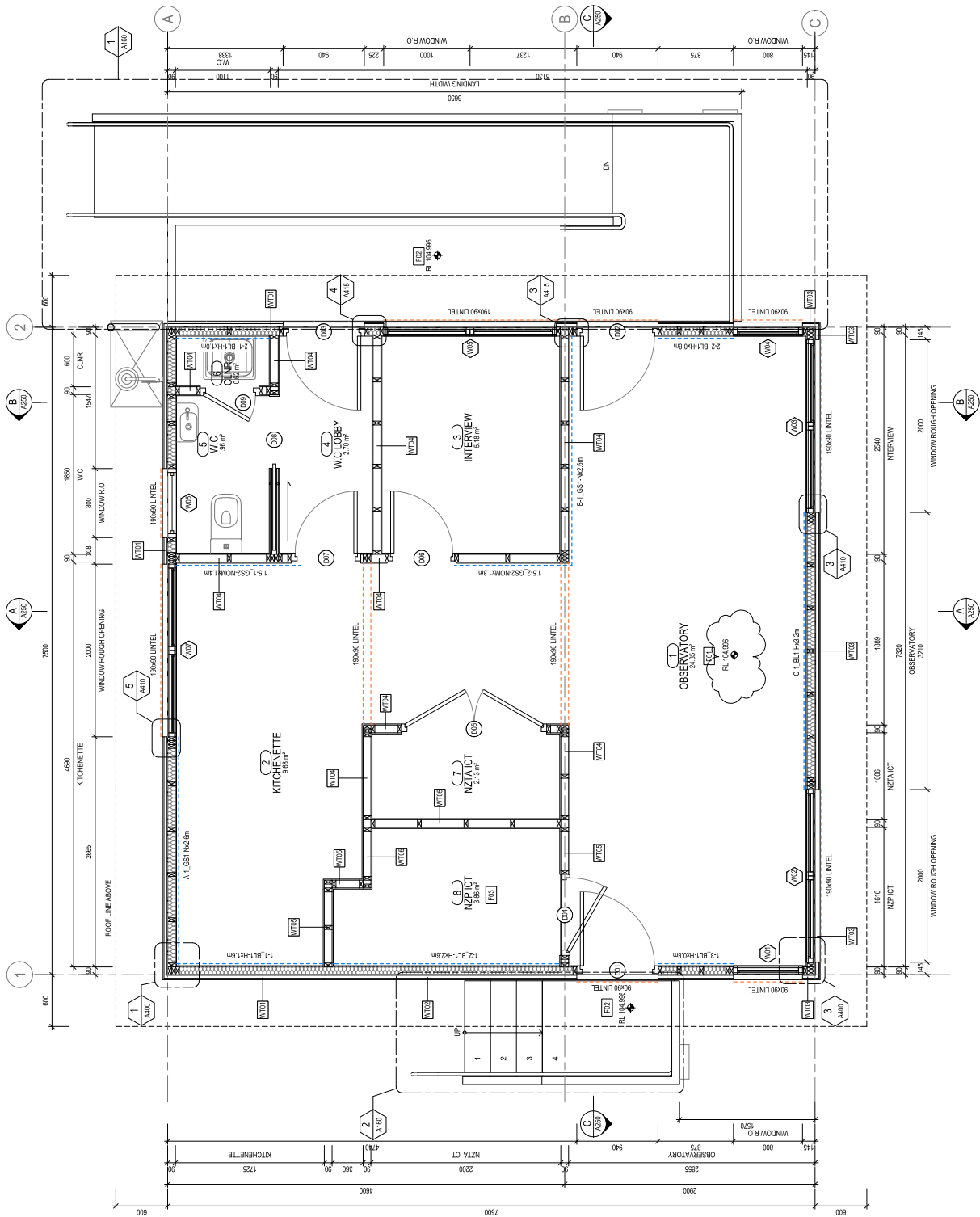
- 19mm PLYWOOD
- 10445mm FLOOR JOISTS AT 450mm CRS
- 13mm S FANBOARD PLASTERBOARD
- 13mm S FANBOARD PLASTERBOARD
- ADJACENT FLOOR JOISTS AT 180mm
- CRS MAX
- 6mm FIBRE CEMENT LINING TO UNDERFLOOR
- INSULATION R1.9
- 6mm FIBRE CEMENT LINING TO UNDERFLOOR
- PROPRIETARY JOINTERS

**F102 TIMBER DECK**

- 19mm PLYWOOD
- 10445mm FLOOR JOISTS AT 450mm CRS
- 13mm S FANBOARD PLASTERBOARD
- 13mm S FANBOARD PLASTERBOARD
- ADJACENT FLOOR JOISTS AT 180mm
- CRS MAX
- 6mm FIBRE CEMENT LINING TO UNDERFLOOR
- INSULATION R1.9
- 6mm FIBRE CEMENT LINING TO UNDERFLOOR
- PROPRIETARY JOINTERS

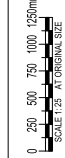
**F103 TIMBER FLOOR**

- 19mm PLYWOOD
- 10445mm FLOOR JOISTS AT 450mm CRS
- 13mm S FANBOARD PLASTERBOARD
- 13mm S FANBOARD PLASTERBOARD
- ADJACENT FLOOR JOISTS AT 180mm
- CRS MAX
- 6mm FIBRE CEMENT LINING TO UNDERFLOOR
- INSULATION R1.9
- 6mm FIBRE CEMENT LINING TO UNDERFLOOR
- PROPRIETARY JOINTERS



**SET OUT PLAN**

SCALE 1:25



**GHDWOODHEAD**

creativespaces

Level 1, Gary Thorpe Building  
215 Lambton Quay, Wellington, New Zealand  
T 64 4 472 0999 F 64 4 472 0833  
E graham@ghd.com W www.ghd.com

Conditions of Use  
GHd's client (and any other person who  
uses the drawings) shall be responsible for  
the drawings and shall not be used for any other  
purpose without the written consent of GHd.  
**DO NOT SCALE**  
Scale As indicated

**TENDER / CONSENT**

Client **NEW ZEALAND TRANSPORT AGENCY**

Project **TIER 1.5 - CONTROL BUILDING**

Title **ARCHITECTURAL WORKS SET OUT PLAN**

Drawing No: **51-34037-A101**

Rev: **A**

CT01

HEIGHT

CT02

HEIGHT

PLASTERBOARD CEILING

- 13mm PLASTERBOARD, L4 FINISH, PAINTED
- 308 RONDO CEILING BATTENS ON GIB Z26 CLIP FIXED TO ROOF FRAMING

SOFT Lining

- 6mm THICK FIBRE CEMENT SHEET
- PROPRIETARY JOINTERS
- 45mm FRAMING
- PAINTED

- REFER TO ELECTRICAL DRAWINGS TO COORDINATE LIGHTING AND FOR INSTALLATION DETAILS
- REFER TO SERVICES DRAWINGS FOR OTHER REQUIREMENTS
- PROVIDE ACCESS PANELS IN CEILING IN ACCORDANCE WITH BUILDING SERVICES REQUIREMENTS
- CEILING PLANS TO BE READ IN CONJUNCTION WITH ELECTRICAL AND MECHANICAL DRAWINGS
- CEILING EQUIPMENT SETTINGS TO ALIGN WHERE POSSIBLE (CENTRE LINES) AND BE SPACED EVENLY BETWEEN ADJACENT WALLS
- PROVIDE ADDITIONAL SUPPORTS IN THE CEILING SUSPENSION SYSTEM WHERE CEILING MOUNTED CURTAIN TRACKS ARE HUNG.
- CEILING MOUNTED EQUIPMENT (LIGHTS/FANS) ARE TO BE SPACED EVENLY BETWEEN ADJACENT WALLS



**NEW ZEALAND TRANSPORT AGENCY  
TIER 1.5 - CONTROL BUILDING  
ARCHITECTURAL WORKS  
REFLECTED CEILING PLAN**

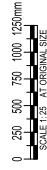
DO NOT SCALE

Scale 1:25

Drawing No: **51-34037-A105**

Rev: 1

Level 1, Grant Thornton House  
215 Lambton Quay, Wellington New Zealand  
T 64 4 472 0799 F 64 4 472 0833  
E [weinfo@ghd.com](mailto:weinfo@ghd.com) W [www.ghd.com](http://www.ghd.com)



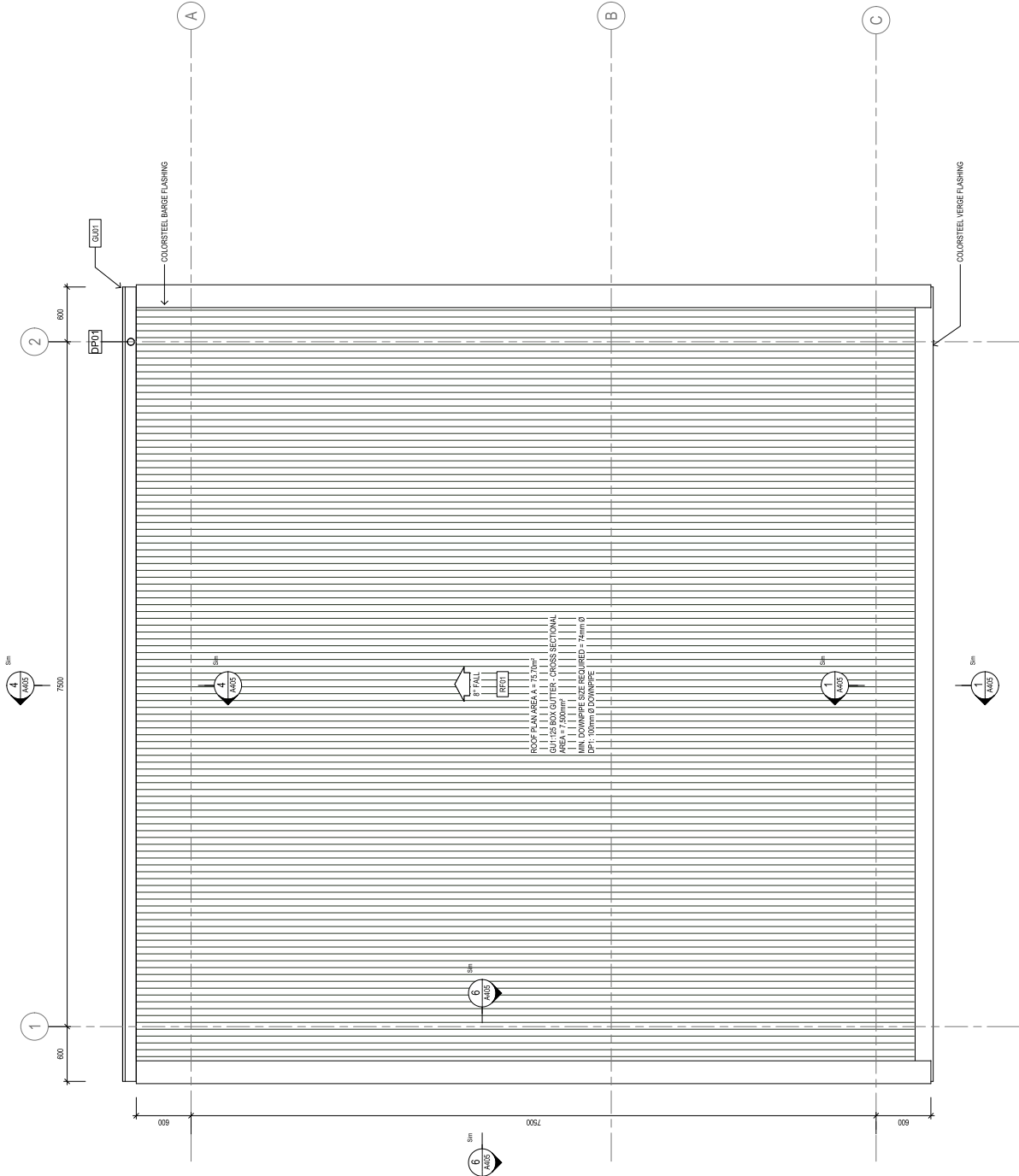
| 1  | CONSENT ISSUE | Drawn | Date |
|----|---------------|-------|------|
| No | Revision      |       |      |

ROOF LEGEND

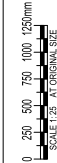
- [RST] **RAINWATER CORRUGATE**
- RAINWATER CORRUGATE 0.55mm ROOFING
  - COLORSTEEL MAXX FINISH - COLOUR
  - ROOF UNDERLAY
  - 70x45mm H3.2 TIMBER PURLING @ 800mm
  - CTRG FIXED ON PLAT
  - 48mm CTRG NOTCHED AT SOFFIT
- [GUT] **STEEL GUTTER - 125 BOX**
- RAINWATER CORRUGATE 0.55mm ROOFING
  - COLORSTEEL MAXX FINISH - COLOUR
  - GREY FRAMES
- [DPT] **100mm D DOWN PIPE**
- RAINWATER CORRUGATE 0.55mm ROOFING
  - FINISH TO MATCH ROOFING

ROOF NOTES

1. ALL OTHER RAINWATER GOODS, FINISHES AND SERVICES TO BE SPECIFIED BY THE CLIENT. REFER TO SERVICES DRAWINGS FOR ALL ROOF SERVICE PENETRATIONS.
2. ALL ROOF PENETRATIONS TO BE APPROPRIATELY SPECIFIED AND REFERRED TO THE CLIENT TO SPECIFICATION FOR DETAILS.
3. CONNECT ALL NEW RAINWATER DOWNPIPES TO EXISTING RAINWATER RETICULATION AND LEGAL POINT OF DISCHARGE.



ROOF PLAN  
SCALE 1:25



**GHDWOODHEAD**  
creativespaces

Level 1.5, Quay Theatre  
215 Lambton Quay, Wellington, New Zealand  
T 64 4 472 0999 F 64 4 472 0833  
E graham@ghd.com W www.ghd.com

TENDER / CONSENT

Client **NEW ZEALAND TRANSPORT AGENCY**  
Project **TIER 1.5 - CONTROL BUILDING**  
Title **ARCHITECTURAL WORKS**  
Drawing No: **51-34037-A110**

Conditions of Use:  
This drawing is the property of GHD and is to be used only for the purpose for which it was prepared and must not be used for any other purpose without the written consent of GHD.

**DO NOT SCALE**

Scale As indicated

Rev: 1

Cad File No: G:\134837\CAD\Rev\TIER 1.5 RAINWATER\1.5 RAINWATER.dwg

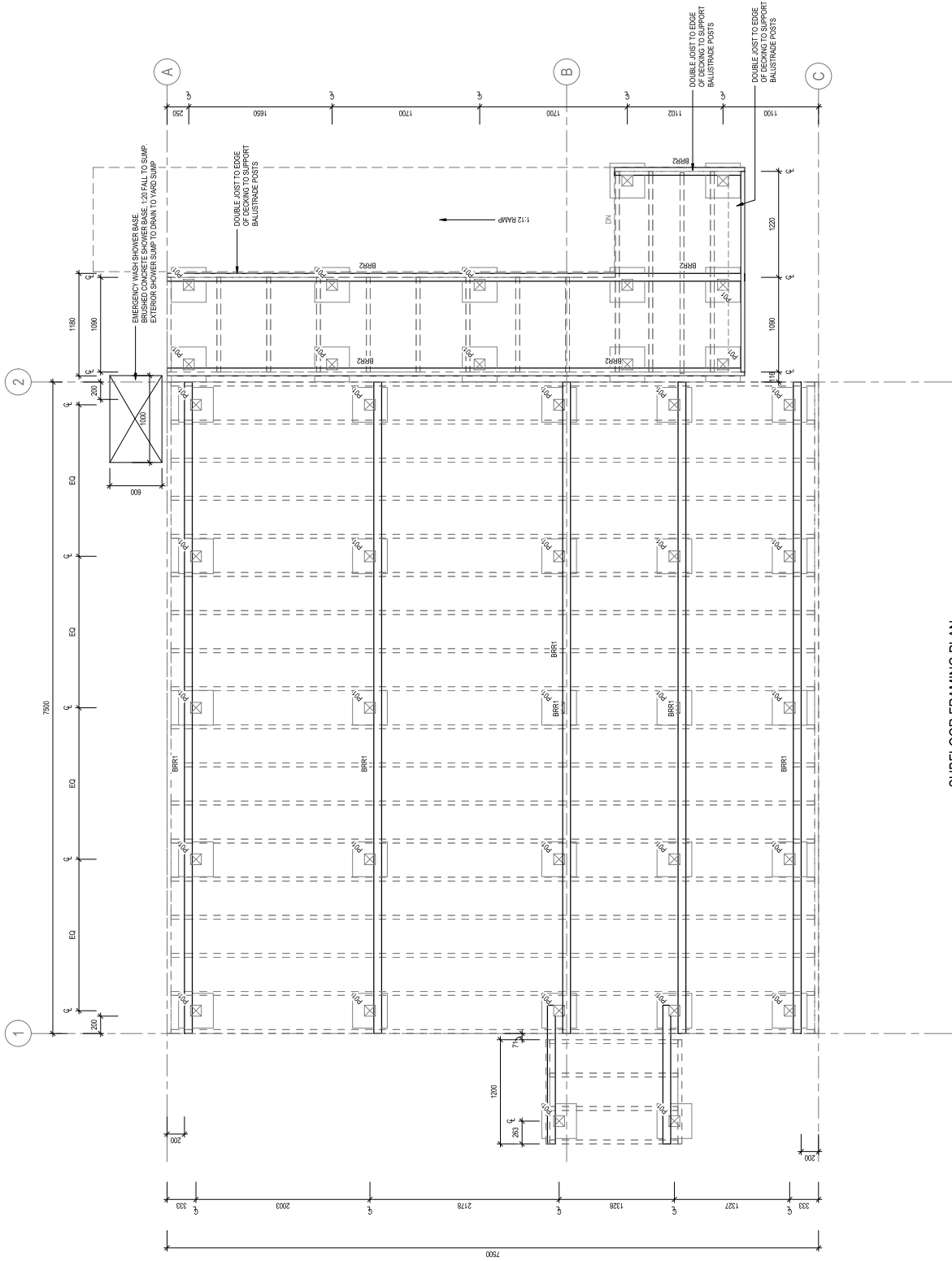
Plot Date: 15/10/2019 10:37:38

SUB FLOOR FRAMING & FOOTING LEGEND

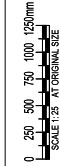
GROUND BEARING CAPACITY  
BEARING CAPACITY OF THE SOIL MUST MEET 10000  
GROUND CONDITIONS AS DEFINED IN NZS3804:2011 AND  
HAVE A ULTIMATE BEARING CAPACITY OF 3000Pa.  
IF THE PILE HOLE FAIL TO MEET THE 3000Pa ULTIMATE  
BEARING CAPACITY THE PILE MUST BE NOTIFIED  
AND A SPECIFIC ENGINEERING DESIGN WILL BE  
REQUIRED.

- TIMBER PILE**  
• 125x125mm H5 TIMBER PILE IN 400x400,  
900mm DEEP CONCRETE FOOTING
- TIMBER BEAMER - PROTECTED**  
• 140x60mm SS PILE TIMBER BEAMER
- TIMBER BEAMER - EXPOSED**  
• 140x60mm SS PILE TIMBER BEAMER

NOTE: THE TIMBER BEAMERS SHALL BE FIXED TO EACH  
TIMBER PILE WITH 1/1M2 SS BOLT WITH 50x50x4mm SS  
WASHER BEAMER TO BE OVER 300mm TO GROUND



SUBFLOOR FRAMING PLAN  
SCALE 1:25



GHDWOODHEAD  
creativespaces

Level 1, Gary Thomas Building  
215 Junction Quay, Wellington, New Zealand  
T 64 4 472 0989 F 64 4 472 0833  
E graham@ghd.com W www.ghd.com

TENDER / CONSENT

Client NEW ZEALAND TRANSPORT AGENCY  
Project TIER 1.5 - CONTROL BUILDING  
Title ARCHITECTURAL WORKS  
SUBFLOOR FRAMING PLAN

Original Size: A1 Drawing No: 51-34037-A120 Rev: 1

Conditions of Use  
This drawing is to be used only for the project  
GHD's client (and any other person who  
has been authorised by GHD to use it) and  
must not be used for any other purpose  
without the written consent of GHD.

DO NOT SCALE

Scale As indicated

Consent Issue

1 05.07.19

DA 05.07.19

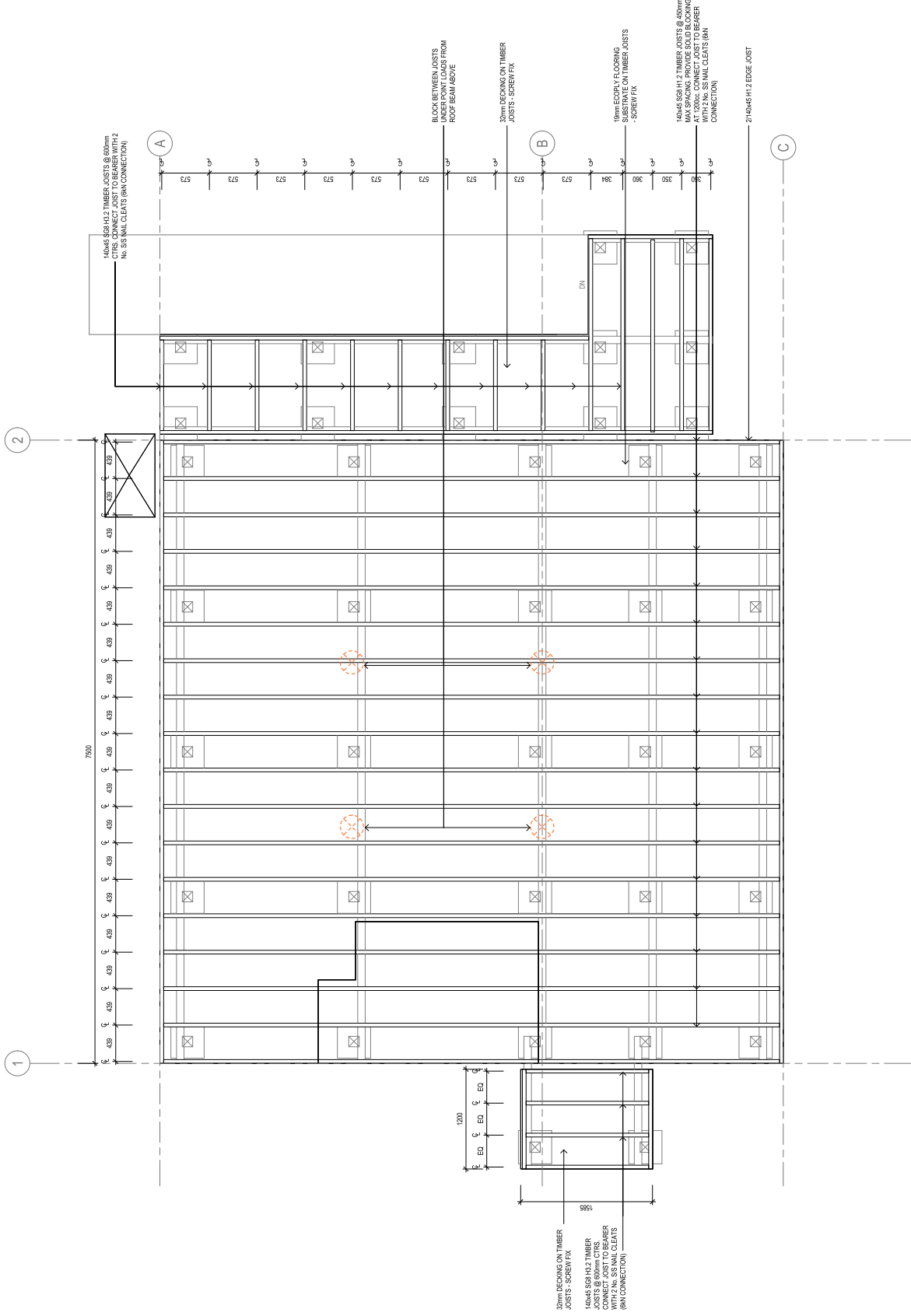
Drawn Date

1 05.07.19

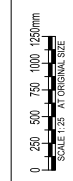
DA 05.07.19

Plot Date: 15/10/2019 10:37:38

Cad File No: G:\134037\CAD\Rev\TIER 1.5 RAKAM\TIER 1.5 RAKAM.dwg



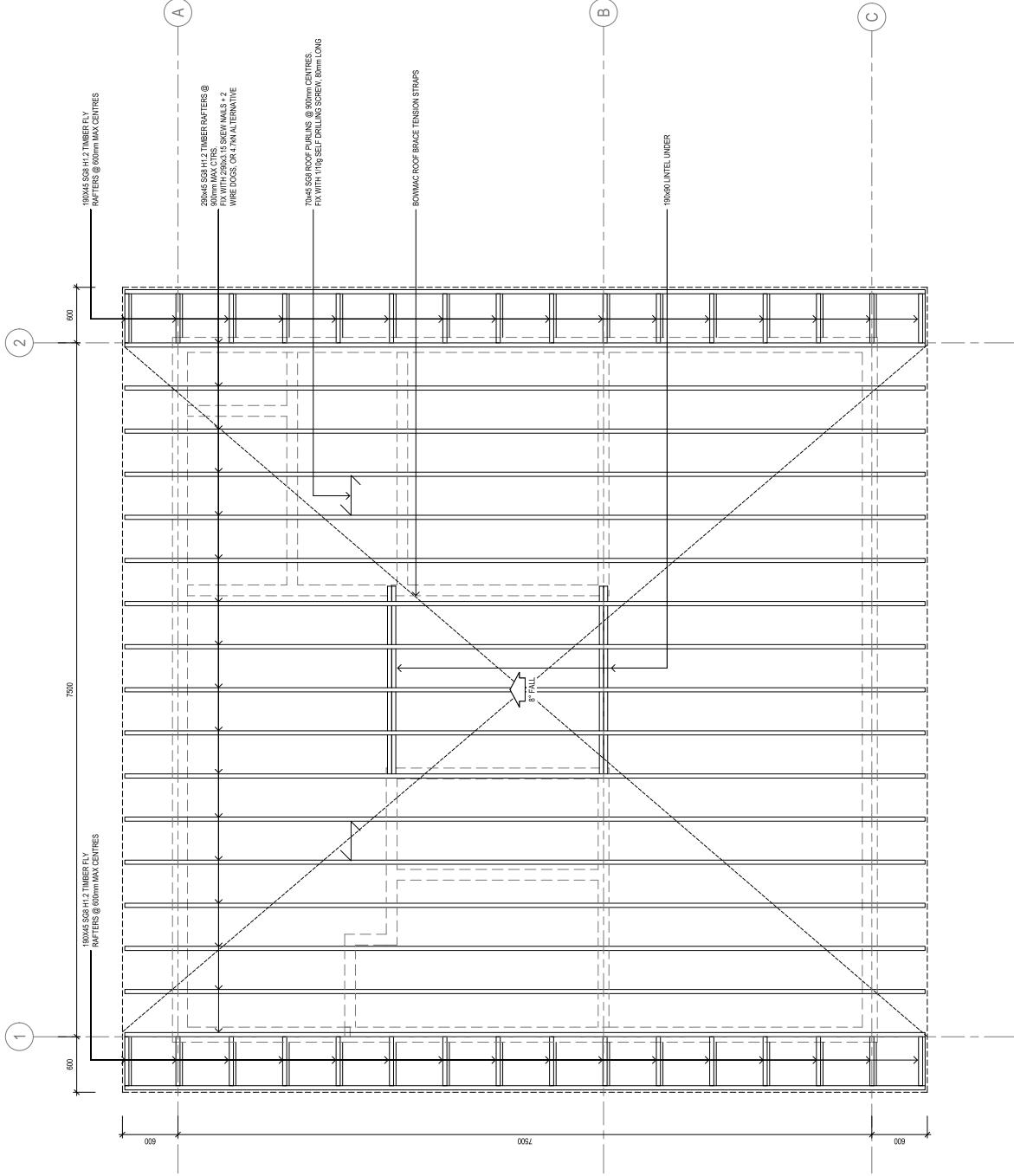
FLOOR FRAMING PLAN  
SCALE 1:25



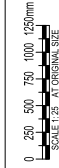
**GHDWOODHEAD**  
creativespaces  
Level 1.5, Quay Theatre  
215 Junction Quay, Wellington, New Zealand  
T 64 4 472 0989 F 64 4 472 0833  
E ghd@ghd.com W www.ghd.com

**TENDER / CONSENT**  
Client: NEW ZEALAND TRANSPORT AGENCY  
Project: TIER 1.5 - CONTROL BUILDING  
Title: ARCHITECTURAL WORKS  
Drawing No: 51-34037-A121  
Rev: 1

Conditions of Use:  
This drawing is to be used only for the project and site for which it was prepared and must not be used for any other purpose without the written consent of GHD.  
**DO NOT SCALE**  
Scale: 1:25



ROOF FRAMING PLAN  
SCALE 1:25



**GHDWOODHEAD**  
creativespaces  
Level 1, Quay Theatre  
215 Junction Quay, Wellington, New Zealand  
T 64 4 472 0789 F 64 4 472 0833  
E ghd@gdh.com W www.gdh.com

TENDER / CONSENT

Client **NEW ZEALAND TRANSPORT AGENCY**  
Project **TIER 1.5 - CONTROL BUILDING**  
Title **ARCHITECTURAL WORKS  
ROOF FRAMING PLAN**

Drawing No: **51-34037-A122**  
Rev: **1**

Conditions of Use:  
This drawing is to be used only for the project and site for which it was prepared and must not be used for any other purpose without the written consent of GHD.  
**DO NOT SCALE**  
Scale 1:25

| No | Revision      | Drawn | Date     |
|----|---------------|-------|----------|
| 1  | CONSENT ISSUE |       | 05.07.19 |



WALL TYPE LEGEND

WALL TYPES:

- W101**
- CORRUGATED HORIZONTAL CLADDING
  - 13mm STANDARD PLASTERBOARD
  - 7mm EOPPLY R4B - SCREW FIXED
  - 90mm TIMBER FRAMING - STUDS @ 400mm CENTRES
  - R23 ULTRA WALL BATTIS
  - 45/21 H&Z TIMBER CAVITY BATTENS
  - CORRUGATED PROFILE CLADDING
- W102**
- CORRUGATED HORIZONTAL CLADDING
  - 13mm STANDARD PLASTERBOARD
  - 7mm EOPPLY R4B - SCREW FIXED
  - 90mm TIMBER FRAMING - STUDS @ 400mm CENTRES
  - 2mm GAUGED STEEL SHEET INSTALLED OVER CORRUGATED CLADDING
  - R23 ULTRA WALL BATTIS
  - 45/21 H&Z TIMBER CAVITY BATTENS
  - CORRUGATED PROFILE CLADDING
- W103**
- JAMES HARDIE HARDIEFLEX PANEL
  - INTERIOR LINING - 1/4 FINISH
  - 90mm TIMBER FRAMING - STUDS @ 400mm CENTRES
  - R23 ULTRA WALL BATTIS
  - 45/21 H&Z TIMBER CAVITY BATTENS
  - JAMES HARDIE HARDIEFLEX SHEET CLADDING
- W104**
- INTERIOR TIMBER WALL
  - 90mm TIMBER FRAMING - STUDS @ 400mm CENTRES
  - 13mm GB PLASTERBOARD BOTH SIDES 1/4 FINISH - REFER TO BRACING PLAN FOR LOCATION BRACING UNITS
- W105**
- INTERIOR TIMBER WALL
  - 90mm TIMBER FRAMING - STUDS @ 600mm CENTRES
  - 13mm GB PLASTERBOARD BOTH SIDES 1/4 FINISH - REFER TO BRACING PLAN FOR LOCATION BRACING UNITS
- W106**
- INTERIOR TIMBER WALL
  - 90mm TIMBER FRAMING - STUDS @ 600mm CENTRES
  - 13mm GB PLASTERBOARD BOTH SIDES 1/4 FINISH - REFER TO BRACING PLAN FOR LOCATION BRACING UNITS

ROOF LEGEND

- R101**
- DIMOND CORRUGATE
  - DIMOND CORRUGATE 0.55mm ROOFING
  - COATED STEEL - MAX FINISH - COLOUR: COLOUR
  - 30BT FRAMES
  - ROOF UNDERLAY
  - 22x45mm H&Z TIMBER RAFTERS AT 480mm CTRS
  - 480mm CTRS NOTCHED AT JOIST
- G101**
- STEEL GUTTER - 125 BOX
  - DIMOND BOX 125 GUTTER PROFILE
  - COATED STEEL - MAX FINISH - COLOUR: COLOUR
  - 30BT FRAMES
  - FINISH TO MATCH ROOFING
- D101**
- WALL TO ROOF JUMP
  - DIMOND 0.55mm STEEL
  - FINISH TO MATCH ROOFING

ROOF NOTES

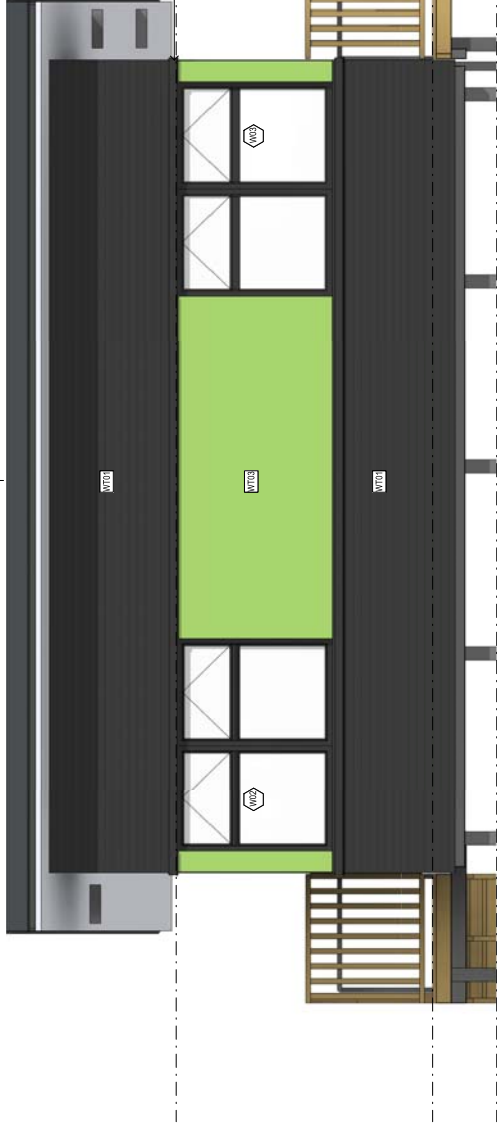
- ALL ROOFING MATERIALS, FLASHINGS AND SEALANTS TO MATCH ROOF SHEETINGS.
- REFER TO SERVICES DRAWINGS FOR ALL ROOF SERVICES PENETRATIONS.
- ROOF PENETRATIONS TO BE APPROPRIATELY FLASHED/SEALED TO RIDGE LINE. REFER TO SPECIFICATION FOR DETAILS.
- STORM WATER DOWNPIPES TO BE DISCHARGED TO STORM WATER RETENTION AND LEGAL POINT OF DISCHARGE.



SIDE ELEVATION 1

SCALE 1:25

R101



FRONT ELEVATION

SCALE 1:25

0 250 500 750 1000 1250mm

SCALE 1:25 - AS SHOWN ON SITE



Level 1.5, Guya Thurston House  
215 Junction Quay, Wellington, New Zealand  
T 64 4 472 0799 F 64 4 722 0833  
E greg@ghd.com W www.ghd.com

GHD WOODHEAD  
creativespaces

DO NOT SCALE  
GHD WOODHEAD is a trading name of  
GHD Pty Ltd.  
This document may only be used by  
GHD's client (and any other person who  
has been authorised by GHD's client  
for the purpose for which it was prepared  
and for any other purpose).

Drawn R. WURSTER  
Checked  
Approved  
Project Director  
Scale As indicated

Designer D. ANGUS  
Design  
Check

Client  
Project  
Title

NEW ZEALAND TRANSPORT AGENCY  
TIER 1.5 - CONTROL BUILDING  
ARCHITECTURAL WORKS  
EXTERIOR ELEVATIONS

Original Date

A1 Drawing No: 51-34037-A200

Rev: 2

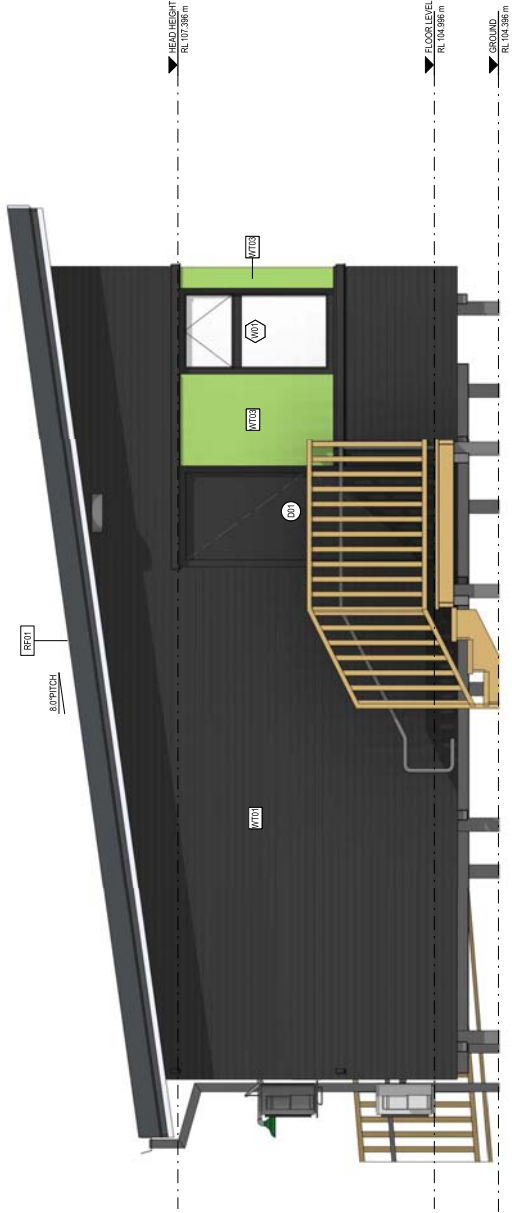
WALL TYPE LEGEND

WALL TYPES:

- W101
- CORRUGATED HORIZONTAL CLADDING
  - 13mm STANDARD PLASTERBOARD
  - 7mm ECPOL R4B - SCREW FIXED
  - 80mm TIMBER FRAMING - STUDS @ 400mm CENTRES
  - R23 ULTRA WALL BATTS
  - 4521 H&Z TIMBER CAVITY BATTENS
  - CORRUGATED PROFILE CLADDING
- W102
- CORRUGATED HORIZONTAL CLADDING
  - 13mm STANDARD PLASTERBOARD
  - 7mm ECPOL R4B - SCREW FIXED
  - 80mm TIMBER FRAMING - STUDS @ 400mm CENTRES
  - 2mm GAUGED STEEL SHEET INSTALLED OVER CORRUGATED CLADDING
  - R23 ULTRA WALL BATTS
  - 7mm ECPOL R4B - SCREW FIXED
  - 80mm TIMBER FRAMING - STUDS @ 400mm CENTRES
  - JAMES HARDIE HARDIFLEX PANEL
  - INTERIOR LINING - 14 FINISH
  - 13mm STANDARD PLASTERBOARD
  - R23 ULTRA WALL BATTS
  - 4521 H&Z TIMBER CAVITY BATTENS
  - JAMES HARDIE HARDIFLEX SHEET CLADDING
- W103
- JAMES HARDIE HARDIFLEX PANEL
  - INTERIOR LINING - 14 FINISH
  - 13mm STANDARD PLASTERBOARD
  - R23 ULTRA WALL BATTS
  - 4521 H&Z TIMBER CAVITY BATTENS
  - JAMES HARDIE HARDIFLEX SHEET CLADDING
- W104
- INTERIOR TIMBER WALL
  - 80mm TIMBER FRAMING - STUDS @ 400mm CENTRES
  - 13mm GB PLASTERBOARD BOTH SIDES 14 FINISH - REFER TO BRACING PLAN FOR LOCATION BRACING UNITS
- W105
- INTERIOR TIMBER WALL
  - 80mm TIMBER FRAMING - STUDS @ 400mm CENTRES
  - 13mm GB PLASTERBOARD BOTH SIDES 14 FINISH - REFER TO BRACING PLAN FOR LOCATION BRACING UNITS

ROOF LEGEND

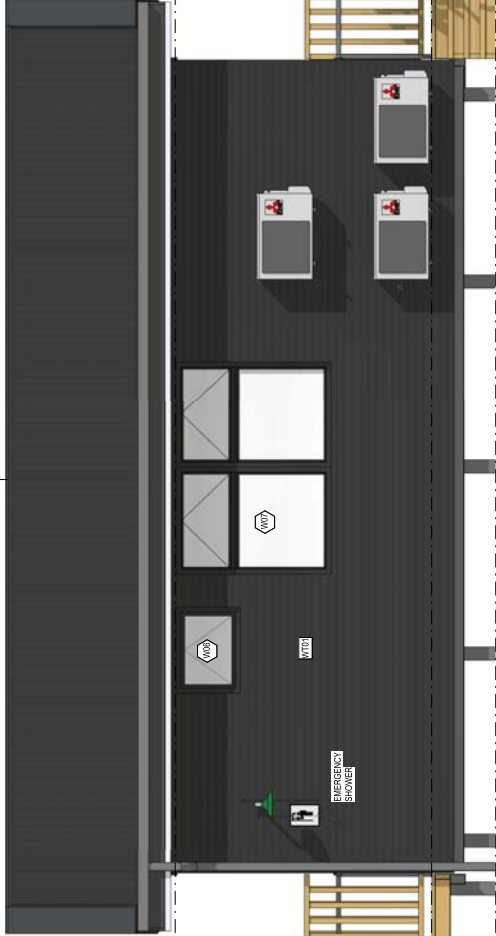
- R001
- DIKWAD CORRUGATE
  - DIKWAD CORRUGATE 0.55mm ROOFING
  - COLORSTEEL MAXX FINISH - COLOUR: GREY FRAMES
  - 70x45mm H&Z TIMBER PURLINS @ 900mm
  - CITS FIXED ON FLAT
  - 48mm CITS NOTCHED AT SLOPE
- G001
- STEEL CUTTER - 125 BOX
  - COLORSTEEL MAXX FINISH - COLOUR: GREY FRAMES
- D001
- 100mm Ø DOWN PIPE
  - DIKWAD 0.55mm STEEL
  - FINISH TO MATCH ROOFING
- ROOF NOTES
- ALL OTHER RAINWATER GOODS: FRINGS AND SEALANTS TO MATCH ROOF SHEETING.
  - ALL RAINWATER GOODS TO BE SPECIFIED FOR ALL ROOF SERVICE PENETRATIONS.
  - ALL ROOF PENETRATIONS TO BE APPROPRIATELY SPECIFIED TO MATCH ROOF SHEETING. REFER TO SPECIFICATION FOR DETAILS.
  - CONNECT ALL NEW RAINWATER DOWNPIPES TO EXISTING RAINWATER RETICULATION AND LEGAL POINT OF DISCHARGE.



SIDE ELEVATION 2

SCALE 1:25

R001



REAR ELEVATION

SCALE 1:25

TENDER / CONSENT

|                              |  |               |                              |  |               |                              |  |
|------------------------------|--|---------------|------------------------------|--|---------------|------------------------------|--|
| NEW ZEALAND TRANSPORT AGENCY |  | Client        | NEW ZEALAND TRANSPORT AGENCY |  | Client        | NEW ZEALAND TRANSPORT AGENCY |  |
| TIER 1.5 - CONTROL BUILDING  |  | Project       | TIER 1.5 - CONTROL BUILDING  |  | Project       | TIER 1.5 - CONTROL BUILDING  |  |
| EXTERIOR ELEVATIONS          |  | Title         | EXTERIOR ELEVATIONS          |  | Title         | EXTERIOR ELEVATIONS          |  |
| A1 Drawing No: 51-34037-A201 |  | Original Date | A1 Drawing No: 51-34037-A201 |  | Original Date | A1 Drawing No: 51-34037-A201 |  |
| Rev: 1                       |  | As Indicated  | Rev: 1                       |  | As Indicated  | Rev: 1                       |  |

GHDWOODHEAD  
creativespaces

Louise L. Gray, Thomas Hines  
215 Junction Quay, Wellington, New Zealand  
T 64 4 472 0799 F 64 4 472 0833  
E lgray@ghd.com W www.ghd.com

NZTRANSPORT  
AGENCY  
WAKA MATARE

0 250 500 750 1000 1250mm  
SCALE 1:25

1. CONSENT ISSUE  
No Revision Note: \* indicates signatures on original issue of drawing or last revision of drawing

05.07.19  
Project Manager  
Date



Original Size  
A1  
Drawing No: 51-34037-A250  
Rev: 1

Plot Date: 15/10/2019 10:37:51

**GENERAL:**  
It is the joiners responsibility to setout and build in all kitchen appliances. Appliances are to be fitted in accordance with the manufacturers recommendations with all venting spaces, clearances etc.  
All appliances shall be supplied by owner. Owner to confirm appliance models prior to kitchen joinery fabrication commencing.

All drawers to have full height sides and backs with soft closing system. Drawer slider tracks to be sized to suit drawer width/weight

**Site measure** prior to manufacturing of joinery, coordinate joinery with the setout of new and relocated appliances

to be brought to designers attention. Units and tops are to be a neat fit between walls. Allow to template bench top to ensure an accurate fit.

Carcasses are to be 18mm standard white Melteca in general. All carcasses are to be clashed with 3mm PVC where not visible. Concealed shelves are to be 18mm white Melteca with 3mm PVC all sides except where noted otherwise.

**L1 JOINERY FINISH - TYPE 1**  
18mm MDF(MR) Bestwood Melamine, colour CARBON EMBOSSED

**L2 JOINERY FINISH - TYPE 2**  
18mm MDF (MR) Bestwood Melamine - colour RANFURI Y OAK WIL

Brushed stainless steel sheet mounted on 18mm MDF(MR)

**KITCHEN SINK**  
Burns & Fernal BFD390R15 Single Bowl sink (Bowl size 390x400x200) at  
integrate into Stainless Steel benchtop

**HT** **HANDLES • BASE CADINETS & DRAWERS**  
Mardeco Arezzo 4030, 264mm x 8.5mm Black

**H2** HANDLES - OVER HEAD CABINETS  
Marlecco Arrezzo 4030 264mm x 85mm Black

**SPLASHBACK**  
10mm Tonnard glass carblock

**Methvan Minimalist goose neck sink mixer tap**



## SCALE 1:10

**NEW ZEALAND TRANSPORT AGENCY  
TIER 1.5 - CONTROL BUILDING  
ARCHITECTURAL WORKS  
KITCHEN JOINERY SETOUT**

Original Size  
Drawing No: **51-34037-A300**  
**A1**  
Rev: **1**

As indicated  
This Drawing must not be  
used for Construction unless  
signed as Approved

**GHDWOODHEAD**  
creativespaces

Level 1, Grant Thornton House  
215 Lambton Quay, Wellington New Zealand  
T 64 4 472 0799 F 64 4 472 0833  
E [we@gnmail.co.nz](mailto:we@gnmail.co.nz) W [www.gnd.com](http://www.gnd.com)

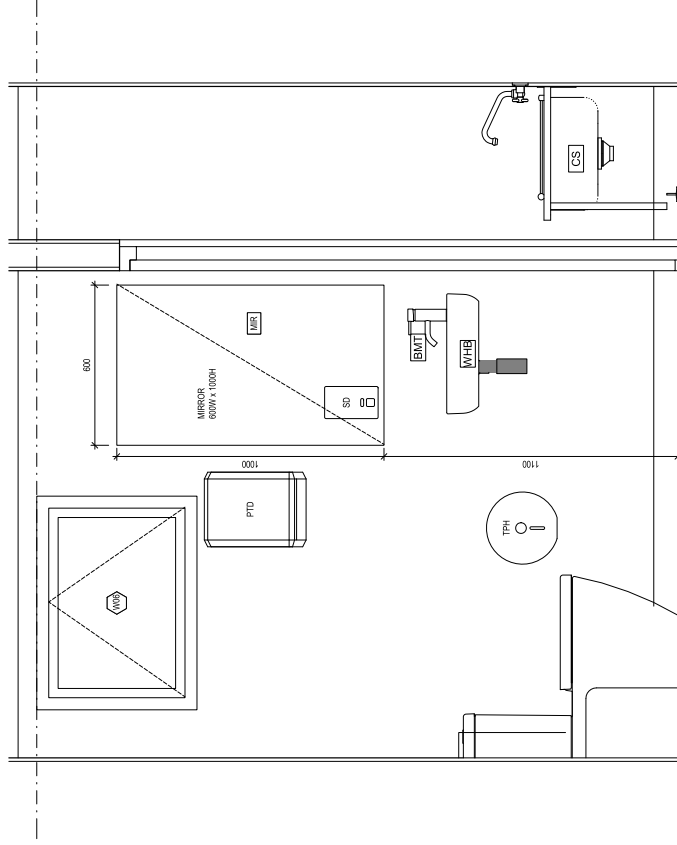


| 1  | CONSENT ISSUE | RW      | DA                                                                            | NW          | 05.07.19         |
|----|---------------|---------|-------------------------------------------------------------------------------|-------------|------------------|
| No | Revision      | Note: * | Indicates signatures on original issue of drawing or last revision of drawing | Job Manager | Project Director |
|    |               |         |                                                                               |             |                  |

Card File No: S-61513037CADD001Rev0101 1 SITER: 1.5 BAKAM.rvt  
 Plot Date: 15/01/2019 10:37:53

Cad File No: G:\5134037\CADD\Rev\TIER 1.5\TIER 1.5 RAKAJA.MXD

Plot Date: 15/10/2019 10:37:53



**SCALE 1 : 10**

SCALE 1:10

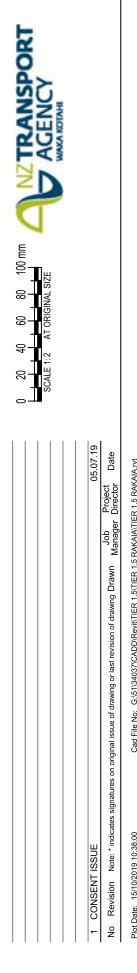
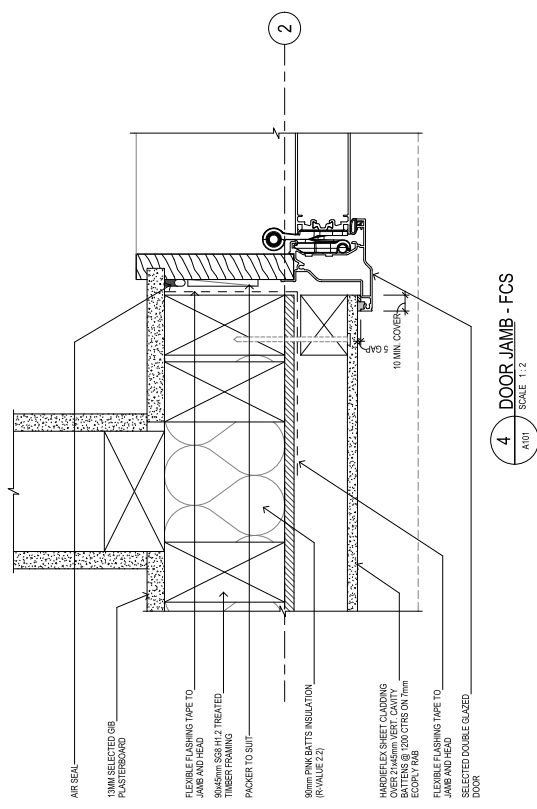
|    |                                                                                                   |
|----|---------------------------------------------------------------------------------------------------|
| WC | WC PAN, CAROMA CUBE WALL FACED TOILET SUITE WITH<br>SOFT CLOSE SEAT, PRODUCT CODE: R4B615W        |
| WB | BASIN, CAROMA LUNA HAND WASH BASIN, BLACK/BOTTLE TRAP                                             |
| BM | BASIN MIXER, CAROMA LUNA BASIN MIXER                                                              |
| CS | CLEANERS SINK, FRANKS 80 C CLEANERS SINK WITH<br>SPRAYBACK AND BUCKET GRID, CODE: FRCS            |
|    | CLEANERS SINK TAPWARE, ZURN SINK FAUCET FOR<br>CLEANERS SINK, MODEL: ZURN 2-5F                    |
| SH | EXTERIOR SHOWER, SPEWMAN PEESAWER WALL MOUNTED<br>EXTERIOR SHOWER, SHOWER HEAD WITH PULL DOWN ARM |

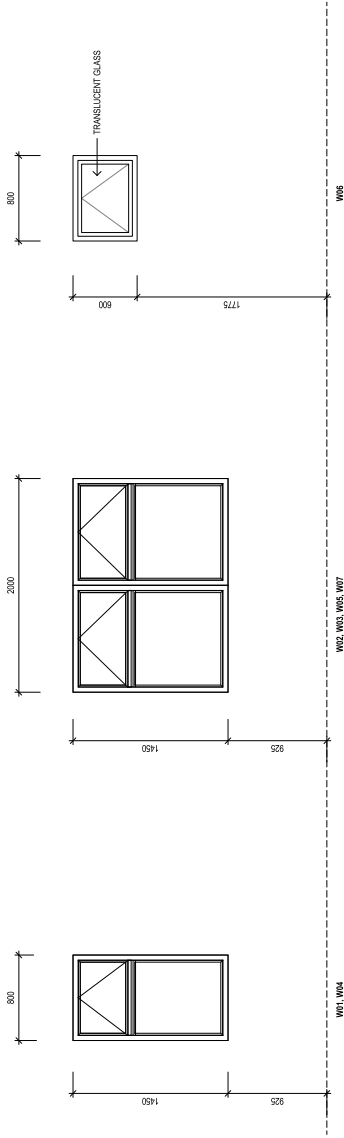
|     |                                                                         |
|-----|-------------------------------------------------------------------------|
| MIR | MIRROR - 1000mmH x 600mmW FRAMELESS MIRROR - GLUE FIXED TO WALL SURFACE |
| TPH | TOILET ROLL HOLDER - SINGLE JUMBO DISPENSER TOWEL, IVVA-COTTONSOFT      |
| SD  | SOAP DISPENSER - AS1 SURFACE MOUNTED VERTICAL SOAP DISPENSER            |
| PTD | PAPER TOWEL DISPENSER - AS1 SURFACE MOUNTED PAPER TOWEL DISPENSER       |





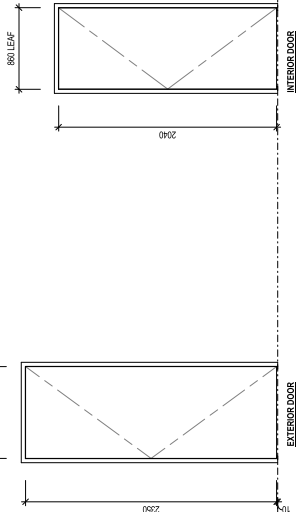






- NULOOK SOVERIGN SERIES- FLUSH SASH
  - FINING OPENING SASHES
  - CONSERVATION CHANNELS TO ALL SILL MEMBERS
  - SAFELITE PBL LAMINATED SAFETY GLAZING
  - BLACK ANODISED FINISH
  - HARDWARE- PROVIDE SS SCISSOR ARM SECURITY STAY & WEDGELESS WINDOW HANDLES
  - GLAZING DOUBLE GLAZED UNIT
  - EXTERIOR GLASS UNIT: 11.5mm ANTI-BANDIT LAMINATED GLASS
  - INTERIOR GLASS UNIT: 6.7mm LAMINATED GLASS
- NULOOK SOVERIGN SERIES- FLUSH SASH
  - FINING OPENING SASHES
  - CONSERVATION CHANNELS TO ALL SILL MEMBERS
  - SAFELITE PBL LAMINATED SAFETY GLAZING
  - BLACK ANODISED FINISH
  - HARDWARE- PROVIDE SS SCISSOR ARM SECURITY STAY & WEDGELESS WINDOW HANDLES
  - GLAZING DOUBLE GLAZED UNIT
  - EXTERIOR GLASS UNIT: 11.5mm ANTI-BANDIT LAMINATED GLASS
  - INTERIOR GLASS UNIT: 6.7mm LAMINATED GLASS
- NULOOK SOVERIGN SERIES- FLUSH SASH
  - FINING OPENING SASHES
  - CONSERVATION CHANNELS TO ALL SILL MEMBERS
  - SAFELITE PBL LAMINATED SAFETY GLAZING
  - BLACK ANODISED FINISH
  - HARDWARE- PROVIDE SS SCISSOR ARM SECURITY STAY & WEDGELESS WINDOW HANDLES
  - GLAZING DOUBLE GLAZED UNIT
  - EXTERIOR GLASS UNIT: 11.5mm ANTI-BANDIT LAMINATED GLASS
  - INTERIOR GLASS UNIT: 6.7mm LAMINATED GLASS

- INTERIOR SOLID CORE DOOR
  - CLEAR GLASS VISION PANEL
  - PAINT FINISH- RESINE GREY FRAMES
  - EMERGENCY RELEASE
  - HARDWARE- REFER TO SPECIFICATION
  - 200mm HIGH SS KICKPLATES EITHER SIDE
- INTERIOR SOLID CORE DOOR
  - CLEAR GLASS VISION PANEL
  - PAINT FINISH- RESINE GREY FRAMES
  - EMERGENCY RELEASE
  - HARDWARE- REFER TO SPECIFICATION
  - 200mm HIGH SS KICKPLATES EITHER SIDE
- INTERIOR SOLID CORE DOOR
  - CLEAR GLASS VISION PANEL
  - PAINT FINISH- RESINE GREY FRAMES
  - EMERGENCY RELEASE
  - HARDWARE- REFER TO SPECIFICATION
  - 200mm HIGH SS KICKPLATES EITHER SIDE



- INTERIOR SOLID CORE DOOR
  - CLEAR GLASS VISION PANEL
  - PAINT FINISH- RESINE GREY FRAMES
  - EMERGENCY RELEASE
  - HARDWARE- REFER TO SPECIFICATION
  - 200mm HIGH SS KICKPLATES EITHER SIDE
- INTERIOR SOLID CORE DOOR
  - CLEAR GLASS VISION PANEL
  - PAINT FINISH- RESINE GREY FRAMES
  - EMERGENCY RELEASE
  - HARDWARE- REFER TO SPECIFICATION
  - 200mm HIGH SS KICKPLATES EITHER SIDE
- INTERIOR SOLID CORE DOOR
  - CLEAR GLASS VISION PANEL
  - PAINT FINISH- RESINE GREY FRAMES
  - EMERGENCY RELEASE
  - HARDWARE- REFER TO SPECIFICATION
  - 200mm HIGH SS KICKPLATES EITHER SIDE

- CS FOR DOORS POWDERSEAL EAS/OPEN CAVITY
  - PAINT FINISH- RESINE GREY FRAMES
  - HARDWARE- CL 100 LAM LOCK WITH TURN AND EMERGENCY RELEASE
  - HARDWARE- REFER TO SPECIFICATION
  - 20mm UNDERCUT TO DOOR LEAF
- CS FOR DOORS POWDERSEAL EAS/OPEN CAVITY
  - PAINT FINISH- RESINE GREY FRAMES
  - HARDWARE- CL 100 LAM LOCK WITH TURN AND EMERGENCY RELEASE
  - HARDWARE- REFER TO SPECIFICATION
  - 20mm UNDERCUT TO DOOR LEAF
- CS FOR DOORS POWDERSEAL EAS/OPEN CAVITY
  - PAINT FINISH- RESINE GREY FRAMES
  - HARDWARE- CL 100 LAM LOCK WITH TURN AND EMERGENCY RELEASE
  - HARDWARE- REFER TO SPECIFICATION
  - 20mm UNDERCUT TO DOOR LEAF

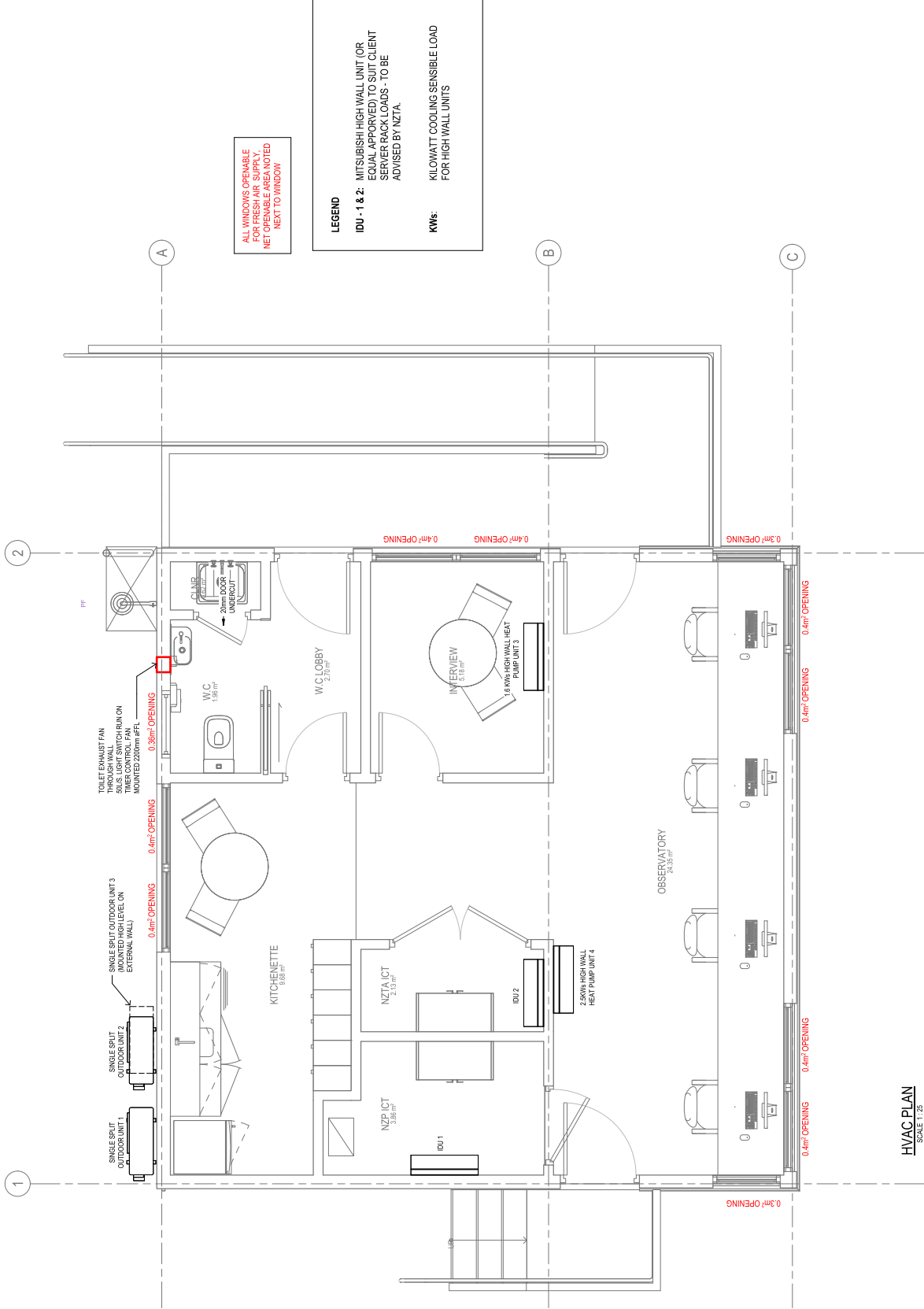
- INTERIOR SOLID CORE DOOR
  - CLEAR GLASS VISION PANEL
  - PAINT FINISH- RESINE GREY FRAMES
  - EMERGENCY RELEASE
  - HARDWARE- REFER TO SPECIFICATION
  - 200mm HIGH SS KICKPLATES EITHER SIDE
- INTERIOR SOLID CORE DOOR
  - CLEAR GLASS VISION PANEL
  - PAINT FINISH- RESINE GREY FRAMES
  - EMERGENCY RELEASE
  - HARDWARE- REFER TO SPECIFICATION
  - 200mm HIGH SS KICKPLATES EITHER SIDE
- INTERIOR SOLID CORE DOOR
  - CLEAR GLASS VISION PANEL
  - PAINT FINISH- RESINE GREY FRAMES
  - EMERGENCY RELEASE
  - HARDWARE- REFER TO SPECIFICATION
  - 200mm HIGH SS KICKPLATES EITHER SIDE

- INTERIOR SOLID CORE DOOR
  - CLEAR GLASS VISION PANEL
  - PAINT FINISH- RESINE GREY FRAMES
  - EMERGENCY RELEASE
  - HARDWARE- REFER TO SPECIFICATION
  - 200mm HIGH SS KICKPLATES EITHER SIDE
- INTERIOR SOLID CORE DOOR
  - CLEAR GLASS VISION PANEL
  - PAINT FINISH- RESINE GREY FRAMES
  - EMERGENCY RELEASE
  - HARDWARE- REFER TO SPECIFICATION
  - 200mm HIGH SS KICKPLATES EITHER SIDE
- INTERIOR SOLID CORE DOOR
  - CLEAR GLASS VISION PANEL
  - PAINT FINISH- RESINE GREY FRAMES
  - EMERGENCY RELEASE
  - HARDWARE- REFER TO SPECIFICATION
  - 200mm HIGH SS KICKPLATES EITHER SIDE

## TENDER / CONSENT

|                                                                                                                                                                                                                                                                                                                                        |  |  |  |                                      |  |  |  |
|----------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|--|--|--|--------------------------------------|--|--|--|
| DO NOT SCALE                                                                                                                                                                                                                                                                                                                           |  |  |  | TENDER / CONSENT                     |  |  |  |
| GHDWOODHEAD is a trading name of GHD Pty Ltd. This document may only be used by GHD's client (and any other person who has been authorised by GHD's client for the purpose for which it was prepared) and may not be reproduced, stored in a retrieval system or used for any other purpose without the prior written approval of GHD. |  |  |  | Client: NEW ZEALAND TRANSPORT AGENCY |  |  |  |
| Drawn: R. WURSTER                                                                                                                                                                                                                                                                                                                      |  |  |  | Project: TIER 1.5 - CONTROL BUILDING |  |  |  |
| Checked: [Signature]                                                                                                                                                                                                                                                                                                                   |  |  |  | Title: ARCHITECTURAL WORKS           |  |  |  |
| Approved: [Signature]                                                                                                                                                                                                                                                                                                                  |  |  |  | Drawing No: 51-34037-A500            |  |  |  |
| Scale: 1:25                                                                                                                                                                                                                                                                                                                            |  |  |  | Rev: 1                               |  |  |  |

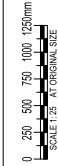




ALL WINDOWS OPENABLE  
DOORS ARE OPENABLE  
NET OPENABLE AREA NOTED  
NEXT TO WINDOW

**LEGEND**  
**IDU - 1 & 2:** MITSUBISHI HIGH WALL UNIT (OR  
EQUAL APPROVED) TO SUIT CLIENT  
SERVER RACK LOADS - TO BE  
ADVISED BY NZTA.  
**KW's:** KILOWATT COOLING SENSIBLE LOAD  
FOR HIGH WALL UNITS

**HVAC PLAN**  
SCALE 1:25



**GHDWOODHEAD**  
creativespaces

Level 1.5, Gray Thornton & Partners  
215 Junction Quay, Wellington, New Zealand  
T 64 4 472 0999 F 64 4 472 0833  
E graham@ghd.com W www.ghd.com

**TENDER / CONSENT**

Client **NEW ZEALAND TRANSPORT AGENCY**  
Project **TIER 1.5 - CONTROL BUILDING**  
Title

**MECHANICAL LAYOUT PLAN**

Drawing No: **51-34037-A801**  
Rev: **1**

Conditions of Use:  
This drawing is to be used only for the project and site  
GHD's client (and any other person who  
uses this drawing) is to be responsible for  
ensuring that the drawing is used for the  
purpose for which it was prepared  
and must not be used for any other  
purpose without the written consent of GHD

**DO NOT SCALE**

Scale 1:25

| No | Revision      | Drawn | Date     |
|----|---------------|-------|----------|
| 1  | CONSENT ISSUE | EB    | 05.07.19 |

NOTES:

1. LARGES WITH NZTA TO ESTABLISH A BOARD DB-1.5. SUPPLY AND FEEDER CABLE WHERE NECESSARY FROM MAIN SUPPLY TO SUPPLY TERMINATION TO BE CO-ORDINATED ON SITE.
2. EXACT LOCATION AND POWER SUPPLY TERMINATION TO BE CO-ORDINATED ON SITE.
3. FOR EXTRACT FAN AT HIGH LEVEL, LIGHTING SWITCHES FOR LIGHTING, GATES, ALARMS ETC. LARGES WITH CIVIL DESIGNER FOR EXACT LOCATION OF TERMINATION TO BE CO-ORDINATED ON SITE.

POWER

- DISTRIBUTION BOARD (DB)
- CONTROL PANEL
- 2
- 4
- CL
- CIRCUIT LINE
- POWER SUPPLY FOR EXTRACT FAN
- POWER SUPPLY FOR FRIDGE
- POWER SUPPLY FOR HOT WATER BOILER
- POWER SUPPLY FOR HOT WATER CYLINDER
- POWER SUPPLY FOR HEAT PUMP OUTDOOR UNIT
- POWER SUPPLY FOR SERVER

COMMUNICATIONS

- COMMUNICATIONS OUTLET FOR DATA. PROVIDE SINGLE SHOWN ON FLOOR PLATE WITH NUMBER OF OUTLETS AS INDICATED (2 SHOWN)

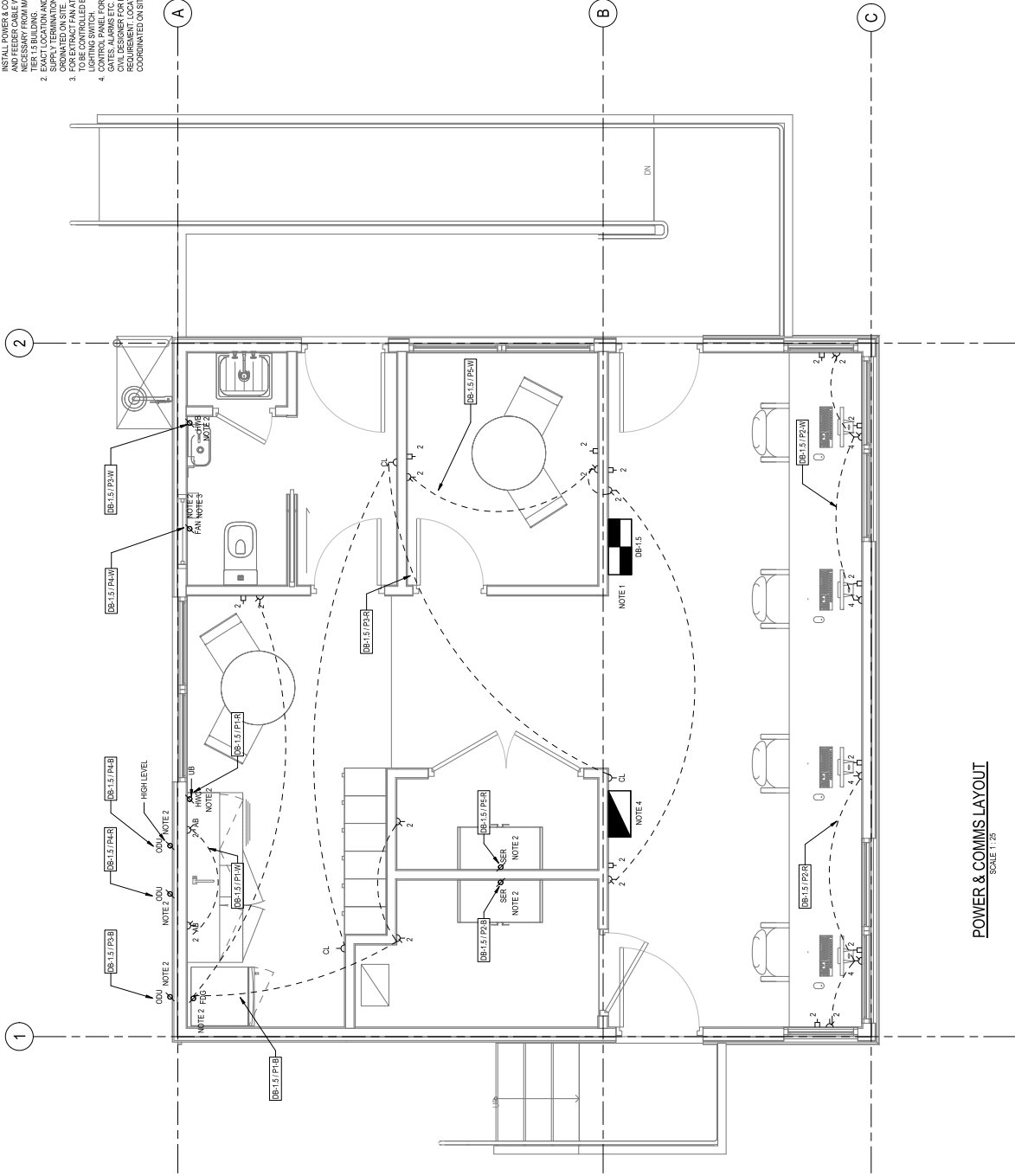
COMMUNICATIONS SERVICES SHOWN FOR INFORMATION & CO-ORDINATION ONLY. SUPPLY AND INSTALLATION OF SERVICES BY OTHERS.

ABBREVIATIONS

- AB ABOVE BENCH
- CL CLEANER SINK
- CW COMPLETE WITH
- HMB HOT WATER BOILER
- HWC HOT WATER CYLINDER
- HTS HEAT PUMP OUTDOOR UNIT
- UB UNDER BENCH

GENERAL NOTES

1. ALL ELECTRICAL INSTALLATION SHALL COMPLY WITH AS/NZS 3000.
2. LOCATION OF ALL DOORS AND DOOR THRESHOLDS SHALL BE INDICATED BY ARCHITECT PRIOR TO LIGHT SWITCH AND WIRING INSTALLATION.
3. SWITCHES TO BE MOUNTED AT 1200mm AFFL AND POWER OUTLETS AT 300mm AFFL.
4. ALL ELECTRICAL SERVICES SHALL BE INSTALLED IN ACCORDANCE WITH THE EMERGENCY LIGHT FITTING AND PROVIDE EMERGENCY LIGHTING.
5. ALL CONDUITS SHALL BE INSTALLED CW.
6. LIASE WITH ALL TRADES.
7. WHERE REQUIRED FOR SUPPORT AND FOR BRACKETING AND FRAMING BETWEEN PAIRINGS PROVIDE AND INSTALL UNDERSTUT C/W.
8. CABLES SHALL BE RUN IN TRACED.
9. ALL DISTRIBUTION BOARDS SHALL BE TYPED FORMATION UNLESS STATED OTHERWISE.
10. ALL CIRCUITS LABELED AS SPARE ON THE DISTRIBUTION BOARD SCHEDULES SHALL BE PROVIDED ON CIRCUIT BREAKERS.
11. ALL SWITCHES SHALL BE GRIPPLE SEISMIC KIT.
12. ALL UNWIRING SHALL BE SEISMIC.
13. THE SUB CONTRACTORS TO COORDINATE DRAWINGS, DETAILS, ELEVATIONS, JOINERY DETAILS AND WITH JOINERY INSTALLER.
14. ALL ELECTRICAL SERVICES SHALL BE INSTALLED IN ACCORDANCE WITH THE EMERGENCY LIGHT FITTING AND PROVIDE EMERGENCY LIGHTING.
15. USE OF MECHANICAL TRAFFIC FOR EXACT LOCATION OF MECHANICAL EQUIPMENT AND FINAL TERMINATION OF CABLES.



POWER & COMMS LAYOUT  
SCALE 1:250

TENDER / CONSENT

Client NEW ZEALAND TRANSPORT AGENCY  
Project TIER 1.5 - CONTROL BUILDING  
Title

POWER, & COMMS LAYOUT

Drawing No: 51-34037-A900

Rev: 1

GHDWOODHEAD  
creativespaces

Level 1, Guy Thomas House  
215 Junction Quay, Wellington, New Zealand  
T 64 4 472 0989 F 64 4 472 0833  
E graham@ghd.com W www.ghd.com

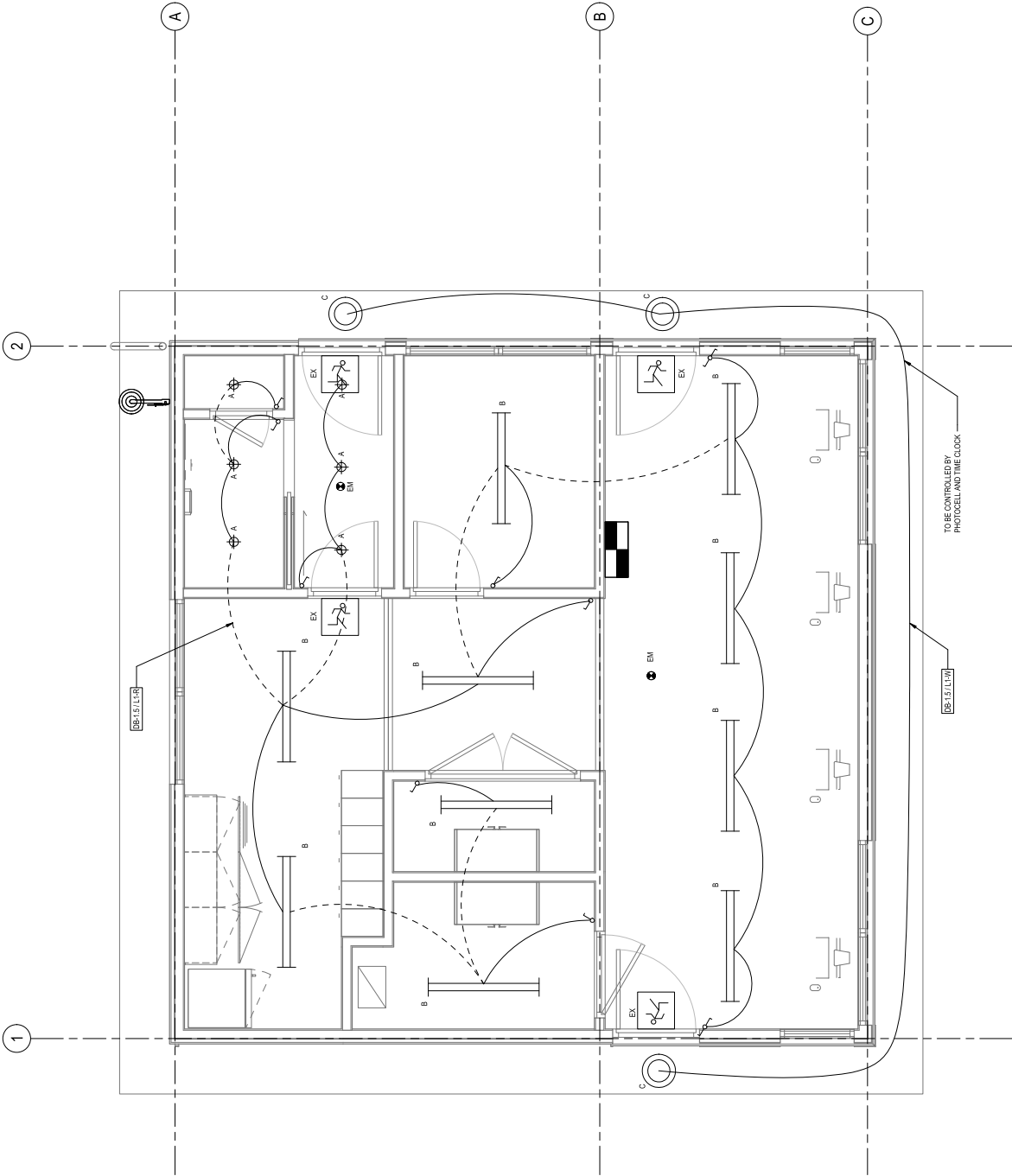
NZTRANSPORT  
AGENCY  
WAKA MATUA

0 250 500 750 1000 1250mm  
SCALE 1:250 ALTERNATIVE

| No | Revision | Issue         | Date     | Drawn | Checked | Project Manager |
|----|----------|---------------|----------|-------|---------|-----------------|
| 1  |          | CONSENT ISSUE | 05.07.19 | NW    | DA      |                 |

Call File No: G0134037/CA02/Rev01/1.5 RAKAM/1.5 RAKAM/1.5

Print Date: 15/10/2019 10:38:04



**LIGHTING**

- SWITCHING LINE
- CIRCUIT LINE
- ONE WAY LIGHT SWITCH
- TWO WAY LIGHT SWITCH
- SURFACE MOUNTED LUMINAIRE
- DOWNLIGHT
- BULKHEAD TYPE LUMINAIRE
- RECESSED OCCUPANCY SENSOR
- SURFACE MOUNTED IP65 PIR SENSOR

**EMERGENCY LIGHTING**

- INTERNALLY ILLUMINATED EXIT SIGN WITH DIRECTIONAL ARROWS
- INTERNALLY ILLUMINATED EXIT SIGN
- RECESSED EMERGENCY LUMINAIRE

| LUMINAIRE SCHEDULE |                                                                                                                                                                                                                                                             |
|--------------------|-------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------------|
| LABEL              | DESCRIPTION                                                                                                                                                                                                                                                 |
| A                  | VERSALUX COMO MINI 11W LED AND 779mm OUTPUT IP44 RATED, FACED CLEAR RECESSED COW LUMINAIRE, 3000K COLOUR TEMPERATURE, LUMINAIRE TO BE RECESSED INTO CEILING                                                                                                 |
| B                  | VERSALUX AXIUM LINEAR EXTRUDED LUMINAIRE, 34W LED WITH 4100mm OUTPUT, IP20 RATED, COW INTERGRAL ELECTRONIC GEAR, 4000K COLOUR TEMPERATURE, LUMINAIRE TO BE SUSPENDED AT 2.60mm AFTL                                                                         |
| D                  | LEGRAID BULKHEAD R 380 PLAIN LED LUMINAIRE WITH EMERGENCY LIGHTING FUNCTION, 18W LED WITH 2700mm OUTPUT, IP65 AND IK10 RATED, COW INTERGRAL FIXED OUTPUT ELECTRONIC GEAR, 4000K COLOUR TEMPERATURE, RECESSED BULKHEAD LUMINAIRE TO BE RECESSED INTO CEILING |
| EM                 | LEGRAID SATELLITE CORRIDOR NON MAINTAINED SPV EMERGENCY LUMINAIRE (66204LL), COW 0.86W LED, LUMINAIRE TO BE RECESSED INTO CEILING                                                                                                                           |
| EX                 | LEGRAID COW LED EMERGENCY MANTAINED SPV RUNNING MAN DECAL EXIT SIGN WHITE BODY, COW LUMIN BATTERY PACK AND WALL SURFACE MOUNTING KIT, MINIMUM 90 MINUTES DISCHARGE TIME, LUMINAIRE TO BE SURFACE MOUNTED TO WALL ABOVE DOOR                                 |

**LIGHTING LAYOUT**  
SCALE 1:25



## APPENDIX C