SCHEDULE OF AMENDMENTS

The amendments to the Selwyn District Plan are due to the following changes:

Name	Description
Land Use Recovery Plan	The Land Use Recovery Plan has been approved by the Minister for Canterbury Earthquake Recovery and was gazetted on 6 December 2013
Clause 20a Amendments	Various minor amendments

Please amend your District Plan by updating the following pages:

Township Volume

Amendments from 20.06.2013 to 06.12.2013

Replace pages - Please recycle all pages removed	
Part AO Contents	A0-005 - A0-006
nsert new Appendix 42, Living Z Zone, Prebbleton	
art A4 Finding Material	A4-011 - A4-012
4.5 Townships and Zones, Table 4.4 – Descriptions of Townships and Zones, elete Living Z, and insert new Living Z description	
art B1 Natural Resources	B1-007 - B1-008
elete Policy B1.1.7 and insert new B1.1.7	
art B2 Physical Resources	B2-025 - B2-026
nend Policy B2.1.26 relating to noise sensitive activities	
rt B4 Growth of Townships	B4-003 - B4-004
	B4-031 - B4-042
elete existing and insert new Policy B4.1.1, Obj B4.3.3, B4.3.5 and B4.3.6, Policy 4.3.1, B4.3.7, B4.3.8, B4.3.9, B4.3.56 and B4.3.76.	B4-069 - B4-070
sert new Policy B4.3.68, and renumber subsequent policies accordingly	B4-075 - B4-108
art C4 Living Zone Rules – Buildings	Entire chapter

Table C4.1 Site Coverage Allowance – delete Living Z and insert new Living Z Allowance

Delete existing and replace with new - C4.7.3.3, C4.7.4.1, C4.9.23, C4.13.1, C4.13.2, C4.13.3

Delete C4.13.5

Insert new rules C4.17 Fences Adjoining Reserves, C4.17.1, C4.17.2 and sub rules.

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Part C12 Living Zone Rules – Subdivision	C12-011 - C12-016 C12-029 - C12-030
Table C12.1 Allotment Sizes, Delete Living Z zone for Lincoln, Prebbleton and Rolleston, and replace with new description. Delete Living 1A for Prebbleton and replace with new description Delete Rule 12.1.7.6 and replace with new rule.	
Part D - Definitions	D-009 - D-016
Delete existing definition of 'Noise Sensitive Activities' and insert new definition	
Part E – Appendix 19 Prebbleton Living 1A, 1A1, 1A2, 1A3, 1A6, LX and 2A Zones, Prebbleton	E19-005 - E19-016
Delete ODPs marked "Selwyn District Council Outline Development Plan Shaw Appeal 27/04/06" and "Selwyn District Council Outline Development Plan Fowler and Troy enterprises 27/04/06". Add new ODP Area 5	
Part E – Appendix 37 Lincoln	E37-009 - E37-010
ODP Area 2, under Density Plan – delete 1st sentence and insert new	E37-017 - E37-026
ODP Area 3, under Density plan – delete paragraph and insert new ODP Area 4, under Density Plan – delete 1 st and 2 nd sentences and insert new, insert new ODP Maps	E37-039 - E37-040
ODP Area 6, under Density Plan – delete paragraph and insert new	
Part E – Appendix 38 Rolleston	E38-011 - E38-012
ODP Area 2, under Residential Density – rename subheading as Density Plan, delete	E38-015 - E38-016
3rd sentence and insert new	E38-021 - E38-030
ODP Area 3, under Density Plan – delete paragraph and insert new ODP Area 6, under Density Plan – delete ODPs and text and insert new ODPs and text	E38-035 - E38-036
ODP Area 7, under Residential Density – rename subheading Density Plan, delete	
text and insert new ODP Area 8, under Density Plan – delete text and insert new	
Part E – Appendix 42 Prebbleton, Living Z Zone, Prebbleton	Entire chapter
Insert new Appendix 42	

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Rural Volume

Amendments from 20.06.2013 to 06.12.2013

Replace pages - Please recycle all pages removed	
Part B2 Physical Resources	B2-025 - B2-026
Amend B2.1.28, Christchurch International Airport	
Part D Definitions	D-007 - D-014
Insert new definition for Noise Sensitive Activities	

Maps Volume

Amendments from 20.06.2013 to 06.12.2013

Replace pages - Please recycle all pages removed

Amended Planning Maps, please replace as follows:

 $\begin{array}{l} \text{Maps: } 008,\,009,\,013,\,014,\,095,\,096,\,097,\,098,\,099,\,101,\,102,\,103,\,104,\,105,\\ 107,\,108,\,109,\,110,\,111,\,113,\,114,\,115,\,116,\,117,\,118,\,119,\,120,\,121,\,122,\\ 123,\,125,\,126,\,127,\,128,\,129,\,\text{and }130. \end{array}$

Schedule of Amendments 003

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Appendix 37

OUTLINE DEVELOPMENT PLAN, LINCOLN
Appendix 38
OUTLINE DEVELOPMENT PLAN, ROLLESTON E38-001
Appendix 39
OUTLINE DEVELOPMENT PLAN, HOLMES BLOCK, ROLLESTON E39-001
Appendix 40
OUTLINE DEVELOPMENT PLAN, SKELLERUP BLOCK, ROLLESTON E40-001
Appendix 41
OUTLINE DEVELOPMENT PLAN, AREA 3 DARFIELD E41-001
Appendix 42
OUTLINE DEVELOPMENT PLAN, LIVING Z ZONE, PREBBLETON E42-001

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Table A4.4 – Description of Township Zones

Zone	Description
Living 1	Areas that are managed to maintain environments that are most pleasant for residing in. Activities in Living zones have effects which are compatible with residential activities and amenity values.
Living 2	As for Living 1 Zone, but with lower building density and development reflective of the rural character expected of low density living environments. While generally adjoining existing living zones, in some circumstances, low density Living 2 Zones can be located on the edge of townships. Larger sections, more space between dwellings, panoramic views and rural outlook are characteristic of this zone.
Living 3	As for Living 2 Zone, but with specific controls and design elements incorporated to ensure development of the land is reflective of and retains elements of rural character expected of the Living 3 Zone, which in essence is a rural residential zone, so as to visually set the development apart from the neighbouring urban area. Similar to the Living 2 Zone, larger sections (with a lower building density than Living 2), more space between dwellings, panoramic views and rural outlook are characteristic of the Living 3 Zone.
Living X	Areas zoned as Living but not yet developed. The developer may choose the residential density for the zone, but it may not be more dense than that of the Living 1 Zone in the township.
Living Z	New urban growth areas within or adjacent to the edge of existing townships. These areas are to be subject to an Outline Development Plan to ensure that good standards of urban design and connectivity with existing townships are achieved. The Living Z zone provides for a range of site sizes and living options, including lower density suburban areas and medium density small sections and townhouses. The Living Z zones, where an ODP is not operative in the Plan, are deferred until such time as an Outline Development Plan for the area is made operative in the District Plan. Where the inclusion of an operative ODP is the only reason for the deferral, the deferral will be considered to be lifted upon an ODP becoming operative within the Plan.
	Medium Density areas shown on an Outline Development Plan are subject to the Medium Density rules, and any business areas shown on an Outline Development Plan are subject to the Business 1 rules.
Business 1	Business 1 Zones have environments which are noisier and busier than Living zones, with more traffic, people, signs and building coverage. Business 1 Zones are still pleasant areas for people to live or work in, with good amenity/aesthetic values. They are also areas where higher density housing can be established as a permitted activity.
Business 2	Business 2 Zones are areas where activities likely to be considered less pleasant by people are located. Aesthetic and amenity standards are less than those in Living or Business 1 Zones. Activities are still managed to protect natural resources and people's health or well-being.
	Activities likely to cause 'reverse sensitivity' issues are discouraged in Business 2 Zones e.g. residential activities.
Business 3	The Business 3 Zone includes Lincoln University and the research organisations located at Lincoln. The zone is a 'hybrid' of Business 1 and 2. Some activities in this zone are likely to be considered unpleasant by people. Activities are also likely to be sensitive to effects from activities that would locate in a Business 2 Zone. Reasonably high aesthetic/amenity values are desirable in the Business 3 Zone due to both:
	- Its location, and
	- The number of customers/visitors to the site.
"A" or "B" Zones	A number of Living and Business zones have an "A" or "B" after the number, which indicates that the particular zone has some special rules or requirements. These special requirements are identified in the policies and rules for each zone.

The boundaries of each zone are either based on the existing character of an area or reflect a change in land use in accordance with an approved Outline Development Plan. The Plan was originally prepared on the basis that land was not zoned in anticipation of changes in activities and character except where:

- The zoning had been altered by a plan change; or
- The land was rezoned in the proposed plan 1995 and there was no resource management constraints on the proposed rezoning; or
- Zone boundaries were being rationalised; or
- It was appropriate to encourage a Business zone to consolidate in one area of a township.

The District Plan contemplated the use of private plan change procedures under the Act as the method by which to expand townships. Any person may request a plan change to rezone their land at almost any time. The plan contained a 'private plan change' or 'market-led' policy framework to guide decisions to rezone land, to ensure it promoted 'sustainable management'. The policy framework for rezoning land as Living or Business, and the Councils' reasons for using this method, are outlined in Part B, Section 4.3.

In order to address the issues arising from a continuing population growth rate and an absence of strategic planning in the provision of appropriate affordable infrastructure, including good connectivity and linkages through and between developments, and retaining the character of townships, the Council is now to take a directive role in determining where, and in what fashion, urban growth is to occur (i.e. a change from a 'market-led' to a 'community-led' approach).

In this respect, the Council has been involved in the development of the Greater Christchurch Urban Development Strategy, which is to provide a direction and planning framework for managing population growth and its impacts in the Greater Christchurch area (including Selwyn District) over the next 35 years (to 2041). The change to a community-led approach is being initiated through the preparation of structure plans for those larger townships within the Greater Christchurch area, including Lincoln, Rolleston and Prebbleton.

The Council also intends to adopt a strategic approach to managing growth in other townships in the district (outside the Greater Christchurch area). It will continue to manage the design and layout of subdivision, and the availability of services, within existing zoned areas and assessing any private plan change request to re-zone land for residential or business purposes against the relevant township policy framework, together with the broader objectives and policies of the Plan relating to the overall strategic provision of infrastructure, and the Selwyn Growth Model. This approach is to be applied until such time as a structure plan (or other strategic planning tool) has been prepared for these townships, either individually or collectively.

Some areas in townships which have been zoned as Residential, Commercial or Rural-Residential in the past have not retained these zonings in the Plan. This does not mean that these areas cannot be rezoned as Living or Business if there is a proposal to develop the area for these purposes and such development will not compromise the strategic growth of the township.

Arthur's Pass, Darfield, Springfield and Springston have areas of land which are no longer needed for their designated purpose, but do not have any new, proposed use. Those areas have been excluded from the respective township in the interim, except for an area at Darfield, which has been zoned a combination of Living 1, Business 1 and Business 2.

As with higher density residential areas, rural residential development is provided for through the Regional Policy Statement. Accordingly the District Plan specifically provides for rural residential opportunities as has long been the case in Selwyn District.

Policy B1.1.4

Co-operate with Environment Canterbury to encourage people to contain or 'clean up' contaminated sites.

Explanation and Reasons

Regional Councils have a specific function to control the discharge of contaminants into water and on to land. Any leak or spill of a substance (accidental or deliberate) is a discharge. Consequently, it is primarily the role of Regional Councils to address the containment or 'clean up' of 'discharges'. Selwyn District Council will support constructive initiatives by Environment Canterbury and by landowners to contain or 'clean up' contaminated sites. Selwyn District Council is unlikely to provide financial assistance to landowners to 'clean up' contaminated sites. As a general principle, the Council does not support ratepayers funds being used to pay to 'clean up' pollution caused by individuals. However, the Council is aware that some landowners may have unwittingly purchased contaminated sites. The Council maintains its discretion to offer financial assistance in special cases.

Methods

Information

Testing and cleaning up contaminated sites.

Advocacy

 Support Environment Canterbury initiatives to work with landowners to clean up contaminated sites.

UNSTABLE LAND

Policy B1.1.5

Ensure activities do not create unstable land.

Explanation and Reasons

Policy B1.1.5 is to avoid creating unstable land, in the first instance. Parts II and IV of the Regional Land and Vegetation Management Plan also cover parts of Selwyn District. Part II of the plan is operative and addresses effects associated with earthworks and vegetation clearance on the Port Hills between Christchurch City and Gebbies Pass. Part IV addresses effects associated with high country burning. The proposed Regional Land and Vegetation Plan addresses effects of clearing vegetation on hill and high country and clearing vegetation and earthworks on the Port Hills. The Building Act 2004 requires building consents for the construction of dams over a certain size but not earthworks for other activities. The District Plan rules address effects of large scale earthworks on land stability that are not covered by other regulations.

The policy and rules do not prevent large scale earthworks and earthworks on slopes. The Plan requires these activities be managed through a resource consent, to ensure sites are properly stabilised, filled and recontoured so the land is stable in the future.

Methods

District Plan Rules

Earthworks (all zones)

Regional plan rules

- Vegetation Clearance Hill and High Country, Port Hills
- Earthworks (Port Hills)

Policy B1.1.6

Carry out all legal requirements to record information about sites which may have unstable land, on Land Information Memoranda.

Explanation and Reasons

Under the Local Government Official Information and Meetings Act 1987, the Council has a duty to disclose any information it has about a property. This includes any information the Council has about erosion, slips or subsidence on the property, or any activities which may have involved digging pits or holes that may not be properly filled. The Council is keeping a Register of such sites. The Register is not a comprehensive list of all sites with possibly unstable land in the District. The sites listed in the Register may not be unstable. The Register indicates that there is a possibility some land is unstable, which should be investigated.

The Council acknowledges this policy and the Register may be unpopular, because it may affect property values. The Council has a legal duty to disclose any information it has about a property on a LIM. If the Council does not do this, it risks litigation which may be costly for ratepayers.

Methods

Information

LIMs and property files

Advocacy

Information about what to do should a LIM list a site as possibly having unstable land.

Policy B1.1.7

Avoid adverse effects from erecting buildings or structures on unstable land or land that is prone to liquefaction.

Explanation and Reasons

The most likely adverse effects from unstable land occur when buildings or structures are located on or close to the unstable area. The Council has powers under the Building Act 2004 to ensure any building site is appropriate for the proposed activity, and to avoid building on land subject to erosion, slippage or subsidence.

When a request is made for a plan change to rezone land for new residential or business development, the Council will require information on past activities on the site. The Council may require the stability of the site (or part of the site) be tested.

Explanation and Reasons

Many townships in Selwyn District have developed alongside main road or rail routes. As these routes get busier and residents' expectations about the quality of their living environment increase, conflicts occur. Busy roads or railway lines can affect communities. Effects include:

- noise, fumes, glare, dust and vibration from vehicles and trains;
- actual or potential safety risks from pedestrians and motorists having to cross railway lines or busy roads.

A busy road or railway line bisecting a township can also create psychological barriers for the community; the 'other side' of the transport route can be considered to be another community.

Policy B2.1.22 discourages this land use pattern from happening in the first instance. Policy B2.1.23 discourages existing patterns from being exacerbated. The policies provide, in the first instance, for new residential or business areas to expand at angles to rather than parallel with the transport route if possible. The policies recognise that there may be other resource management constraints to expanding townships in those directions. If so, the next 'best' option is to confine any further expansion of the township to one side of the transport route.

Policy B2.1.24

Encourage heavy vehicles to use routes which bypass townships, where practical and appropriate, and avoid new residential development along heavy vehicle bypasses.

Explanation and Reasons

Heavy vehicles travelling through townships can adversely affect:

- Residential amenity values through dust, noise and vibration;
- Perceptions of safety, especially for cyclists and pedestrians; and
- Roads, if they are not designed for heavy vehicles.

Policy B2.1.24 encourages heavy vehicles to use routes that bypass rather than bisect townships, where practical and appropriate, in order to avoid these effects. The preferred method to achieve this in these circumstances is to design ring roads and bypasses that are quicker and easier to use, than roads which bisect townships. Consequently, once a bypass or heavy vehicle route is created, it is important that it is not adversely affected by new residential or business activities occurring along the route, and then trying to slow or restrict the traffic using it.

Methods

Selwyn District Council Engineering Code of Practice

Road design

Bylaws to prevent heavy vehicle use of roads if necessary

Asset Management Plans

Roads

District Plan Policies

 To assess plan change requests to rezone land for the expansion of townships around heavy vehicle routes or bypasses

Policy B2.1.25

Mitigate adverse effects from the construction or maintenance of roads or railway lines on:

- adjoining residents;
- any waterbodies or ecosystems; or
- any special landscape, cultural, heritage or amenity values of the site or area.

Explanation and Reasons

When transport routes are constructed, maintained or replaced, works involve noise, dust and vibration. Earthworks can affect local waterbodies. Special ecological or cultural sites may be disturbed or landscape values affected by either the transport route itself or the surrounding land not being landscaped or replanted after the work. Many of these effects are only "temporary" effects. However, the duty under section 5(2)(c) of the Act (to avoid, remedy or mitigate any adverse effects of activities on the environment) applies to "temporary effects" too (see section 3 of the Act).

Transport routes are important economic and social activities in the District. The Plan policies and rules are not intended to prevent transport routes from being constructed, repaired, redesigned or replaced. Rather, the Plan provisions encourage these activities to occur in ways that reduce or mitigate associated effects on the environment.

Most work on transport routes is undertaken by requiring authorities on designated sites. In these cases the District Plan provisions do not apply (see section 176 of the Act). The methods the Council shall use include: information and advocacy; its powers under section 176(A) of the Act; and ensuring it acts responsibly when undertaking its function to manage roads in the District.

Methods

District Plan Rules (if applicable)

- Earthworks
- Construction Noise and Vibration
- Subdivision

Information and Advocacy

 Support New Zealand Transport Agency's policies for environmental protection during road construction and realignment

Designation Outline Plans

 Use powers under section 176(A) of the Act to encourage requiring authorities to minimise the effects of their activities on the environment

CHRISTCHURCH INTERNATIONAL AIRPORT

Policy B2.1.26

Avoid new residential development and other noise sensitive activities occurring on land which is located underneath the airport flightpath noise contours shown on Planning Map 013 for 50 dBA Ldn or greater.

meet their needs. This includes alternative housing forms such as comprehensive residential development provided the spacious character and amenity values associated with adjoining and surrounding low density living environments are maintained. The role of the District Plan is twofold:

- To set 'bottom lines' or base requirements to maintain the overall spacious character.
- To ensure there are not any unnecessary rules or regulations that stop people from choosing the appropriate section or house sizes they want.

The plan sets these 'bottom lines; through policies and rules for average allotment sizes and for site coverage.

For some locations in and around townships, a low density Living 2 environment, which result in a compact town form and is consistent with preferred growth direction for townships, may be appropriate. Such a low density residential area will add to the range of living environments within the District.

Medium Density areas are to be identified in Outline Development Plans to provide for anticipated population growth within a consolidated urban area and provide choice and opportunity for a variety of housing types. The provision of medium density areas will result in a more urban character that is nonetheless anticipated to provide a high quality living environment, albeit with a less spacious character than the typical low density environments that currently exist. Any new living zones shall be consistent with the General Policies, Town Form policies and any relevant Specific Township policies by which all plan change requests are judged.

At some locations around townships, a low density Living 2 environment may be appropriate on the edge of towns. Within such low density living environments particular regard should be had to reducing the potential for reverse sensitivity by increasing the size of allotments at the rural interface, reducing the number of people exposed to potential reverse sensitivity effects and in turn providing an appropriate buffer between the rural area proper and the more concentrated township areas.

Any Living 3 Zone being a rural residential zone shall be located beyond the 'urban limits' but where it can be economically provided with reticulated sewer and water supply, and appropriate stormwater treatment and disposal. The Living 3 Zone will have regard to providing a visual transition area between the 'urban area' and the rural area which exists beyond townships by incorporating certain design elements of rural character, which are common in rural settings so the land is visually set apart from the neighbouring urban area.

New residential areas should be attractively laid out, with allotments of an appropriate shape to build a house, and with access to sunlight. As well as functional utilities, new residential areas need some open space, plantings and landscaping to make them aesthetically pleasing. Objective B4.1.2 is to ensure future residential areas maintain the current attractive layout of Selwyn's townships. For low density Living 2 and 3 Zones, the careful consideration and application of design treatment to such matters as road formation, kerbs, letterboxes, power supply, entry treatment, fencing, landscaping, lighting and the like will ensure the retention of open, spacious rural character. The market can be relied on to achieve this to a certain extent – many people won't buy sections in an unattractive area. However, not all people have the money to choose allotments in more attractive subdivisions. The District Plan provisions set some "bottom lines" to ensure all new residential areas achieve a standard of aesthetic appeal.

RESIDENTIAL DENSITY — POLICIES AND METHODS

Policy B4.1.1

- (a) Provide for a variety of allotment sizes for erecting dwellings in Living 1 Zones, while maintaining average section size similar to that for existing residential areas in townships, except within the Living Z Zone, including any Medium Density area identified in an Outline Development Plan where a higher density of development is anticipated.
- (b) Facilitate development in the Living 1A6 Zone in Prebbleton where it is consistent with the density provisions of the Regional Policy Statement, and is compatible with the form and character of development in adjacent Living zones, with a particular emphasis on maintaining residential lots of not less than 1,000m² along the common boundary of the Kingcraft Drive Existing Development Area.

Explanation and Reasons

Policy B4.1.1 (a) and associated rules allow for a variety of section sizes when land is subdivided to erect dwellings in Living 1 Zones, provided small sections are balanced with larger ones. This keeps residential density more spacious than in Christchurch City. However, in the Medium Density areas identified on Outline Development Plans, developments are encouraged to accommodate anticipated urban growth, retain a compact urban form, and provide for housing diversity through a higher residential density. Subdivision rules include an average section size for each Living zone. The rules allow for a smaller average section size for flats or townhouses. The number of these allotments in each zone is controlled through the resource consent process or through an Outline Development Plan. There is no average section size in Business zones.

Policy B4.1.1 (b) also requires development of the Living 1A6 Zone to be consistent with the density provisions of the Regional Policy Statement, whilst having regard to the form and character of development of the adjacent living zones. This is to ensure development proceeds in a sustainable and compact manner, without adversely impacting on the 'spacious' character of existing development in the vicinity of the site.

Methods

District Plan Rules

Subdivision (Living zones)

Policy B4.1.2

Maintain Living 2 and 3 Zones as areas with residential density which is considerably lower than that in Living 1 Zones.

Explanation and Reasons

Living 2 Zones replaced rural-residential zones in the Transitional District Plan, where these zones adjoin townships. Average section sizes in these zones vary from 0.5 to 1 hectare. Roads and other utilities have been designed for a population of that density to reflect the sense of open space and 'spaciousness' anticipated by persons wishing to live in a low density residential environment.

RESIDENTIAL AND BUSINESS DEVELOPMENT — OBJECTIVES

Objective B4.3.1

The expansion of townships does not adversely affect:

- Natural or physical resources;
- Other activities;
- Amenity values of the township or the rural area; or
- Sites with special ecological, cultural, heritage or landscape values.

Objective B4.3.2

For townships outside the Greater Christchurch area, new residential or business development adjoins existing townships at compatible urban densities or at a low density around townships to achieve a compact township shape which is consistent with the preferred growth direction for townships and other provisions in the Plan.

Objective B4.3.3

For townships within the Greater Christchurch area, new residential or business development is to be provided within existing zoned land or priority areas identified in the Regional Policy Statement and such development is to occur in general accordance with an operative Outline Development Plan.

Objective B4.3.4

New areas for residential or business development support the timely, efficient and integrated provision of infrastructure, including appropriate transport and movement networks through a coordinated and phased development approach.

Objective B4.3.5

Ensure that sufficient land is made available in the District Plan to accommodate additional households in the Selwyn District portion of the Greater Christchurch area between 2013 and 2028 through both Greenfield growth areas and consolidation within existing townships.

Objective B4.3.6

Ensure that subdivision and development in Living Z zoned areas achieves an average net density over an Outline Development Plan area of at least ten household units per hectare.

Objective B4.3.7

Ensure that any rural residential development occurs outside the urban limits identified in the Regional Policy Statement and such development occurs in general accordance with an operative Outline Development Plan, supports the timely, efficient and integrated provision of infrastructure, and provides for the long-term maintenance of rural residential character.

Explanation and Reasons

Objective B4.3.1 outlines the types of effects that the expansion of townships should avoid. It includes both tangible effects on natural and physical resources; and intangible effects on the 'character' of both townships and rural areas. The intangible effects are often the most difficult to manage. The plan does this using a zoning system to identify and manage areas based on their character. For example, Living zones provide an environment that is most pleasant for residential activities. Business zones accommodate the effects of business activities as well as residential activities.

Objective B4.3.2 seeks to maintain the distinction between 'town and country' in Selwyn District by promoting most residential and business development in or adjoining existing townships. A more managed approach applies within the Greater Christchurch area, where new residential or business growth is to be contained within the Urban Limits identified in the Regional Policy Statement. Residential and business development can occur in the rural areas but at densities and under conditions that maintain rural character. In particular, the maintenance of a ratio of buildings to 'open space' and dominance of vegetation cover rather than hard-standing areas.

Objective B4.3.2 also encourages the expansion of existing townships in preference to the creation of new settlement areas. Additional controls apply to the expansion of those townships within the Greater Christchurch area, whereby residential or business development is to be contained within township urban limits to achieve a compact urban form. Further, new low density living environments need not necessarily be adjoining townships but should be located around the edge of existing townships where they will promote a compact town shape and consolidated form. Selwyn District already has 22 townships. None are sufficiently large to provide employment for most residents. Many do not have sufficient population to sustain basic business services and community facilities, or to fund basic utilities, such as reticulated sewage treatment and disposal.

Objective B4.3.3 seeks to ensure that sufficient land is provided within township Urban Limits (as identified within the Regional Policy Statement) to accommodate the projected growth of those townships within the Greater Christchurch area. It is anticipated that residential or business development within the Urban Limits is to occur in accordance with an Outline Development Plan that has been inserted into the District Plan. In Medium Density areas identified in Outline Development Plans, higher density development is provided for in order to accommodate anticipated population growth, whilst achieving both a compact urban form and a diversity of living environments.

Objective B4.3.4 recognises that the structure plan and subsequent Outline Development Plan and Plan Change processes provide a reliable mechanism for integrating future and anticipated urban development with infrastructure requirements. They also provide the mechanism for achieving the intensity, type and form of development necessary to accommodate urban growth in a sustainable way. In addition, ODPs provide certainty for the community, developers, network utility providers and territorial authorities that all constraints associated with the development of an area are investigated, addressed, protected and provided for at the time of initial urban zoning.

Objective B4.3.5 recognises the need to ensure that sufficient land is made available to accommodate the anticipated number of new households within Selwyn District's portion of the Greater Christchurch area. Objective B4.3.6 seeks to ensure that in order for the new growth areas to accommodate the predicted growth in household numbers set out in Objective B4.3.5, development of those growth areas needs to generally achieve an average net density of 10 units per hectare. This density will be implemented through Outline Development Plans and subsequent subdivision consents.

Objective B4.3.7 seeks to ensure that rural residential development occurs outside the urban limits in a manner that accords with the Regional Policy Statement. Given the nature of rural-

residential development, Objectives B4.1.1, B4.1.2 and B4.1.3 of the Rural Volume of the District Plan are also relevant when assessing the sustainability of rural land for rural residential purposes.

The relevant policies are outlined in the next three subsections.

RESIDENTIAL AND BUSINESS DEVELOPMENT — GENERAL POLICIES

All objectives and policies in Part B, Sections 1.1 to 4.2 and 4.4 apply to the expansion of townships. Any variation or plan change request to rezone land should be either consistent with those objectives and policies, or seek to change them.

Table B4.3.1 provides cross references to the objectives and policies in Part B, Sections 1.1 to 4.2 and 4.4 which may be particularly relevant to rezoning land for new residential or business development. Table B4.3.1 does not limit the application of Part B of the Plan. It does not purport to be either complete or exclusive, and is included for ease of reference only.

Table B4.3.1 — Policy Framework for Expansion of Townships (Part B Objectives and Policies)

Plan Section	Objective / Policy
Land and Soil	Objectives B1.1.1 and B1.1.2
	Policies B1.1.3, B1.1.7, B1.1.8
Water	Objectives B1.2.1 and B1.2.2
	Policies B1.2.2 - B1.2.7, B1.2.9
Ecosystems	Objectives B1.3.1 and B1.3.2
	Policies B1.3.1 - B1.3.3
Outstanding Natural Features and Landscapes	Objectives B1.4.1, B1.4.2 and B1.4.4
	Policies B1.4.6, B1.4.10, B1.4.11, B1.4.13 - B1.4.17
Transport	Objectives B2.1.1 - B2.1.4
	Policies B2.1.3, B2.1.4(a) & (b), B2.1.7, B2.1.9 - B2.1.11, B2.1.15, B2.1.17 - B2.1.20, B2.1.22 and B2.1.23
Utilities	Objective B2.2.1
	Policies B2.2.1 - B2.2.3, B2.2.5, B2.2.6, B2.2.11
Community Facilities	Objective B2.3.1
	Policies B2.3.1, B2.3.2, B2.3.4, B2.3.8 and B2.3.9
Waste Disposal	Objective B2.4.1
	Policies B2.4.2, B2.4.4 and B2.4.5
Natural Hazards	Objective B3.1.1
	Policies B3.1.2 - B3.1.5, B3.1.7
Hazardous Substances	Objective B3.2.1
	Policy B3.2.2

Plan Section	Objective / Policy
Culture and Heritage	Objective B3.3.1
	Policies B3.3.6 and B3.3.7
Quality of the Environment Objectives B3.4.1 - B3.4.5	
	Policies B3.4.1, B3.4.3 - B3.4.7, B3.4.23, B3.4.35, B3.4.36, B3.4.38 and B3.4.39
Residential Density	Objectives B4.1.1 and B4.1.2
	Policies B4.1.3, B4.1.4, B4.1.10 and B4.1.11
Subdivision Objective B4.2.2 – B4.2.4	
	Policies B4.2.1, B4.2.2 and B4.2.8 - B4.2.12

Methods

- Plan Changes.

NOTE:

See guide in Appendix 14 for information to be included with plan changes.

RESIDENTIAL AND BUSINESS DEVELOPMENT — TOWN FORM POLICIES

Policies B4.3.1 to B4.3.10 of this section apply to rezoning of land for residential or business development in addition to the general policies in Part B, Sections 1.1 to 4.2 and 4.4. Any variation or plan change request should be consistent with each and every relevant policy in these two parts of the Plan or seek to change those policies with which it is not consistent.

Policy B4.3.1

Ensure new residential, rural residential or business development either:

- Complies with the Plan policies for the Rural Zone; or
- The land is rezoned to an appropriate Living Zone that provides for rural-residential activities (as defined within the Regional Policy Statement) in accordance with an Outline Development Plan incorporated into the District Plan; or
- The land is rezoned to an appropriate Living or Business zone and, where within the Greater Christchurch area, is contained within existing zoned land and greenfield priority areas identified in the Regional Policy Statement and developed in accordance with an Outline Development Plan incorporated into the District Plan.

Explanation and Reasons

Zoning is an integral part of the approach the District Plan uses to promote sustainable management of natural and physical resources. New residential, rural residential or business activities need to occur on land which is appropriately zoned, to ensure the most appropriate policies and rules in the Plan apply to the activity. Additional requirements apply to those townships within the Greater Christchurch area, whereby new residential or business development within Greenfield areas is to be contained within the Urban Limits identified in the Regional Policy Statement and such development is to be carried out in accordance with an approved ODP that has been inserted into the District Plan. The purpose of these provisions is to consolidate and manage the rate of growth within these townships to ensure the integration of development with the appropriate provision of infrastructure, transport linkages, reserves and other community facilities.

Policy B4.3.2

In areas outside the Greater Christchurch area, require any land rezoned for new residential or business development to adjoin, along at least one boundary, an existing Living or Business zone in a township, except that low density living environments need not adjoin a boundary provided they are located in a manner that achieves a compact township shape.

Policy B4.3.3

Avoid zoning patterns that leave land zoned Rural surrounded on three or more boundaries with land zoned Living or Business.

Explanation and Reasons

To achieve Objective B4.3.2, any land rezoned for new residential or business development in areas outside the Greater Christchurch area should adjoin an existing township zone along at least one boundary, except in the case of low density residential development located around townships that achieve a compact and consolidated town shape. The new zoning pattern should also avoid leaving a patch of land zoned 'Rural' surrounded by Living or Business zones. Such a land zoning pattern can increase the potential for 'reverse sensitivity' issues between the site and adjoining zones. It also makes the provision of utilities and services more difficult, because it is uncertain whether this land will be rezoned in the future. In turn there are uncertainties about whether pipes and cables installed past the boundary should include capacity to serve the area, and, if so, who pays for it.

'Adjoining' land includes land separated by a road, water race, drain, easement or other vehicular accessway.

Policy B4.3.4

Encourage new residential or business development to occur on vacant land in existing Living or Business zones, if that land is available and appropriate for the proposed activity.

Explanation and Reasons

Many townships in Selwyn District have sites within existing Living or Business zones that may be able to be used for new residential or business activities. The Council encourages use of sites, in existing zones, in the first instance, to encourage a consolidated township area. Consolidated Living or Business areas may have the following benefits:

- Reduced potential 'reverse sensitivity' issues with activities in the Rural zone by reducing the 'interface' of the zones.
- More efficient utilisation of infrastructure, especially reticulated utilities.
- Fostering of a 'village' atmosphere.

Policy B4.3.4 Also recognises that sites in existing zones may not always be available for new activities. In addition they may not be available at an appropriate price, or be suitable for the proposed activity.

Policy B4.3.5

Ensure that land to be rezoned to Business 1 is assessed as to whether it has appropriate dimensions and characteristics to allow for the creation of the type of Business 1 zone environment sought in Policy B3.4.23a.

Explanation and Reasons

The usability of business land is highly dependent on its shape. If land is rezoned without regard to how it can be used, the result can be a town centre or neighbourhood centre where a high quality built environment is difficult to achieve.

Often, due to the shape of the land rezoned, it is difficult to achieve an improved urban form. It is therefore important that an application for rezoning demonstrates how the land can be used a way that will result in a high quality built environment, as described in Policy 3.4.23(a).

In instances where a high quality built environment is not possible or would depend to a great extent on the form of development chosen, then site specific rules may be required, either to restrict what can be done on the site, or the form of development which occurs.

The Council's Commercial Design Guide is a useful reference on how the shape of land may constrain development options.

Policy B4.3.6

Encourage townships to expand in a compact shape where practical.

Explanation and Reasons

New residential or business development, Policy B4.3.6 promotes shapes and situations that foster a compact or consolidated town shape. Compact means zones of similar lengths and widths, rather than long, thin strips of land following a particular road, waterbodies or other feature.

A compact shaped township helps mitigate adverse effects in the following ways:

- Reduction of the number of allotments that share a boundary with the Rural Zone, and therefore the area for potential conflicts between incompatible activities.
- Facilitating of cost-effective provision of services such as reticulated water, sewage, roads and footpaths because more connections are provided, or houses served, per kilometre of pipe or road.
- Reduction in the distance of houses to businesses and community facilities, which may
 encourage people to walk or cycle rather than use motor vehicles for short trips. It may
 also help to facilitate the 'village' atmosphere of townships, such that the shopping centre
 and community facilities are close to all houses.
- Reduction in the impacts on the road network, and more opportunity to impose lower speed limits in the township (refer to Part B, Section 2.1).
- Maintenance of the visual distinction between the rural area and townships (refer to Part B, Section 3.4).

Policy B4.3.6 recognises that a compact shaped zone may not always be practical if there is a constraint to a zone expanding in a particular direction(s). Such constraints may include (but are not limited to) geographical features such as mountains, waterbodies, and transport routes.

Method

Plan changes to rezone land

NOTE:

See guide in Appendix 14 for information to be included with requests for plan changes.

Policy B4.3.7

Living Z urban growth areas identified in the District Plan shall not be developed for urban purposes until an operative Outline Development Plan for that area has been included within the District Plan. Each Outline Development Plan shall:

 Be prepared as a single plan for any identified Outline Development Plan area identified on the Planning Maps and Appendices;

- Be prepared in accordance with the matters set out in Policy B4.3.8;
- Take account of the Medium Density and Subdivision Design Guides.

Explanation and Reasons

It is important to ensure that any areas identified in the District Plan as being suitable for urban growth are developed in a coordinated manner that achieves good levels of urban design and connectivity. The inclusion of an Outline Development Plan within the District Plan is therefore a key method for guiding development and achieving good environmental outcomes. Relevant urban design and subdivision design guidelines for Selwyn District are to be considered and addressed in the creation of an Outline Development Plan. A singular plan is necessary for each ODP area to maintain good strategic outcomes.

Method

District Plan Rules

Subdivision

District Plan Maps

Identify ODP Areas

Medium Density Design Guide

Subdivision Design Guide

Policy B4.3.8

Each Outline Development Plan shall include:

- Principal through roads, connection and integration with the surrounding road networks, relevant infrastructure services and areas for possible future development;
- Any land to be set aside for
 - community facilities or schools;
 - parks and land required for recreation or reserves;
 - any land to be set aside for business activities;
 - the distribution of different residential densities;
 - land required for the integrated management of water systems, including stormwater treatment, secondary flow paths, retention and drainage paths;
 - land reserved or otherwise set aside from development for environmental or landscape protection or enhancement; and
 - land reserved or otherwise set aside from development for any other reason, and the reasons for its protection.

- Demonstrate how each ODP area will achieve a minimum net density of at least
 10 lots or household units per hectare;
- Identify any cultural (including Te Taumutu Runanga values), natural, and historic or heritage features and values and show how they are to be enhanced or maintained;
- Indicate how required infrastructure will be provided and how it will be funded;
- Set out the phasing and co-ordination of subdivision and development in line with the phasing shown on the Planning Maps and Appendices;
- Demonstrate how effective provision is made for a range of transport options, including public transport systems, pedestrian walkways and cycleways, both within and adjoining the ODP area;
- Show how other potential adverse effects on and/or from nearby existing or designated strategic infrastructure (including requirements for designations, or planned infrastructure) will be avoided, remedied or appropriately mititgated;
- Show how other potential adverse effects on the environment, the protection and enhancement of surface and groundwater quality, are to be avoided, remedied or mitigated;
- Include any other information which is relevant to an understanding of the development and its proposed zoning; and
- Demonstrate that the design will minimise any reverse sensitivity effects.

Explanation and Reasons

The preparation of an Outline Development Plan requires consideration and inclusion of many aspects, particularly the location of low and medium density housing areas, layout of infrastructure and compatibility and connectivity with surrounding land uses. The phasing and general co-ordination of development is also an appropriate consideration for detailing how the ODP will be implemented.

Method

District Plan Rules

Subdivision

District Plan Maps

Identify ODP Areas

Medium Density Design Guide

Subdivision Design Guide

Policy B4.3.9

The phasing of any Living Z Zone shown on the Planning Maps and Appendices occurs as follows:

- In Rolleston 11 Living Z areas have been identified, and an Outline Development Plan for six of these areas have been incorporated into the District Plan. The remaining five Living Z ODP Areas have been deferred. The deferment for these areas shall be lifted once an operative Outline Development Plan for that area has been incorporated into the District Plan.
- In Lincoln six Living Z areas have been identified, and an Outline Development Plan for each area incorporated into the District Plan. A Business 2 Zone has also been identified with an operative ODP included in the District Plan.

Explanation and Reasons

The phasing of development is crucial to ensuring that sufficient land is made available to accommodate the District's predicted household growth, whilst also enabling a consolidated urban form of the Greater Christchurch area that includes both Christchurch City and Waimakariri Districts. The District Plan provides sufficient land at Rolleston to meet the anticipated growth until 2020, however four of these growth areas require an operative Outline Development Plan to be included within the District plan to ensure their integrated development and have therefore been deferred until an ODP is included. Within Lincoln, sufficient land has been made available to accommodate predicted households until 2020. A number of ODP areas in Lincoln however cover land areas that are much larger than necessary for accommodating urban growth to 2020. These ODP areas have therefore been split into two phasing periods, with the land within the second period deferred until 2021 (or 2017 in the case of ODP Area 5) and until adequate infrastructure has been provided. In terms of ODP Area 5, it is considered that due to the limited amount of households that can be achieved within the area, the deferred status can be lifted in 2017 rather than 2021.

The Business 2B zoned area is not subject to a phasing criteria and an ODP has been included into the District Plan.

Method

District Plan Rules

Subdivision

District Plan Maps

Identify Phases within ODP areas

Medium Density Design Guide

Subdivision Design Guide

Policy B4.3.10

To ensure that the key principles and outcomes sought in operative Outline Development Plans are achieved and where development is proposed that is not in general accordance with an operative Outline Development Plan in the District Plan, consideration shall be given as to whether:

- the proposed change will better achieve the key principles of the Outline Development Plan, as set out in Policy 4.3.7 and any specific ODP requirements set out in any areaspecific ODP policy, than the land use pattern shown in the operative ODP;
- the proposed change will potentially compromise the outcomes sought within the remainder of the Outline Development Plan area. This is especially the case where changes are proposed that only cover a portion of an ODP area and/or have implications for other parts of the ODP area beyond the applicant's control. Where development that is not in general accordance with the ODP is proposed via a subdivision consent application, it is preferable that the application covers the entire ODP area so that the implications of such changes are able to be fully understood and assessed;
- adequate provision has been made to ensure that such changes are aligned with the corresponding land use provisions of the District Plan and that this is transparent to current and future landowners.

Explanation and Reasons

Outline Development Plans are a key method for achieving the integrated, sustainable development of Greenfield urban growth areas. It is therefore anticipated that development will be in general accordance with Outline Development Plans. It is nonetheless recognised that through the detailed preparation of subdivision consent applications or asset design processes there is the potential for alternative solutions or routes to be developed that better achieve the outcomes sought in ODPs than the broad land use pattern shown on the ODP. When assessing applications for development that is not in accordance with an ODP, it is anticipated that such applications will only be granted where they are able to demonstrate that the proposed development better achieves the key principles and outcomes sought in the ODP than the layout shown in the ODP. Such demonstration is made easier where the proposed development encompasses the entire ODP area so that the implications of the changes can be fully understood. Conversely, where the proposed development is for only a portion of an ODP area the applicant will need to demonstrate that the outcomes sought for the entire ODP area will not be compromised.

Development that is not in general accordance with an ODP can also have implications for the rule package that will apply to that development over time. For example the Business 1 zone rules apply to neighbourhood business areas shown on ODPs, however if the final location of the centre changes, then the applicant will need to demonstrate that an appropriate mechanism such as consent notices on individual property titles is in place to provide certainty to future landowners regarding the relevant rule package.

Method

District Plan Rules

Subdivision

District Plan Maps

ODP areas

District Plan Appendices

Operative ODPs

Medium Density Design Guide

Subdivision Design Guide

Policy B4.3.11

Provide Neighbourhood and Local Centres, as shown in operative Outline Development Plans, to satisfy the more localised and convenience needs of people and communities, whilst recognising that neighbourhood and local centres are to complement existing Town Centres which shall remain the primary focus for commercial, social and community activity within each Township.

Explanation and Reasons

The Town Centres within each township serve an important function as significant physical resources providing for the social, cultural and economic wellbeing of their respective communities of influence. Crucially such Town Centres serve as focal points in terms of providing important public and private services and facilities in the most efficient manner to the wider community, and should remain the focus of commercial, business and retail development in future.

The majority of such Town Centres are comprised of a core business area providing primarily retail, service and limited community activities. It is considered that over time the range and diversity of such functions, including that of office and commercial activity, will increase.

Whilst the Business 1 Zone Objectives, Policies and Rules also apply to neighbourhood and local centres shown on ODPs, the services and facilities provided for such centres are generally to have a more localised area of influence due to their limited range of activities and accessibility, and accordingly need to complement existing Town Centres, and not compete with them in terms of being a substantial business or retail destination at a strategic level.

Accordingly, such neighbourhood centres, as identified by the ODPs should:

- Range in size, but generally comprise 1,000m² 2,000m² total floorspace and include:
 - up to 15 shops, with a maximum retail tenancy of 350m² GFA; and provide
 - a limited range of community facilities.

Local centres, as identified by the ODPs should:

- Range in size, but generally comprise up to 450m² total floorspace and include;
 - 1 5 shops, with a maximum retail tenancy of 350m² GFA; and provide
 - a limited range of community facilities.

LINCOLN

Specific Policies

Policy B4.3.56

Ensure that new Greenfield urban growth only occurs within the Outline Development Plan areas identified on the Planning Maps and Appendices, and in accordance with the phasing set out in Policy B4.3.9.

Explanation and Reasons

The future urban growth of Lincoln has been identified through both the Regional Policy Statement and the Lincoln Structure Plan. These two processes have identified the Greenfield areas adjacent to Lincoln that are most suitable for urban growth and where the potential environmental effects of such growth are able to be sustainably managed. These growth areas are identified on the planning maps and in the appendices and will be sufficient to meet the foreseeable needs of the Lincoln community and will enable the efficient provision of infrastructure to service that growth.

Policy B4.3.57

Ensure any land rezoned for new residential development has motor vehicle access from an alternative collector or arterial road other than Gerald Street.

Explanation and Reasons

Residents in Lincoln have expressed concerns about the effects, on pedestrian safety, of traffic along Gerald Street. The resulting 'traffic calming' works mean Gerald Street may not be as efficient for carrying vehicles 'through' the township as an alternative road intersecting directly with Springs Road, Ellesmere Road or Birches Road. This policy is consistent with policies B2.1.8 and B2.1.9.

Policy B4.3.58

Ensure stormwater disposal from any land rezoned for new residential or business development will not adversely affect water quality in the LI or LII waterbodies; or exacerbate potential flooding from the LI or LII waterbodies "downstream".

Explanation and Reasons

Stormwater disposal at Lincoln is commonly disposed of by a reticulated system that drains into the LI or LII waterbodies. These waterbodies flow into Te Waihora/Lake Ellesmere. Stormwater runoff can adversely affect these waterbodies by changing the sediments and contaminants contained in the stormwater, or the rate at which stormwater runs off the land. Policy B4.3.58 is to ensure no adverse effects arise from these changes. It is consistent with Policy B1.2.2.

Policy B4.3.59

Achieve integration between the rezoning of land for new residential development at Lincoln and associated provisions for utilities, community facilities and areas for business development.

Explanation and Reasons

A study prepared for the Council (Barber, 1999: Demand for Land for Commercial and Industrial Uses in Selwyn District) indicates that if projected residential growth occurs at Lincoln, there will be a shortage of space for associated business activities in the current Business 1 Zone.

In submissions on private requests for plan changes to rezone land at Lincoln, residents have expressed concerns about the impacts of additional residents on schools and other community facilities. Policy B4.3.59 is consistent with Policy B2.3.1.

Policy B4.3.60

Ensure rezoning any land for new residential or business development does not create or exacerbate potential "reverse sensitivity" issues in respect of activities in the Business 3 Zone or surrounding Rural Zone.

Explanation and Reasons

Activities in the Business 3 Zones and associated activities in the immediately adjoining Rural Zone may have effects that are incompatible with the amenity values of residential areas. An example is that sewage holding ponds south of the Living 1A Zone. To ensure existing activities in these zones continue relatively freely, residential development should be kept to the east of the Business 3 Zones, unless it is developed under conditions that ensure any potential 'reverse sensitivity' effects are minor. Policy B4.3.60 is consistent with policies B3.4.37 and B2.2.5.

Policy B4.3.61

Consider any potential adverse effects of rezoning land for new residential or business development to the north of Lincoln Township on the 'rural-urban' landscape contrast of the area with Christchurch City, as identified in the RPS.

Explanation and Reasons

Objective 3 and Policy 5 of Chapter 12 of the RPS identify an area of land between Christchurch City and a line extending from West Melton to Tai Tapu as having important landscape and amenity values. The RPS seeks to maintain this land in "rural and recreational" areas. Policy B1.4.17 of the District Plan addresses this issue.

The area identified by the definition in Chapter 12, Objective 3 and Policy 5 of the RPS is currently to the north of Lincoln so that the present urban area is not constrained by that policy. If, however, Lincoln grows to the north it is likely that such future growth will need to take account of the RPS policy.

Each ODP area has been identified predominately by using significant features that cause severance e.g. major roads or rivers, whilst also taking into consideration the ability to provide for infrastructure. Smaller ODP areas have generally been avoided as they will reduce the potential to achieve a strategic outcome. One exception in Lincoln is ODP Area 6, which due to its location adjacent to the Lincoln Community Centre and its use for medium density purposes only, is suited to a small ODP. Comparatively, providing one ODP for the entire area will create difficulties with multiple land ownership, zoning and staging such that the ability to readily implement future growth, particularly in the short term is likely to be heavily constrained by such difficulties. It is considered therefore that the approach taken with identified ODP areas is both the most practical in terms of maintaining the strategic objective and enabling development to continue in the short term. It is also considered that this approach is consistent with the Regional Policy Statement.

Whilst an ODP is generally conceptual in nature, some specific criteria have been listed which must be addressed in an ODP for any of the identified ODP areas. Each ODP area within Lincoln has specific requirements that have been identified through the Structure Plan process. Where an ODP is not already prepared, important infrastructure requirements have been listed under this specific criteria and these include matters such as roading links, water supply, stormwater management, wastewater treatment and disposal, Rail Trail route, and pedestrian and cycle links to the town centre, university and esplanade reserves. These criteria must be addressed when preparing an ODP within an ODP area in Lincoln. It is also relevant to consider this criteria where a change to an operative ODP is proposed, as noted in Policy B4.3.10.

General Policies

General policies which may be particularly relevant to Lincoln include:

Plar	Section	Objectives / Policies	Issue
1.1	Land and Soil	Policies B1.1.3 and B1.1.8	Possible contaminated soil in areas used for agricultural research in the past.
			Lincoln is surrounded by "versatile soils" except to the south-west (LUC Class I or II)
1.2	Water	Policy B1.2.2 and B1.2.5	Stormwater disposal to LI and LII.
			Reticulated sewerage.
2.1	Transport	Policy B2.1.8 and B2.1.9	Community perceptions of traffic on Gerald Street. Impact of growth of Lincoln and road links into Christchurch City
2.2	Utilities	Policies B2.2.1 and B2.2.4	Impacts of rate of residential growth on Lincoln's sewerage system
2.3	Community Facilities (and Reserves)	Policy B2.3.1	Impact of rate of residential growth at Lincoln on community facilities
3.4	Quality of the Environment	Policy B3.4.37	Reverse sensitivity issues, Business 3 and Rural zones

Reticulated Sewage

Reticulated sewage treatment and disposal is required at Lincoln.

PREBBLETON

Preferred Growth Option

The first preferred areas for expansion of Prebbleton are east and west of Springs Road, between the north and south limits of the existing Living and Business zones as identified in Appendix 31, inclusive of the additional 'Greenfield' development sites that are zoned Living Z and identified in the outline Development Plans contained in Appendix 42.

Specific Policies

Policy B4.3.64

Encourage land located to the east and west of the existing Living and Business zones, being those Living and Business zones that adjoin Springs Road, which is located as close as possible to the existing township centre as the first preferred areas to be rezoned for new residential development at Prebbleton, provided sites are available and appropriate for the proposed activity.

Explanation and Reasons

Much land rezoned for residential development at Prebbleton has been to the south of the township, to avoid using land containing 'versatile soils' (LUC Class I or II). As a result, Prebbleton is developing in an elongated north-south pattern along Springs Road. The Council's preferred areas for future residential development are east and west of Springs Road located as close as possible to the existing town centre. This policy is consistent with:

- Creating a compact shaped township; Town Form Policy B4.3.6.
- Minimising effects on Springs Road as a Strategic Road; Policy B2.1.18.
- Minimising the length of "rural-residential" boundaries and potential for "reverse sensitivity" issues; Policy B3.4.39.

Although this area contains LUC Class II soils, there are other resource management effects of continuing residential development on soils that are not "versatile" at Prebbleton.

Policy B4.3.65

Discourage further expansion of Prebbleton township north or south of the existing Living zone boundaries adjoining Springs Road.

Explanation and Reasons

Springs Road is one of the busiest Strategic Roads in Selwyn District. Further elongation of Prebbleton township north-south along Springs Road is inconsistent with Policy B2.1.18 and Town Form Policy B4.3.6.

Policy B4.3.66

Encourage rezoning of any land for new business development to adjoin the existing Business 1 Zone on the east side of Springs Road.

Explanation and Reasons

Confining Business 1 Zones to one side of Springs Road may lessen the number of pedestrian and vehicle movements crossing the road than if Business 1 Zones were to occur on both sides of the road. This policy is consistent with Policy B2.1.3. Consolidating similar Business 1 Zones into one area in a township is consistent with policies B3.4.35 and B3.4.36.

Policy B4.3.67

Consider any potential adverse effects of rezoning land for new residential or business development at Prebbleton on the 'rural-urban' landscape contrast of the area with Christchurch City, as identified in the RPS.

Explanation and Reasons

Objective 3 and Policy 5 of Chapter 12 of the RPS identify an area of land between Christchurch City and a line extending from West Melton to Tai Tapu as having important landscape and amenity values. The RPS seeks to maintain this land in "rural and recreational" uses. Policy B1.4.17 of the District Plan addresses this issue.

Policy B4.3.68

Ensure that development within each of the Living Z zone Outline Development Plan areas identified on the Planning maps and Appendices within Prebbleton address the specific matters relevant to each ODP Area number listed below:

Prebbleton

Outline Development Area 1

- Provision of a primary loop road linking up to two access points off Blakes Road;
- Provision of a connection to The Paddocks subdivision to the north (Living 2A zone);
- Provision of pedestrian and cycle links through the ODP area to connect with the adjoining urban area to the west and east via Blakes Road;
- Provision of a comprehensive stormwater system that has sufficient capacity for the ODP area;
- Provision of a reticulated water supply and wastewater systems that have sufficient capacity for the ODP area;
- Provision of a local neighbourhood park;
- Houses developed along Blakes Road must front this road;
- Provision of a minimum net density of 10 households per hectare averaged over the ODP area.

Outline Development Area 2

- Provision of a primary north-south road connection through the area linking from Trents Road to the Cairnbrae subdivision (Living 1A6 Zone) to the north and to secure through connections to any future residential development that may establish on the opposite southern side of Trents Road;
- Provision of a comprehensive stormwater system that has sufficient capacity for the ODP area:
- Provision of reticulated water supply and wastewater systems that have sufficient capacity for the ODP area:
- Provision of local neighbourhood park;
- Houses developed along Trents Road must front this road;
- Houses developed along the north-western boundary with the Kingcraft Drive Existing Development Area to be of lower residential densities with appropriate fencing and boundary treatments at the rural residential/urban interface;
- Provision of a minimum net density of 10 households per hectare averaged over the ODP area.

Outline Development Area 3

- Provision of a primary north-south road connection through the area linking from Springs Road to Trents Road;
- Provision of a secondary east-west road connection through the area linking from the adjacent Sterling Park subdivision (Living X Zone) to the main north-south primary road connection within this ODP;
- Provision of pedestrian and cycle links within and through the ODP area to connect with the adjoining urban areas, including specifically a connection from Trents Road to the Prebbleton nature Park;
- Provision of a comprehensive stormwater system that has sufficient capacity for the ODP area;
- Provision of reticulated water supply and wastewater systems that have sufficient capacity for the ODP area;
- Provision of a local neighbourhood park and green links;
- Houses developed along Springs road and Trents Road must front these roads;
- Provision of a minimum net density of 10 households per hectare averaged over the ODP area;
- Houses developed along the north-western boundary with the Rural (Inner Plains) Zone to contain appropriate fencing and boundary treatments at the rural/urban interface.

Outline Development Area 4

- Provision of a primary road connection through the area linking from Station masters Way to Tosswill road;
- Provision of a secondary east-west road connection through the area to Prebbleton Central;
- Provision of a local east-west secondary connection through the area to conductors road;
- Provision of pedestrian and cycle links within and through the ODP area to connect with the adjoining urban areas;
- Provision of an integrated stormwater management scheme to service the wider catchment, which shall incorporate wetland and riparian margins that form part of an environmentally sustainable solution that enhances ecological, cultural and Te Taumutu Runanga values;
- Interim stormwater management solutions established in advance of the integrated scheme are to have sufficient capacity for the development area;
- Provision of reticulated water supply and wastewater systems that have sufficient capacity for the ODP area;
- Provision of an 'open space corridor' through to area to link up with the Prebbleton Central subdivision and Business 1 Zone, including capacity to support stormwater and pedestrian/cycling links;
- Provision of a local neighbourhood park;
- Houses developed along Tosswill Road must front this road;
- Provision of larger lots of at least 1,000m2 around the northern and eastern boundaries of the ODP Area, with appropriate perimeter fencing and planting to preserve the rural outlook and Port Hills views;
- Provision of medium density lots along the 'open space corridor', to incorporate appropriate
 permeable fencing treatments and set backs to promote passive surveillance and
 integration of housing with the wider reserve network;
- Provision of a minimum net density of 10 households per hectare averaged over the ODP area.

General Policies

General policies that may be particularly relevant to Prebbleton include:

Plan	Section	Objectives / Policies	Issue
1.1	Land and Soil	Policy B1.1.8	Many areas around Prebbleton contain "versatile soils" (LUC Class I or II)
1.2	Water	Policy B1.2.5	Reticulated sewage (see note below)
2.1	Transport	Policies B2.1.3, B2.1.9, B2.1.10 and B2.1.18	Effects of additional traffic on Springs Road.
			Growth of a township "straddling" a Strategic Road.
2.2	Utilities	Policy 2.2.1	Impact of rate of town growth on utilities
2.3	Community Facilities (and Reserves)	Policy 2.3.1	Impact of rate of town growth on community facilities
3.4	Quality of the Environment	Policy B3.4.38	Reverse sensitivity issues, Rural zone

Reticulated Sewage

Reticulated sewage treatment and disposal is required at Prebbleton.

RAKAIA HUTS

Preferred Growth Option

There may be more than one site that complies with all relevant provisions in the District Plan for the future expansion of Rakaia Huts.

Specific Policies

Policy B4.3.69

Avoid rezoning any land for new residential or business development on the south-west side of Pacific Drive and restrict further building development on existing Lots 58-108 as shown in Appendix 24.

Explanation and Reasons

Land on the south-west side of Pacific Drive is subject to flooding from the Rakaia River. For this reason the Council intends to discourage further zoning of land for living or business purposes in this area and restrict development on the lower terrace within the existing Living zone.

Policy B4.3.70

Ensure any land rezoned for new residential or business development at Rakaia Huts does not cause damage or disturbance to archaeological sites or sites that are culturally important to tangata whenua.

Explanation and Reasons

The area around the Rakaia River mouth is a traditional area of occupation and food gathering for local Māori. Remains of a moa hunting ground exist in the area. The Council encourages any person wanting to rezone land for new residential or business development at Rakaia Huts to consult with tāngata whenua and the New Zealand Historic Places Trust Pouhere Taonga about their interests in this area. This policy is consistent with Policy B3.3.2.

NOTE:

If an archaeological site is accidentally uncovered, it has interim protection from further disturbance under the Historic Places Act 1993 – see Part B, Section 3.3.

General Policies

General policies that may be particularly relevant to Rakaia Huts include:

Plar	Section	Objectives / Policies	Issue
1.2	Water	Policy B1.2.6	On-site effluent treatment and disposal system
3.1	Natural Hazards	Policies B3.1.2 and B3.1.3	Flooding and coastal erosion
3.3	Culture and Heritage	Policy B3.3.2	Sites of cultural importance to Tāngata Whenua

ROLLESTON

Preferred Growth Option

There may be more than one area that complies with all relevant plan provisions for the future expansion of Rolleston.

Specific Policies

Policy B4.3.71

Avoid rezoning land for new residential or business development (other than Business 2 and 2A Zoning), west of SH1 and the South Island Main Trunk Line (SIMTL).

Explanation and Reasons

Rolleston Township is currently confined to one side of SH1 and the SIMTL except for the Business 2 and 2A Zones. Policy B4.3.70 is consistent with Policy B2.1.17.

Policy B4.3.72

Avoid rezoning land for new residential development in areas shown under the Airport Flightpath Noise Contours for 50 dBA Ldn or greater, on Planning Map 013.

Explanation and Reasons

Land within Rolleston township is under an approach path for aircraft to Christchurch International Airport. Policy B4.3.71 is consistent with Policy B2.1.22.

Policy B4.3.73

Encourage land rezoned for new business development to adjoin an existing Business zone of similar character, where sites are available and appropriate for the proposed activity.

Policy B4.3.74

Encourage additional Business 2 or 2A Zones to locate west of SH1, preferably adjoining the existing Business 2 or 2A Zone.

Explanation and Reasons

Rolleston contains two Business 1 Zones. That part of the Business 1 Zone bounded by Rolleston Drive, State Highway 1, and Dick Roberts Place has site specific rules and it is intended to provide for the Selwyn District Council's district headquarters and for civic and community activities undertaken by the Council. It is not intended that commerce and retailing will locate in this part of the Business 1 Zone.

Policy B4.3.73 encourages any additional business development to adjoin these zones and create consolidated Business zones in the township, rather than the creation of several isolated zones. This policy is consistent with policies B3.4.35 and B3.4.36.

Keeping the Business 2 and 2A Zones west of SH1 provides a "buffer" area between these activities and the township, which may help reduce potential "reverse sensitivity" issues. Policy B4.3.74 is consistent with Policy B3.4.38.

Policy B4.3.75

Encourage integration between rezoning land for new residential development at Rolleston and associated provisions for utilities, community facilities and areas for business development.

Explanation and Reasons

A study prepared for the Council (Barber, 1999: Demand for Land for Commercial and Industrial Uses in Selwyn District) indicates that if projected residential growth occurs at Rolleston, there will be a shortage of space for associated business activities in the land currently zoned Business 1.

Future residential growth at Rolleston will also result in the need for additional or upgraded utilities and community facilities. Any application to rezone land for a new residential area needs to address these effects in the same way as the Council is required to, should it rezone land for additional residential areas. This policy is consistent with policies B2.2.1 and B2.3.1.

Policy B4.3.76

Encourage new residential development by further subdivision in existing Living 2 Zones where it complies with the objectives and policies of the Plan.

Explanation and Reasons

Residential density and allotment sizes in the Living 2 Zones at Rolleston average either 0.5 ha to 1 ha. These zones cover large areas. Policy B4.3.76 allows Rolleston township to grow through closer residential development in the Living 2 zones, provided: there are no adverse effects on infrastructure; and closer density is supported by the residents in the areas affected. This policy is consistent with Policy B4.1.3 and Town Form Policy B4.3.6.

Policy B4.3.77

Ensure that development within each of the Outline Development Plan areas identified on the Planning Maps and Appendices within Rolleston addresses the specific matters relevant to each ODP Area number listed below:

Outline Development Plan Area 1

- Provision of local secondary road connections through the area to Dunns Crossing Road, Brookside Road, and Stonebrook Drive;
- Provision of pedestrian and cycle links within and through the ODP area to connect with the adjoining urban area to the east and south;

- Provision of a reticulated wastewater system with capacity to accommodate necessary flows;
- Potential provision of primary school subject to agreement with Ministry of Education;
- Provision of one Local Centre;
- Provision of a 40m noise abatement zone from State Highway 1;
- Provision of local neighbourhood parks and a key open space corridor along the alignment of the unformed East Maddisons Road. Green pedestrian and cycle linkages are to be provided from the balance of the area to this open space corridor;
- Provision of a mix of housing densities within the area, with medium density housing to be located in close proximity to local parks and the central open space corridor;
- Provision of a minimum net density of 11 households per hectare averaged over the ODP area.

Outline Development Plan Area 2

- Vehicle access to be via a secondary road connection from the Markham Way cul-de-sac with no direct vehicle access onto Norman Kirk Drive or Rolleston Drive;
- Provision of wells and water pumping facilities to provide sufficient capacity for all future growth in this area, including main trunk connections where necessary;
- Provision of a reticulated wastewater system with capacity to accommodate necessary flows;
- The stopping of an unformed portion of road 'Dick Roberts Place' along the area's western boundary will need to be addressed through a separate road stopping procedure;
- Identification of the whole of this ODP area as a medium density housing area, apart from a strip adjacent to the existing dwellings to the west where lower density lots are to be located to provide an area of transition between the existing dwellings and higher density units;
- Provision of pedestrian and cycle link between Rolleston School and Markham Way;
- Provision of a minimum net density of 20 households per hectare averaged over the ODP area.

Outline Development Plan Area 3

- Provision of local east-west secondary road connections through the ODP area to the existing "Park Lane" subdivision;
- No direct vehicle access from State Highway 1 and restricted vehicle access from Levi Road, to maintain the operational status of these primary roads, with lots fronting Levi Road to have an alternative access such as slip lanes;
- Provision of pedestrian and cycle links within and through the ODP area to connect with the adjoining urban area to the west and the rural area to the east;
- Provision of a comprehensive stormwater system that has sufficient capacity for the ODP area;

- Provision of reticulated water supply and wastewater systems that have sufficient capacity for the ODP area:
- Provision of local neighbourhood parks, including a gateway reserve on the southeastern corner of the ODP area adjacent to Levi Road;
- Provision of larger lots of at least 1,000m² around the northern, eastern, and southern boundaries of the ODP area;
- Provision of a minimum net density of 10 households per hectare averaged over the ODP area.

Outline Development Plan Area 4

- Provision of a local secondary road connection to Levi Road and Lincoln Rolleston Road;
- Provision of pedestrian and cycle links within and through the ODP area to connect with the adjoining urban area to the west and the rural area to the east;
- Provision of a comprehensive stormwater system that has sufficient capacity for the ODP area;
- Provision of reticulated water supply and wastewater systems that have sufficient capacity for the ODP area;
- Provision of a minimum net density of 15 households per hectare averaged over the ODP area.

Outline Development Plan Area 5

- Provision of District-function recreation and open space facilities;
- Provision of pedestrian and cycle links within and through the ODP area to connect with the adjoining urban area and ODP area 6.

Outline Development Plan Area 6

- Provision of a main east-west primary road connection through the area from near the intersection of Goulds Road and East Maddisons Road to Springston Rolleston Road;
- Provision of two local north-south secondary road connections through the area, to provide connections between ODP area 5 to the north and the primary road noted above and future urban areas to the south;
- Provision of pedestrian and cycle links within and through the ODP area to connect with the adjoining urban area and ODP area 5;
- Provision of a comprehensive stormwater system that has sufficient capacity for the ODP area;
- Provision of reticulated water supply and wastewater systems that have sufficient capacity for the ODP area;
- Provision of one neighbourhood centre;
- Provision of local neighbourhood parks;

 Provision of a mix of low and medium density housing areas with a minimum net density of 12 households per hectare averaged over the ODP area.

Outline Development Plan Area 7 (Park Grove) - Living Z

- Provision of a main east-west primary road connection through the area linking up to two access points off Overbury Crescent:
- Provision of a 40m noise abatement zone from State Highway 1;
- Provision of pedestrian and cycle links within and through the ODP area to connect to adjoining urban areas;
- Provision of a comprehensive stormwater system that has sufficient capacity for the ODP area:
- Provision of reticulated water supply and wastewater systems that have sufficient capacity for the ODP area:
- Provision of a minimum net density of 19 households per hectare averaged over the ODP area.

Outline Development Plan Area 8 (Park Lane) - Living Z

- ODP Area 8 to align with ODP Area 3;
- Provision of main north-south primary road connection from Marlowe Place to Beaumont Drive:
- Provision of three local east-west secondary road connections to ODP Area 3 (including one connect from Shadbolt Drive to ODP Area 3);
- Provision of a 40m building setback from the State Highway 1;
- No direct access off State Highway 1;
- Provision of pedestrian and cycle links within and through the ODP area to connect to adjoining urban areas;
- Provision of a comprehensive stormwater system that has sufficient capacity for the ODP area;
- Provision of reticulated water supply and wastewater systems that have sufficient capacity for the ODP area;
- Provision of local neighbourhood parks;
- Provision of a minimum net density of 10 households per hectare averaged over the ODP area.

Outline Development Plan Area 9 (Helpet Park) - Living Z Deferred

- ODP Area 9 to align with ODP Area 11;
- Provision of a local north-south secondary road connection from Lowes Road to ODP Area 11;
- Provision of a local east-west secondary road connection from Springston Rolleston Road to Lincoln Rolleston Road;
- Provision of pedestrian and cycle links within and through the ODP area to connect to adjoining urban areas;
- Provision of a comprehensive stormwater sytem that has sufficient capacity for the ODP area;
- Provision of reticulated water supply and wastewater systems that have sufficient capacity for the ODP area;
- Provision of a minimum net density of 10 households per hectare averaged over the ODP Area.

Outline Development Plan Area 10 (East Maddisons / Goulds Road) - Living Z Deferred

- ODP Area 10 to align with ODP Area 6;
- Provision of main east-west primary road connection from East Maddisons Road to Goulds Road;
- Provision of local east-west secondary road connections from East Maddisons Road to Goulds Road;
- Provision of pedestrian and cycle links within and through the ODP area to connect to adjoining urban areas;
- Provision of a comprehensive stormwater system that has sufficient capacity for the ODP area:
- Provision of reticulated water supply and wastewater systems that have sufficient capacity for the ODP area;
- Provision of a minimum net density of 10 households per hectare averaged over the ODP Area.

Outline Development Plan Area 11 (Branthwaite Drive)

- ODP Area 11 to align with ODP Area 9;
- Provision of main north-south primary road connection from ODP Area 9 linking up with Branthwaite Drive and to future development to the south of ODP Area 11;
- Provision of main east-west primary road connection from Springston Rolleston Road to Lincoln Rolleston Road:
- Provision of two local east-west secondary road connections from Springston Rolleston Road to Lincoln Rolleston Road:

- Provision of pedestrian and cycle links within and through the ODP area to connect to adjoining urban areas;
- Provision of a comprehensive stormwater system that has sufficient capacity for the ODP area;
- Provision of reticulated water supply and wastewater systems that have sufficient capacity for the ODP area;
- Potential provision of two Local Centres;
- Potential provision of one partial Neighbourhood Centre;
- Provision of local neighbourhood parks;
- Provision of a minimum net density of 14 households per hectare averaged over the ODP Area;

Explanation and Reasons

A Structure Plan has been prepared to facilitate the integrated growth of Rolleston Township to accommodate an additional 5,375 households by 2041. The Rolleston Structure Plan is based on projected figures from the Urban Development Strategy and Regional Policy Statement, which identify Rolleston as a south-western growth centre.

Due to the large amount of land within the township Urban Limit, the Greenfield growth area has been broken into a number of Outline Development Plan (ODP) areas. Each area is to be developed in accordance with an individual outline development plan while achieving the strategic outcomes sought for the entire township.

Each ODP area has been identified predominately by using significant features that cause severance e.g. major roads, whilst also taking into consideration the ability to provide for infrastructure. Smaller ODP areas have generally been avoided as they will reduce the potential to achieve a strategic outcome. One exception in Rolleston is ODP Area 2 due to it being the last large vacant lot close to the existing town centre available for comprehensive development. Comparatively, providing one ODP for the entire Township will create difficulties with multiple land ownership, zoning and staging such that the ability to readily implement future growth, particularly in the short term is likely to be heavily constrained by such difficulties. It is considered therefore that the approach taken with identified ODP areas is both the most practical in terms of maintaining the strategic objective and enabling development to continue in the short term. It is also considered that this approach is consistent with the Regional Policy Statement.

Whilst an ODP is generally conceptual in nature, some specific criteria have been listed which must be addressed in an ODP for any of the identified ODP areas. Each ODP area within Rolleston has specific requirements that have been identified through the Structure Plan process. Where an ODP is not already prepared, important infrastructure requirements have been listed under this specific criteria and these include matters such as roading links, water supply, stormwater management, wastewater treatment and disposal, and pedestrian and cycle links. These criteria must be addressed when preparing an ODP within an ODP area in Rolleston. It is also relevant to consider this criteria where a change to an operative ODP is proposed, as noted in Policy B4.3.10.

General Policies

General policies that may be particularly relevant to Rolleston include:

Plan Section		Objectives / Policies	Issue
1.1	Land and Soil	Policy B1.1.8	'Versatile soils' are located north of the township (LUC Class I or II)
1.2	Water	Policy B1.2.5	Reticulated sewage is required
2.1	Transport	Policies B2.1.17, B2.1.22 and B2.1.23	Confining Rolleston to one side of SH1/SIMTL.
			Christchurch International Airport
2.2	Utilities	Policy 2.2.1	Impact of rate of town growth on utilities
2.3	Community Facilities (and Reserves)	Policy 2.3.1	Impact of rate of town growth on community facilities
3.4	Quality of the Environment	Policies B3.4.35, B3.4.36 and B3.4.38	Consolidating Business zones and reverse sensitivity effects
4.1	Residential Density	Policies B4.1.2 and B4.1.3	Further subdivision, Living 2 zones

SHEFFIELD/WADDINGTON

Preferred Growth Option

There may be more than one area that complies with all the relevant plan provisions for the future expansion of Sheffield/Waddington.

Policy B4.3.77

Encourage new residential or business activities to use sites in the existing Living 1 Zone, if sites are available and appropriate for the proposed activity.

Explanation and Reasons

There is land not used for residential activities in the existing Living 1 zone at Sheffield and Waddington. Using this land is consistent with Town Form Policy B4.3.4.

Policy B4.3.78

Encourage any land rezoned for new residential or business development to occur north of the existing Living 1 Zone boundaries at Sheffield; north of the existing Living 1 Zone at Waddington; and south of the existing Living 1 Zone at Sheffield, but not on to the base of the Malvern Hills.

Policy B4.3.79

Discourage rezoning land for new residential or business development (other than Business 2) that is located:

- North east or west of the Living 1 Zone at Sheffield; or
- East of the Living 1 Zone at Waddington

Explanation and Reasons

Sheffield Township straddles SH73 and the Midland Railway Line. Waddington Township is confined to one side of SH73 and the Midland Railway Line. Policy B4.3.78 and B4.3.79 are consistent with policies B2.1.17 and B2.1.18. It is also consistent with Policy B1.4.13 which protects the landscape values of the Malvern Hills.

Policy B4.3.80

Encourage any land zoned for business development to be located on one side of SH73 only.

Explanation and Reasons

Activities which locate in Business 1 Zones tend to have relatively high numbers of vehicle or pedestrian movements compared with activities in Living zones. Policy B4.3.80 is to discourage these activities from locating on both sides of SH73 and the Midland Railway Line, so as to avoid a potential increase in the number of people and motor vehicles moving across transport routes.

This is consistent with Policy B2.1.17. It is also consistent with policies B3.4.35 and B3.4.36 which encourage Business zones to consolidate in one area of a township.

Policy B4.3.81

Ensure any land rezoned for residential or business development south of Vogel Street does not create or exacerbate a natural hazard.

Explanation and Reasons

The area to the south of the Living zones beyond Vogel Street is terraced. An assessment of the natural hazards and stability affecting the upper terrace (Living 1A) has been undertaken and the area is generally suitable for residential development. However particular sites, including those close to the edge of the upper terrace, may require further investigation at the time that applications for subdivision and/or building consents are made. Before any residential or business development takes place on the lower terrace further investigation will be needed relating to land stability and the risk of flooding, including that caused by the flow of stormwater from the upper terrace.

General Policies

General policies that may be particularly relevant to Sheffield/Waddington include:

Plan Section		Objectives / Policies	Issue
1.2	Water	Policy B1.2.6	On-site effluent treatment and disposal system
1.4	Outstanding Natural Features and Landscapes	Policy B1.4.13	Avoid residential or business development on Malvern Hills
2.1	Transport	Policies B2.1.17, B2.1.18	SH73 and Midland Railway Line
3.1	Natural Hazards	Policy B3.1.2	Terraced area south of Vogel Street

SOUTHBRIDGE

Preferred Growth Option

There may be more than one area for the future expansion of Southbridge that complies with all relevant provisions in the Plan.

Specific Policies

Policy B4.3.82

Encourage new residential or business areas to locate on sites in the existing Living and Business zones, if sites are available and appropriate for the proposed activity.

Explanation and Reasons

There is land not currently used for residential or business activities in the existing Living and Business 1 Zones at Southbridge. The use of that land for new residential or business activities is consistent with Town Form Policy B4.3.4.

Policy B4.3.83

Ensure that any land rezoned for new residential or business development in the existing Living 1 Zone along Gordon Street and High Street, does not create or exacerbate a flooding hazard.

Explanation and Reasons

The drain in this area tends to overflow in heavy rainfall, therefore flooding the area. Any new residential or business development should not occur on land which has a reasonable risk of flooding. It should not increase the risk of flooding "downstream" by increasing the rate of rainfall runoff into the drain. This policy is consistent with Policy B3.1.2.

Policy B4.3.84

Encourage any new Business zone to adjoin an existing Business zone of similar character, if sites are available and appropriate.

Explanation and Reasons

Southbridge has Business 1 and 2 Zones. Policy B4.3.84 is to encourage consolidated Business zones in townships, rather than the creation of several isolated zones. This policy is consistent with policies B3.4.35 and B3.4.36.

Policy B4.3.85

Ensure any land rezoned for new residential or business development does not create or exacerbate "reverse sensitivity" issues in respect of activities in the existing Business 2 Zone.

Explanation and Reasons

Southbridge has an existing Business 2 Zone that adjoins the Living zone in all areas except west of High Street. Some activities in the Business 2 Zone may have effects that are incompatible with residential activities. Ensuring there are options for the Business 2 areas to continue or expand without creating "reverse sensitivity" issues will promote sustainable management. This policy is consistent with policies B3.4.38 and B3.4.39.

General Policies

General policies which may be particularly relevant to Southbridge include:

Plan Section		Objectives / Policies	Issue	
1.1	Land and Soil	Policy B1.1.8	Southbridge is surrounded by "versatile" soils – LUC Class I or II	
1.2	Water	Policy B1.2.5	Reticulated sewage treatment and disposal is required at Southbridge (see note below).	
2.2	Utilities	Policies B2.2.1, B2.2.3 and B2.2.4	There is no public reticulated sewerage system available at Southbridge at present (see note below)	
3.1	Natural Hazards	Policy B3.1.2	Flooding from the drain in Gordon Street / High Street area	
3.4	Quality of the Environment	Policies B3.4.35 - B3.4.39	Reverse sensitivity in Business 2 zones	

Sewage Treatment and Disposal

Reticulated sewage treatment and disposal is required at Southbridge – see Policy B1.2.5.

SPRINGFIELD

Preferred Growth Option

There may be more than one area for the future expansion of Springfield that complies with all relevant provisions in the Plan.

Specific Policies

Policy B4.3.86

Encourage new residential or business activities to use sites in the existing Living 1 Zone, if sites are available and appropriate for the proposed activity.

Explanation and Reasons

There is land not used for residential or business activities in the existing Living 1 zone at Springfield. Using land in the existing zone is consistent with Town Form Policy B4.3.4.

Policy B4.3.87

Encourage any new Living zone to occur on the north side of SH73 and avoid new Living or Business 1 Zones:

- East of the existing Living 1 zone;
- On the south side of SH73; or
- North of the Midland Railway Line.

Explanation and Reasons

Springfield Township is currently confined to one side of a Strategic Road (SH73) and the Midland Railway Line. Policy B4.3.86 is consistent with Policy B2.1.17, in keeping the township confined to one side of both transport routes.

The Russell Range is identified in the Plan as having special landscape values. Confining new residential or business development to the north side of SH73 is consistent with Policy B1.4.13.

Policy B4.3.88

Ensure that any land rezoned for new residential or business development north of Springfield does not create or exacerbate potential "reverse sensitivity" issues in respect of the Midland Railway Line.

Explanation and Reasons

Springfield Township is "sandwiched" between SH73 and the Midland Railway Line. There is some land zoned "Rural" between the Living 1 Zone and the Railway Line. The land was designated for railway purposes but this designation is not now required. This area may be suitable for the expansion of Springfield. However, potential "reverse sensitivity" effects with noise, dust and vibration from trains using the Midland Railway Line would need to be addressed.

General Policies

General policies that may be particularly relevant to Springfield include:

Plan Section		Objectives / Policies	Issue
1.2	Water	Policy B1.2.6	On-site effluent treatment and disposal system
1.4	Outstanding Natural Features and Landscapes	Policy B1.4.13	Landscape values of Russell Range
2.1	Transport	Policies B2.1.17	Confining growth of Springfield to one side of the Midland Railway Line and SH73.

SPRINGSTON

Preferred Growth Option

There may be more than one area for the future expansion of Springston that complies with all relevant provisions in the Plan.

Specific Policies

Policy B4.3.89

Encourage any land rezoned for new residential or business development at Springston to create compact shaped areas behind the existing Living zones, with frontage on to Leeston, Waterholes or Ellesmere Junction Roads, if sites are available and appropriate.

Explanation and Reasons

Springston Township has developed around the intersections of Ellesmere Junction Road/Waterholes Road and Ellesmere Junction Road/Leeston Road. The Living 1 Zone is often only "one section deep" with houses fronting these roads. The preferred option for future expansion of Springston is by using land behind the existing Living 1 Zone rather than extending it further along these arterial roads. This policy is consistent with Town Form Policy B4.3.6. The use of some of this land for future residential or business development is complicated by its tenure as part of the 'Gammack Estate'.

Policy B4.3.90

Avoid rezoning land for new residential or business development:

- Further west along Ellesmere Junction Road than the Domain;
- Further east along Ellesmere Junction Road than the Living 1A Zone.

Policy B4.3.91

Avoid rezoning land for new residential or business development along both sides of Waterholes Road or both sides of Leeston Road.

Explanation and Reasons

Springston Township had developed along the intersection of three main roads, Leeston Road, Ellesmere Junction Road and Waterholes Road. In many parts houses are only one row deep along the road frontage. Policy B4.3.91 is to avoid extending the township further along Ellesmere Junction Road. This policy is consistent with Policy B2.1.18.

Policy B4.3.91 is to avoid creating a township that "straddles" both sides of Leeston or Waterholes Road. The policy contemplates extending the zone boundary along these roads, but preferably on one side only. This policy is consistent with Policy B2.1.17.

Policy B4.3.92

Ensure that any land rezoned for new residential or business development does not create or exacerbate a natural hazard from flooding of the Springston drainage network.

Explanation and Reasons

The land to the east and south east of the existing Living 1 Zone along Leeston Road has a high water table. If it is used for a new residential area, the water ponding in this area will need to be collected and disposed of. That activity should not increase the risk of local drains overflowing and flooding land 'downstream'.

General Policies

General policies that may be particularly relevant to Springston include:

Plan Section		Objectives / Policies	Issue
1.1	Land and Soil	Policy B1.1.8	Springston is surrounded by "versatile soils" – LUC Class I or II
1.2	Water	Policies B1.2.1 and B1.2.5	The public reticulated water supply at Springston will need upgrading if it is used to supply any new allotments.
			Reticulated sewage.
2.1	Transport	Policies B2.1.17, B2.1.18	Further growth of townships along Strategic Roads
2.2	Utilities	Policies B2.2.2 and B2.2.5	Upgrades to water supply and sewerage (see notes below).
			Alternative to public water supply.

Water and Sewage

A reticulated water supply and sewage treatment and disposal is required at Springston – see policy B1.2.3 and B1.2.5.

Gammack Estate

Land held by the Gammack Trust surrounds Springston to the west, north-west and north-east. The Trustees do not have power to sell this land (it has to be done through an Act of Parliament). There is also debate whether the land can be used for purposes other than farming, due to the wording of the Will of the late James Gammack.

TAI TAPU

Preferred Growth Options

There may be more than one area for the future expansion of Tai Tapu that complies with all relevant provisions in the Plan.

Specific Policies

Policy B4.3.93

Encourage new residential or business activities to locate on sites that have the least risk of being subject to flooding.

Explanation and Reasons

Tai Tapu is located in a stormwater ponding area for the surrounding catchment. The township is lower lying than the adjoining Halswell River. Parts of the township are flooded by stormwater ponding during quite regular heavy rainfall events (1 in 5 year storms). Policy B4.3.93 is to encourage new residential or business activities to locate on sites that are least subject to flooding. This may mean avoiding some sites in the existing Living 1A or 2A Zones in favour of rezoning a new area. In this case, the Council considers it better promotes sustainable management to utilise sites that are less prone to natural hazards, even if it results in a less consolidated township. Any new zoning must accord with Policy B4.3.94, and all other relevant District Plan policies.

Policy B4.3.94

Ensure any land rezoned for new residential or business development does not increase potential natural hazards from flooding.

Explanation and Reasons

Any new residential or business development should not increase potential natural hazards from flooding at Tai Tapu. This includes:

- Locating buildings in areas subject to flooding.
- Earthworks or buildings that divert floodwater onto other property; or
- Increasing the risk of flooding downstream by increasing the rate of rainfall runoff into the Halswell River or waterbodies in the catchment. This policy is consistent with policies B3.1.2, B3.1.4 and B3.1.5.

Policy B4.3.95

Avoid rezoning land for new residential or business development along both sides of SH75.

Explanation and Reasons

Tai Tapu township straddles a Strategic Road - SH75. Policy B4.3.95 is to avoid extending this pattern by confining future north-south expansion of the township to one side of SH75. This

policy is consistent with Policy B2.1.18. Future development should also occur in a manner that promotes a compact or consolidated town shape consistent with Town Form Policy B4.3.6.

Policy B4.3.96

Consider any potential adverse effects of rezoning land for new residential or business development at Tai Tapu on the "rural-urban" landscape contrast of the area with Christchurch City, as identified in the RPS.

Explanation and Reasons

Objective 3 and Policy 5 of Chapter 12 of the RPS identify an area of land between Christchurch City and a line extending from West Melton to Tai Tapu as having important landscape and amenity values. The RPS seeks to maintain this area in "rural and recreational" uses. Policy B1.4.17 of the District Plan addresses this issue.

General Policies

General policies that may be particularly relevant to expansion of Tai Tapu include:

Plan Section		Objectives / Policies	Issue
1.1	Land and Soil	Policy B1.1.8	Land around Tai Tapu contains "versatile soils" – LUC Class I or II
1.2	Water	Policy B1.2.1 and B1.2.5	Effects of stormwater disposal on water quality in Halswell River and other waterbodies.
			Reticulated sewage treatment and disposal.
1.4	Outstanding Natural Features and Landscapes	Policy B1.4.17	Landscape contrast with Christchurch City
2.1	Transport	Policy B2.1.18	Further expansion of a township which "straddles" a Strategic Road
4.3	Residential and Business Development	Town Form Policy B4.3.6	Expansion of townships in a compact shape.

WEST MELTON

Preferred Growth Option

Substantial growth is expected to take place in and around West Melton. The focal point of this growth will be either side of Weedons Ross Road north of State Highway 73, but not extending north of Halkett Road. A lesser extent of lower density residential development will occur south of State Highway 73. The township is serviced with reticulated sewerage. Any further development will also be expected to be serviced by a reticulated sewerage system.

Specific Policies

Policy B4.3.97

Provide a primary focus for new residential or business development north of State Highway 73 and south of Halkett Road, and to allow only a limited extent of new low density residential development south of State Highway 73.

Explanation and Reasons

West Melton has developed with community facilities on both the northern and southern sides of State Highway 73. Residential development has taken place north of the highway centred on Westview Crescent. The primary focus for future growth of the township is to be provided for north of the State Highway. Limited new residential growth will be enabled south of the highway but will be limited in extent and density to minimise effects on the safety and efficiency of the highway. A pedestrian/cycle link will also be provided under the highway to provide an alternative connection between the two areas. This pattern of growth is consistent with maintaining a consolidated form for the future growth of the township, and with Policy B2.1.18 and Town Form Policy B4.3.6.

Policy B4.3.98

Promote a consolidated pattern of future urban growth in West Melton.

Explanation and Reasons

West Melton's future growth is to adhere to a form of development which maintains a generally compact shape. While allowing a substantial amount of urban growth with higher densities north of State Highway 73, development south of the highway is to be limited in extent and density. This pattern is consistent with Town Form Policy B4.3.6.

Policy B4.3.99

Avoid using Laird Place or Westview Crescent as collector roads to access any significant new residential or business areas, in West Melton.

Explanation and Reasons

Westview Crescent and Laird Place are local roads that provide access to properties in the existing West Melton village. The Council prefers a new collector road off Weedons Ross Road to provide access to any new residential or business area on the west side of Weedons Ross Road, rather than any attempt to upgrade these roads. A small area of additional residential development will be provided for off Laird Place. This policy is consistent with Policy B2.1.8.

Policy B4.3.100

Promote new residential areas in West Melton that maintain the lower residential density of the existing village, where practical, whilst providing for the efficient and effective development of the Living WM zone.

Explanation and Reasons

West Melton village is an area with larger section sizes than those found in most townships in Selwyn District, particularly those close to Christchurch. Policy B4.3.100 recognises the character of the existing village and the support for larger section sizes in the Township survey results for West Melton (November 1998). A wide variety of lot sizes in response to market demand, have been provided for, but recognising the potential for West Melton to provide a lower density alternative living environment near Christchurch. However, the efficient and effective development of the Living WM zone must be provided for to achieve the anticipated residential growth for this zone.

Policy B4.3.101

Require any community reticulated sewage treatment and disposal system at West Melton to be designed so it can be connected to the public system when it becomes available.

Explanation and Reasons

Reticulated sewage treatment and disposal is required at West Melton (see Policy B1.2.5). A reticulated system is now available to cater for planned township growth. The Council prefers any community scheme used in the interim to be able to connect into the public system, so residents can transfer their responsibility for the maintenance and repair of their system to the Council. Any community reticulated scheme used at West Melton shall comply with Policy B2.2.4.

General Policies

General policies which may be particularly relevant to West Melton include:

Plan	Section	Objectives / Policies	Issue
1.1	Land and Soil	Policy B1.1.8	Versatile soils are located to the south-west of the existing village, (LUC Class I and II)
1.2	Water	Policy B1.2.5	Protection zone for any new water supply
2.1	Transport	Policy B2.1.18	Township largely confined to one side of a Strategic Road
2.2	Utilities	Policy B2.2.1	An upgraded community water supply will be needed for new residential or business development at West Melton
2.4	Waste Disposal	Policies B2.4.2 and B2.4.5	Reticulated sewage treatment and disposal is needed
4.1	Residential Density	Policies B4.1.3 and B4.1.9	Residential density in new residential areas

WHITECLIFFS

Preferred Growth Option

The preferred option for residential or business development at Whitecliffs is to use sites within the existing Living 1 Zone, provided use of those sites complies with the District Plan policies.

Specific Policies

Policy B4.3.102

Encourage new residential or business activities to use sites in the existing Living 1 Zone, provided the use of any such site complies with Policy 3 and the policies of the District Plan.

Explanation and Reasons

There are many sites within the existing Living 1 Zone at Whitecliffs that are not currently used for residential or business activities. The preferred option for expansion of Whitecliffs is to utilise sites in the Living 1 Zone. This policy is consistent with the Town Form Policy B4.3.4. However, some sites within the Living 1 Zone of Whitecliffs may not be suitable for residential or business activities. Some areas are prone to flooding from stormwater runoff from the Malvern Hills. Some sites south of Whitecliffs Road are subject to erosion from the Selwyn River/Waikirikiri. Conventional on-site effluent treatment and disposal systems will not work on some sites due to ground conditions.

Policy B4.3.103

Ensure any land rezoned for new residential or business development does not create or exacerbate:

- Inundation from stormwater running off the Malvern Hills;
- Landslip from the Malvern Hills; or
- Erosion of the Selwyn River/Waikirikiri Terrace on the south side of Whitecliffs Road.

Explanation and Reasons

Sites within and around the Living 1 Zone at Whitecliffs may be subject to: flooding from stormwater ponding; erosion from slumping land on the Malvern Hills; or erosion by the Waikirikiri/Selwyn River. Any new business or residential development should not result in:

- Buildings being erected on land subject to flooding or erosion; or
- Increases in stormwater runoff on to other sites, including sites "downstream" of the Waikirikiri/Selwyn River; or
- Increases in the rate or risk of erosion on other land through protection works on that site.

This policy is consistent with Policy B3.1.2.

General Policies

General policies that may be particularly relevant to expansion of Whitecliffs, include:

Plan Section		Objectives / Policies	Issue	
1.2	Water	Policy B1.2.6	On-site effluent treatment and disposal system	
1.4	Outstanding Natural Features and Landscapes	Policies B1.4.13 and B1.4.14	Protect landscape values of Malvern Hills and natural character of Waikirikiri/Selwyn River	
3.1	Natural Hazards	Policy B3.1.2	Flooding, landslip and erosion	

B4.4 DEVELOPMENT CONTRIBUTIONS — ISSUES

The distribution of the costs between private parties and general rates for:

- Providing reserves and network and community infrastructure; and
- Mitigating the fiscal effects of providing growth related infrastructure

What are Development Contributions?

The term development contributions, defined in Section 197 of the Local Government Act 2002, means a contribution:

- a) provided for in a development contribution policy included in the long-term council community plan of a territorial authority; and
- b) calculated in accordance with the methodology; and comprising:
 - i) money;
 - ii) land, including a reserve or esplanade reserve (other than in relation to a subdivision consent), but excluding Maori land within the meaning of Te Ture Whenua Maori Act 1993, unless that Act provides otherwise; or
 - iii) both.

Development contributions can be taken to provide for:

- Reserves
- Network infrastructure
- Community infrastructure

In most cases provision for reserves (for open space and recreation), land for esplanade purpose, network infrastructure or community infrastructure is made at the time of subdivision of the land. However, in some instances land use development and activities are undertaken without associated subdivision of land.

Contributions of land and/or cash arising from growth-related development can be required by the Council either under the Resource Management Act by way of the District Plan or under the Local Government Act 2002 by way of a Development Contribution Policy in the Long Term Plan (LTP). The Council has developed a Development Contribution Policy within the requirements of the Local Government Act. Accordingly, the Council's requirements for land and/or cash for the provision of growth-related reserves and for network and community infrastructure are contained within the Development Contribution Policy and such contributions are no longer taken under the District Plan. Requirements for the provision of esplanade reserves/strips and provisions relating to environmental compensation and the form of land contributions are included within the Subdivision provisions of the District Plan, while requirements for the provision of financial contributions to mitigate environmental damage are included as policies in Part B Section 3, Peoples Health, Safety and Values.

Where costs are incurred in relation to maintenance of infrastructure or for improvements in service levels, these costs are met through targeted rates rather than development contributions. Therefore, the purposes for which development contributions are taken and the proportion of costs which are funded through development contributions is an important part of the Council's financial planning.

DEVELOPMENT CONTRIBUTIONS — STRATEGY

The provision to take development contributions under the LTP Development Contribution Policy will be complimentary to the subdivision consent process. Esplanade reserves/strips will be taken under the subdivision consent process, while consideration of the form of land to be provided as reserves will also be considered at the time of subdivision.

The LTP Development Contribution Policy will ensure that those responsible for development and/or subdivision that places additional demands on the Councils provision of reserves and network or community infrastructure will contribute a fair and reasonable contribution towards the expansion of those services.

DEVELOPMENT CONTRIBUTIONS — OBJECTIVES

Objective 4.4.1

The parties creating the need for the expenditure meet the costs of establishing or upgrading reserves and network and community infrastructure and developing and enhancing the recreational and amenity values of the District.

Objective 4.4.2

The Council uses its discretion to take development contributions under the LTCCP Development Contribution Policy in a transparent and consistent manner.

Explanation and Reasons

Development contributions are taken to help address the fiscal effects which a new development may have on the existing ratepayers of an area. If development contributions are not taken, the costs of addressing these effects must be funded from general rates.

DEVELOPMENT CONTRIBUTIONS — POLICIES AND METHODS

Policy 4.4.1

To ensure that subdividers and/or developers meet the costs of any required provision of works and services as a result of land use development and/or subdivision.

Explanation and Reasons

The Council has decided that all development contributions for new or upgraded reserves or network and community infrastructure as a result of development and/or subdivision will be in accordance with the Development Contribution Policy under the LTCCP and the Local Government Act 2002. This Policy will enable the Council to ensure that those responsible for development and/or subdivision that places additional demands on the Council's provision of reserves and network or community infrastructure will contribute a fair and reasonable contribution to the provision and expansion of these services.

Methods

LTP

Development Contribution Policy

District Plan

Subdivision

DEVELOPMENT CONTRIBUTIONS — ENVIRONMENTAL RESULTS

The following environmental results should occur from implementing this section:

New developments and subdivisions which increase demand on the Council's reserves and network and community infrastructure contribute towards the cost of provision and expansion of them.

DEVELOPMENT CONTRIBUTIONS — MONITORING

See Appendix 1.

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4 LIVING ZONE RULES — BUILDINGS

4.1 BUILDINGS AND NATURAL HAZARDS

Restricted Discretionary Activities — Buildings and Natural Hazards

- 4.1.1 Erecting any <u>dwelling</u> or other <u>principal building</u> on land located in the Living 1A or 2A zones at Tai Tapu where the minimum floor level is less than 6.93m above mean sea level shall be a restricted discretionary activity.
- 4.1.2 Under Rule 4.1.1 the Council shall restrict the exercise of its discretion to:
 - 4.1.2.1 The nature of any flooding or land instability and whether this makes the site unsuitable to erect the proposed building or undertake the proposed earthworks.
 - 4.1.2.2 Any effects of <u>buildings</u> or <u>earthworks</u> in displacing or diverting floodwaters and increasing the potential risk of flooding elsewhere.
 - 4.1.2.3 Any mitigation measures proposed.

Non-Complying Activities — Buildings and Natural Hazards

4.1.3 Erecting any new <u>dwelling</u>, or part <u>dwelling</u> thereof, or other <u>principal building</u>, on Lots 58 to 108 shown on the Plan attached as Appendix 24 at Rakaia Huts shall be a non-complying activity.

Prohibited Activities — Buildings and Natural Hazards

4.1.4 Erecting any <u>dwelling</u> or other <u>principal building</u> between any <u>waterbody</u> and any stop bank designed to contain flood water from that <u>waterbody</u> shall be a prohibited activity.

4.2 BUILDINGS AND LANDSCAPING

Permitted Activities — Buildings and Landscaping

- 4.2.1 Except for the Living 3 Zone at Rolleston identified in the Outline Development Plan in Appendix 39 and 40, any <u>principal building</u> shall be a permitted activity if the area between the <u>road boundary</u> and the <u>principal building</u> is landscaped with shrubs and:
 - Planted in lawn, and/or
 - Paved or sealed, and/or
 - Dressed with bark chips or similar material.

For the Living 3 Zone at Rolleston identified on the Outline Development Plan in Appendix 39 and 40 the following shall apply:

- 4.2.2 Any principal building shall be a permitted activity if:
 - i. That apart from one vehicle crossing and access not exceeding 100m² in area all land within the setback areas from roads as specified in Rule 4.9.26(i), excepting State Highway 1, will be devoted to landscaping; including the provision of at least one specimen tree capable of growing to at least 8m high being planted for every 10 metres of frontage and to be spaced at no less than 5 metres and no greater than 15 metres. The area between all road boundaries (other than with State Highway 1) and a line parallel to and 15m back from the road boundary is landscaped with shrubs and specimen trees covering as a minimum the lesser of 30% of the area or 250m²; and
 - ii. The number of specimen trees in this area is not less than 1 per 10m of road frontage or part thereof; and
 - iii. The trees are selected from the list below planted at a grade of not less than Pb95; and
 - iv. Shrubs are planted at 'aa' grade of not less than Pb3 and a spacing of not less than 1 per square metre, typically located within a garden area dressed with bark chips or similar material; and
 - v. Any paved surface area within the area does not exceed 100m² in area.
 - vi. The list of suitable specimen trees for the purpose of this rule is:
 - Maple, Silk Tree, Alder, Birch, River She Oak, Leyland Cypress, Monterey Cypress, Lacebark, American sweet gum, Magnolia, Pohutukawa, weeping Kowhai, Common Olive, Pine, Lemonwood, Kohuhu, Ribbonwood, Plane, Totara, Poplar, Oak, Elm, Michelia
 - vii. The Council will require a planting plan to be submitted at building consent stage, prepared by a suitably qualified landscape professional, identifying compliance with the above control.
 - viii. The landscaping shall be maintained and if dead, diseased or damaged, shall be removed and replaced.

Note: Rule 4.2.2 shall not apply to allotments of 4ha or greater in the Living 3 Zone identified on the Outline Development Plan in Appendix 39 and 40.

- 4.2.3 Any Fencing in the Living 3 Zone on a road or Rural zoned boundary or in the area between the road boundary and the line of the front of the principal building or in the area between the Rural Zone boundary and the line of the rear of the principal building:
 - Shall be limited to a maximum height of 1.2m, be at least 50% open, and be post and rail, traditional sheep, deer fencing, solid post and rail or post and wire only; and
 - Shall be of a length equal to or greater than 80% of the length of the front boundary, of a minimum height of 0.6m and be at least 50% open.

Except that nothing in the above controls shall preclude the use of other fencing types when located within 10m of the side or rear of the principal building. Such fence types shall not project forward to the line of the front of the building.

Note: Except that fences on boundaries adjoining reserve areas, cycleways or pedestrian accessways identified in the Outline Development Plan for Lincoln in Appendix 18 and for the Living 1A6 Zone in Prebbleton shall not exceed 1.2m in height.

Discretionary Activities — Buildings and Landscaping

4.2.4 Any activity which does not comply with Rule 4.2.1 or 4.2.2 shall be a discretionary activity.

Restricted Discretionary Activities — Buildings and Landscaping

- 4.2.5 Any activity which does not comply with Rule 4.2.3 shall be a restricted discretionary activity. Council shall restrict the exercise of its discretion to the consideration of:
 - 4.2.5.1 The extent to which the proposed fencing achieves high levels of visual transparency;
 - 4.2.5.2 The extent to which the proposed fencing is in keeping with rural character elements;
 - 4.2.5.3 Whether the proposed fencing is necessary as an integral part of a recreational facility such as a swimming pool or tennis court;
 - 4.2.5.4 Whether the proposed fencing is necessary for the care and management of specialist livestock.

4.3 BUILDINGS AND CONTAMINATED LAND

Refer to Rule 10.1 - Activities and Contaminated Land.

4.4 BUILDINGS AND WATER SUPPLY

Permitted Activities — Buildings and Water Supply

- 4.4.1 In all Living zones the erection of any <u>dwelling</u> or <u>principal building</u> connected to a reticulated water supply shall be a permitted activity, provided that it complies with the current New Zealand Drinking Water Standards.
- 4.4.2 In the Living 1 Zone at Lincoln, as shown in Appendix 35, rainwater storage tanks with a minimum capacity of 3000 litres shall be installed for each dwelling for non-potable uses, such as garden irrigation.

Discretionary Activities — Buildings and Water Supply

4.4.3 Any activity which does not comply with Rule 4.4.2 shall be a discretionary activity.

Non-Complying Activities — Buildings and Water Supply

4.4.4 Any activity which does not comply with Rule 4.4.1 shall be a non-complying activity.

4.5 BUILDINGS AND SEWAGE TREATMENT AND DISPOSAL

Permitted Activities — Buildings and Sewage Treatment and Disposal

- 4.5.1 In the Living zones at Castle Hill, Doyleston, Lake Coleridge Village, Leeston, Lincoln, Prebbleton, Rolleston, Southbridge, Springston, Tai Tapu, and West Melton, the erection of any <u>dwelling</u> or <u>principal building</u> shall be a permitted activity provided that it is connected to a reticulated sewage treatment and disposal system.
- 4.5.2 In all other Living zones in the district <u>dwellings</u> shall be permitted activities provided that they are serviced by on-site effluent treatment and disposal systems.

Non-Complying Activities — Buildings and Sewage Treatment and Disposal

4.5.3 Any activity which does not comply with Rules 4.5.1 or 4.5.2 shall be a non-complying activity

Notes

- 1. A discharge permit is required from Environment Canterbury to dispose of sewage on-site at Kirwee and Darfield.
- 2. If the Council and the community decide to install a reticulated sewage treatment and disposal system, the Council may require existing <u>dwellings</u> and <u>principal buildings</u> to connect, pursuant to provision in the Local Government Act 1974.

4.6 BUILDINGS AND BUILDING DENSITY

Permitted Activities — Buildings and Building Density

- 4.6.1 The erection on an <u>allotment</u> (other than a <u>site</u> at Castle Hill) of not more than either:
 - One dwelling and one family flat up to 70m² in floor area; or
 - One <u>principal building</u> (other than a <u>dwelling</u>) and one <u>dwelling</u>, shall be a permitted activity, except that within a comprehensive residential development within a Living Z Zone, more than one dwelling may be erected on the balance lot prior to any subsequent subdivision consent that occurs after erection of the dwellings (to the extent that the exterior is fully closed in).
- 4.6.2 The erection of not more than one <u>principal building</u> on any <u>site</u> at Castle Hill shall be a permitted activity.

4.6.2.1 The erection of any <u>dwellings</u> in the Living WM Zone shall comply with the building densities and locations shown on the Outline Development Plan and associated Layer Plans (appendix 20A) for this zone.

Restricted Discretionary Activities — Buildings and Building Density

- 4.6.3 Except as provided in Rule 4.6.6 the erection of not more than two <u>dwellings</u> on an <u>allotment</u> in a Living 1 zone shall be a restricted discretionary activity.
- 4.6.4 Under Rule 4.6.3 the Council shall restrict the exercise of its discretion to:
 - 4.6.4.1 Whether each <u>dwelling</u> has adequate outdoor living space for the exclusive use of that dwelling for residential activities; and
 - 4.6.4.2 Whether each outdoor living space will receive direct sunlight on the shortest day of the year; and
 - 4.6.4.3 Whether there is adequate privacy between the habitable rooms of the two <u>dwellings</u> erected on the same <u>allotment</u>; and
 - 4.6.4.4 The proportion of <u>allotments</u> in the street or subdivision where there is more than one <u>dwelling</u> or <u>principal building</u>; and
 - 4.6.4.5 Any adverse effects, including cumulative effects, on the residential density or sense of spaciousness of the area; and
 - 4.6.4.6 The need for a 'step in plan' to be provided at each 20 metre interval along a continuous <u>building</u> wall in order to mitigate any adverse effects of continuous '<u>building</u> bulk' being close to the <u>boundary</u> of a neighbouring property. The Step shall be sufficient spacing, depth, and length to provide a well articulated façade that provides visual variety and relief from long monotonous buildings.
 - 4.6.4.7 Within the Lowes Road Outline Development Plan Area, that the siting of the dwelling does not preclude the establishment of any roads or indicative walkways as shown in Appendix 34.

Note: <u>Building</u> density and <u>site</u> coverage rules both apply.

Discretionary Activities — Buildings and Building Density

4.6.5 Except as provided in Rule 4.6.6, the erection on any <u>allotment</u> of any <u>building</u> (other than an <u>accessory building</u>) which does not comply with Rule 4.6.1, 4.6.2.1 or Rule 4.6.3 shall be a discretionary activity in Living 1 zones and the Living WM Zone.

Non-Complying Activities — Buildings and Building Density

4.6.6 The erection on an <u>allotment</u> of any <u>building</u> (other than an <u>accessory building</u>) which does not comply with Rule 4.6.1 shall be a non-complying activity in the Living Z, 1A, 1A2, 1A3, 1A4 and Living 1A6 Deferred zones at Prebbleton and all Living Z, 2, 2A and Living 3 zones.

Note: There is no maximum number of <u>accessory buildings</u> allowed on an <u>allotment</u>, but Rule 4.7 – Site Coverage – applies to all buildings.

4.7 BUILDINGS AND SITE COVERAGE

Permitted Activities — Buildings and Site Coverage

4.7.1 Except as provided in Rule 4.7.2, the erection of any <u>building</u> which complies with the <u>site</u> coverage allowances set out in Table C4.1 below shall be a permitted activity.

Table C4.1 Site Coverage Allowances

Zone		Coverage
Living 1	Including garage	35%
	Excluding garage	35% minus 36m²
	Emergency Services only	50%
Living 1A	Castle Hill	35%
Living 1A3	Lincoln	40%
Living 1A4	Lincoln	35%
Living Z	Including Garage	35%
	Excluding Garage	35% - 36m²
	Medium Density	Including garage 40%
		Excluding garage 40% - 18m ²
		Where a site is located in a Medium Density area and forms part of a comprehensive residential development of four or more adjoining lots less than 350m² in size, the maximum site coverage shall be 45% and shall be calculated across the area of the entire comprehensive residential development, excluding any undeveloped balance lot.
Living 1A2	Prebbleton	35%
Living 1A5	Prebbleton	35%
		For <u>comprehensive residential</u> <u>development</u> , <u>site</u> coverage shall be applied over the whole Living 1A5 Zone
Living 1A6	Prebbleton	35%
Living WM	Including garage	35%
	Excluding garage	35% minus 36m²
	Emergency Services only	50%

Zone		Coverage
Living 2 (all townships not otherwise Including garage listed) and Living 2A (Blakes Road, Prebbleton)		Lesser of 20% or 500m ²
	Excluding garage	Lesser of 20% minus 36m² or 500m² minus 36m²
	Emergency Services only	40%
Living 2A	Prebbleton and West Melton	10% and a maximum additional area in hardsurfacing of 10%
	Emergency Services only	40%
Living 2A1	Darfield	10% and a maximum additional area in hardsurfacing of 10%
	Emergency Services only	40%
Living 3		Lesser of 10% or 500m ²

Note: the Living 2 requirement in Rule 4.7.1 does not apply to Dunsandel Primary School.

Temporary Activities

- 4.7.2 Maximum <u>site</u> coverage rules do not apply to:
 - 4.7.2.1 Any <u>building</u>, tent, caravan, trailer or marquee erected for a <u>temporary activity</u>, provided the structure is removed within 2 days after the activity ceases; or
 - 4.7.2.2 Any <u>building</u> erected for <u>temporary accommodation</u> associated with a construction <u>project</u> on the <u>site</u>, provided the <u>building</u> is removed within 12 months or when construction ceases, whichever is the shorter time.

Restricted Discretionary Activities — Buildings and Site Coverage

- 4.7.3 Any activity which does not comply with Rule 4.7.1 shall be a restricted discretionary activity if it complies with all of the following standards and terms:
 - 4.7.3.1 The <u>site</u> is located in a Living 1, Living 1A, Living Z, or Living WM zone and the maximum area of the <u>site</u> covered by a <u>building</u> (s) is:
 - (a) 40% including a garage; or
 - (b) 40% minus 36m² excluding a garage; or
 - 4.7.3.2 The <u>site</u> is located in a Living 1A3 or Living 1A4 zone at Lincoln and the maximum area of the <u>site</u> occupied by a <u>building</u> or <u>buildings</u> is:
 - (a) Living 1A3 Zone 45%; or
 - (b) Living 1A4 Zone 40%; or

- 4.7.3.3 The site is located in a Living Z Medium Density area located within an Outline Development Plan and the maximum area of the site occupied by a building(s) is:
 - (a) 45% including a garage; or
 - (b) 45% 18m² excluding a garage; or
 - (c) part of a comprehensive residential development of four or more adjoining lots under 350m² in size, in which case the maximum site coverage shall be 50% and shall be calculated across the area of the entire comprehensive residential development, excluding any undeveloped balance lot.
- 4.7.4 Under Rule 4.7.3, any resource consent application shall not be notified and shall not require the written approval of affected parties, and the Council shall restrict the exercise of its discretion to consideration of:
 - 4.7.4.1 The number of <u>sites</u> in the street or subdivision where <u>site</u> coverage already exceeds 35%, except in Medium Density areas within Living Z Zones where the site coverage already exceeds 40%.
 - 4.7.4.2 Any adverse effects, singularly or cumulatively, on the residential density or 'spaciousness' of the area.
 - 4.7.4.3 In any Living Z Medium Density areas located within an Outline Development Plan only:
 - (a) the extent to which a complying outdoor living area and opportunities for tree planting and garden landscaping are to be provided:
 - (b) whether there are any areas of communal or public open space in the immediate vicinity of the site;
 - (c) the extent to which a balance is achieved between buildings and hardsurfacing, and landscaping and open space;
 - (d) the avoidance of an appearance of cramped development that is out of keeping with an open and spacious streetscene; and
 - (e) whether the visual effects of increased site coverage are offset by the provision of an attractive, well designed street frontage with good levels of architectural detailing and articulation and the siting of garaging and parking areas to the rear of the site.

Non-Complying Activities — Buildings and Site Coverage

4.7.5 Any activity which does not comply with Rule 4.7.3 shall be a non-complying activity.

4.8 BUILDINGS AND BUILDING HEIGHT

Permitted Activities — Buildings and Building Height

4.8.1 The erection of any <u>building</u> which has a <u>height</u> of not more than 8 metres shall be a permitted activity.

Discretionary Activities — Buildings and Building Height

4.8.2 Any activity which does not comply with Rule 4.8.1 shall be a discretionary activity.

Note

1. Any structure erected in the Living zones at Arthur's Pass or Castle Hill is also subject to Rule 11.1.

4.9 BUILDINGS AND BUILDING POSITION

Permitted Activities — Buildings and Building Position

The following shall be permitted activities:

Recession Planes

- 4.9.1 Except in Rule 4.9.1.1 and 4.9.1.2, the construction of any <u>building</u> which complies with the Recession Plane A requirements set out in Appendix 11;
 - 4.9.1.1 In a Living Z medium density area located within an Outline Development Plan (ODP) on any internal boundary which is:
 - a) not a boundary of a lot in a low density area; and
 - b) which is not a boundary of the ODP area as a whole the construction of any building which complies with a recession plan angle of 45 degrees, with the starting point for the recession plane to be 4m above ground level; and
 - 4.9.1.2 Where buildings on adjoining sites have a common wall along an internal boundary, the recession plane shall not apply along that part of the boundary covered by such a wall.

Setbacks from Boundaries

4.9.2 Except as provided in Rules 4.9.3 to 4.9.33, any <u>building</u> which complies with the <u>setback</u> distances from <u>internal boundaries</u> and <u>road boundaries</u>, as set out in Table C4.2 below.

Table C4.2 - Minimum Setbacks for Buildings

Building Type	Metres from <u>Boundary</u>	
_	Internal	Road (or shared access where specified)
Dwelling or principal building	2 m	4 m
Garage: Wall length 7m or less and vehicle door faces road or shared access	1 m	5.5 m
Garage: Wall length 7m or less and vehicle door faces internal boundary	1 m	2 m
Garage: Wall length greater than 7m and Vehicle door faces <u>road</u> or shared access	2 m	5.5 m
Garage: Wall length greater than 7m and Vehicle door faces internal boundary	2 m	4 m
Accessory Building with wall length not more than 7m	1 m	2 m
Accessory Building with wall length greater than 7m	2 m	4 m
<u>Utility Structures</u>	0 m	0 m

Note: Where a garage is proposed on a corner site i.e. has two road frontages, only one wall may be located up to 2m from a road boundary, provided that that wall does not contain a vehicle door and is less than 7m in length. All other walls are to be set back at least 4m from the road boundary, with walls containing a vehicle door set back 5.5m from the road boundary.

Setbacks from State Highways and internal noise levels

- 4.9.3 Except for the Living 3 Zone at Rolleston identified on the Outline Development Plan in Appendix 39 and 40, and ODP Area 3 and ODP Area 8 in Rolleston, any dwelling, family flat, and any rooms within accessory buildings used for sleeping or living purposes shall be located no closer than 40m from the edge of the sealed carriageway of State Highways with a posted speed limit of 70 Km/hr or greater.
- 4.9.4 Except for the Living 3 Zone at Rolleston identified on the Outline Development Plan in Appendix 39 and 40, and ODP Area 3 and ODP area 8 in Rolleston, any dwelling, family flat, and any rooms within accessory buildings used for sleeping or living purposes within 100m from the edge of the sealed carriageway of State Highways with a posted speed limit of 70 Km/hr or greater shall have internal noise levels from road traffic that do not exceed the limits set out below with all windows and doors closed.

	24 hours
Within Bedrooms	35 dBA (Leq 24 hour)
Within Living Area Rooms	40 dBA (Leq 24 hour)

Note: Living Area rooms means any room in a dwelling other than a room used principally as a bedroom, laundry, bathroom, or toilet.

Common Wall

4.9.5 <u>Buildings</u> may be sited along an <u>internal boundary</u> of the <u>site</u> if the <u>building</u> shares a common wall with another <u>building</u>.

Castle Hill

- 4.9.6 <u>Buildings</u> or structures shall be <u>setback</u> not less than 6m from the south eastern boundaries of Lots 1 and 2 DP 22544 in the Living 1A Zone at Castle Hill Village.
- 4.9.7 <u>Buildings</u> or structures shall be <u>setback</u> not less than 1.5 metres from all <u>internal and</u> road boundaries within the Living 1A Zone at Castle Hill, except that:
 - 4.9.7.1 Along the Living 1A Zone boundaries the minimum <u>setback</u> shall be 3 metres; and
 - 4.9.7.2 Where an <u>internal boundary</u> is also the <u>boundary</u> of a reserve (other than a <u>road</u> reserve) exceeding 1 metre in width or of an <u>access</u> lot or right of way there shall be no minimum setback.

Prebbleton

- 4.9.8 Any <u>building</u> in the Living 1A Zone at Prebbleton shall be <u>setback</u> from the <u>road</u> <u>boundary</u> of Trices Road by not less than 10 metres. The 10 metre area shall be landscaped.
- 4.9.9 Any <u>building</u> shall be <u>setback</u> not less than 6 metres from the north east or north west zone boundaries of the Living 1A2 Zone at Prebbleton.
- 4.9.10 Any <u>dwelling</u> shall be <u>setback</u> not less than 3 metres from an <u>internal boundary</u> in the Living 1A2, 1A3 and 1A4 Zones in Prebbleton.
- 4.9.11 For the Living 1A6 Zone in Prebbleton, no dwelling shall be sited within 5m of the north western common boundary with the Kingcraft Drive Existing Development Area, as identified in the ODP contained in Appendix 19.
- 4.9.12 Any <u>dwelling</u> in the Living 2A Zone in Prebbleton shall have:
 - 4.9.12.1 A <u>setback</u> from any <u>internal boundary</u> other than the southern zone <u>boundary</u> of not less than 6 metres.
 - 4.9.12.2 A <u>setback</u> from the southern zone <u>boundary</u> of not less than 20 metres.
- 4.9.13 Any <u>dwelling</u> shall be set back not less than 15 metres from the north eastern <u>boundary</u> of the Living 2A (Blakes Road) Zone.
- 4.9.14 Any <u>dwelling</u> shall be set back not less than 48.2m from the north eastern zone <u>boundary</u> of the Living 2A Zone in Prebbleton, as identified in Appendix 19.

West Melton

- 4.9.15 Any <u>dwelling</u> within the area shown in Appendix 20 (Living 1B and Living 2 zones) or Appendix 20A (Living WM Zone) shall be set back at least 40 metres from State Highway 73.
- 4.9.16 Any <u>dwelling</u> in the Living 2A Zone at West Melton shall have:
 - 4.9.16.1 A setback from any internal boundary of not less than 6 metres.

4.9.16.2 A setback from any road boundary of not less than 10 metres.

Leeston

4.9.17 Any <u>dwelling</u> in the Living 2A Zone at Leeston shall have a <u>setback</u> from any Business Zone <u>boundary</u> of not less than 20 metres.

Living Z Medium Density areas located within an Outline Development Plan

- 4.9.18 Any dwelling or principal building shall be set back a minimum of 3m from any road boundary.
- 4.9.19 Where an allotment has legal access to a private Right of Way or shared access, any dwelling or principal building on that allotment shall be set back a minimum of 3m along the entire length of the boundary with that private Right of Way or shared access.
- 4.9.20 Any garage where a vehicle door faces the road, a private Right of Way or shared access shall be set back a minimum of 5.5m from the road boundary, private Right of Way, or shared access.
- 4.9.21 No garage or accessory building is to be located between the front facade of the dwelling and the road boundary, or the private Right of Way or shared access by which the allotment is accessed.
- 4.9.22 Any dwelling or principal building, excluding garages or accessory buildings, shall be set back a minimum of 2m from any internal boundary. Buildings may however be sited along an internal boundary if the building shares a common wall with another building on an adjoining site.
- 4.9.23 No set back is required for any garage or accessory building from an internal boundary, provided that the total length of garages or accessory buildings adjacent to the internal boundary do not exceed 7m and provided those garages or accessory buildings comply with a 45 degree recession plane measured from 2.5m above ground level at the boundary, except when the site is on the boundary of a low density area or another Living zone, in which case Rule 4.9.2 applies.
- 4.9.24 All balconies at first floor level and above may only be located in a façade that faces a road boundary or an internal boundary shared with land vested or designated with Council for stormwater, recreation or esplanade reserve/ strip purposes.
 - 4.9.24.1 Any windows at first floor level or above must:
 - face a road boundary, or an internal boundary shared with land vested or designated with Council for stormwater, recreation or esplanade reserve/ strip purposes; or
 - Be set back a minimum of 10m from an internal boundary; or
 - Have a sill height of at least 1.6m above internal floor level; or
 - Be obscure glazed, and either non-opening or top- hinged, and be associated with a bathroom, toilet, or hallway.

Temporary Activities

- 4.9.25 Rule 4.9.2 does not apply to the siting of any <u>building</u>, tent, caravan or trailer on a site if:
 - 4.9.25.1 The <u>building</u>, tent, caravan or trailer is erected for a <u>temporary activity</u>; and
 - 4.9.25.2 The <u>building</u>, tent, caravan or trailer is removed within 2 days of the activity ceasing.
- 4.9.26 Rule 4.9.2 does not apply to the siting of any <u>building</u> on a <u>site</u> which is for <u>temporary</u> <u>accommodation</u> associated with a construction <u>project</u> on the <u>site</u> if:
 - 4.9.26.1 The <u>building</u> is removed within 12 months or when construction ceases, whichever is the shorter time.

Setback from Lincoln Sewerage Treatment Plant

4.9.27 Any <u>dwelling</u> in the Living 1A and Living Z Zone at Lincoln shall be <u>setback</u> not less than 150 metres from the <u>boundary</u> of the area designated for the Lincoln Sewage Treatment Plant, as identified on Planning Map 122.

Lincoln

4.9.28 Within the Living Z zone at Lincoln, ODP Area 5, Appendix 37, no dwelling or principal building shall be constructed within 50m of the Landscape Buffer located at the northern end of the Business 2B zone until appropriate noise attenuation measures, as determined by a suitably qualified noise expert and designed to achieve the noise standards contained in Rule 22.4.1.6 have been constructed.

Rolleston

- 4.9.29 In ODP Area 3 and ODP Area 8 in Rolleston, no dwellings shall be located closer than 40m (measured from the nearest painted edge of the carriageway) from State Highway 1.
- 4.9.30 In ODP Area 3 and ODP Area 8 in Rolleston, for any dwelling constructed between 40m and 100m (measured from the nearest painted edge of the carriageway) from State Highway 1:
 - Appropriate noise control must be designed, constructed and maintained to ensure noise levels within the dwelling meet the internal design levels in AS/NZS2107:2000 (or its successor) – 'Recommended design and sound levels and reverberation times for building interiors';
 - Prior to the construction of any dwelling an acoustic design certificate from a suitable qualified and experienced consultant is to be provided to Council to ensure that the above internal sound levels can be achieved.
- 4.9.31 Any building in the Living 3 Zone at Rolleston (as shown on the Outline Development Plan in Appendix 39 and 40) shall be set back at least:
 - i) 15 metres from any road boundary except that on corner lots a minimum setback of 10m applies to one road boundary;
 - ii) 5 metres from any other boundary

4.9.32 Any dwelling, family flat, and any rooms within accessory buildings used for sleeping or living purposes, and any internal areas associated with noise sensitive activities in the Living 3 Zone at Rolleston (as shown on the Outline Development in Appendix 39) shall be setback at least 80m from State Highway 1.

For the purposes of this rule, noise sensitive activities means any residential activity, travellers accommodation, educational facility, medical facility or hospital, or other land use activity, where the occupants or persons using such facilities may be likely to be susceptible to adverse environmental effects or annoyances as a result of traffic noise from State Highway 1 over its location.

4.9.33 Any dwelling, family flat, and any rooms within accessory buildings used for sleeping or living purposes in the Living 3 Zone at Rolleston (as shown on the Outline Development Plan in Appendix 39 (Holmes Block) located outside the 'Odour Constrained Area' as shown in Appendix 40 (Skellerup Block)).

Special Character Low Density Areas (Living 1C zoning)

- 4.9.34 In Living 1C zoned areas, buildings shall have a <u>setback</u> from the road boundary of not less than 6m.
- 4.9.35 Dwellings and family flats shall be positioned at least 6m from any existing dwelling or family flat (or footprint of a planned dwelling or family flat for which a building consent has been granted within the previous 2 years).

An exception is where family flats are attached to the principal dwelling.

Restricted Discretionary Activities — Buildings and Building Position

- 4.9.36 Any activity which does not comply with Rule 4.9.1 shall be a restricted discretionary activity.
- 4.9.37 Under Rule 4.9.36 the Council shall restrict the exercise of its discretion to consideration of:
 - 4.9.37.1 Any adverse effects of shading on any adjoining property owner; or on any <u>road</u> or footpath during winter.
- 4.9.38 Any activity which does not comply with Rule 4.9.2 and Rules 4.9.4 to 4.9.15 and 4.9.27 to 4.9.29 shall be a restricted discretionary activity.
- 4.9.39 Under Rule 4.9.38 the Council shall restrict the exercise of its discretion to consideration of:

4.9.39.1 <u>Internal Boundary</u>

Any adverse effects on the:

- (a) privacy
- (b) outlook
- (c) shading; or
- (d) <u>amenity values</u>

of the adjoining property, its occupiers and their activities; and

4.9.39.2 Road Boundary

Any adverse effects on:

- (a) the character of the street
- (b) safety and visibility of pedestrians, cyclists and motorists, and
- (c) shading of the <u>road</u> or footpath in winter;
- (d) methods to mitigate any adverse effects of traffic noise on the occupants of a <u>dwelling</u>; and
- 4.9.39.3 Any reverse sensitivity issues at the southern zone <u>boundary</u> of the Living 2A zone at Prebbleton.
- 4.9.39.4 In the Living 3 Zone at Rolleston as shown in Appendix 39, whether the building development meets the internal sound levels listed in the table below:

Type of Occupancy/Activity	Recommended Internal Design Sound Level (dBA Leq (24hr))
Dwelling/Family Flat/Accessory buildings – bedroom Within Bedrooms	35
All other habitable spaces	40
Noise Sensitive Activities	35

- 4.9.40 Any activity which does not comply with 4.9.34 or 4.9.35 shall be a restricted discretionary activity
- 4.9.41 Under rule 4.9.40 the Council shall restrict the use of its discretion to consideration of the unique spacious character of the area and its sensitivity to incongruous or closely spaced buildings.

Discretionary Activities — Buildings and Building Position

4.9.42 Any activity which does not comply with Rule 4.9.3 shall be a discretionary activity.

Non-Complying Activities — Buildings and Building Position

- 4.9.43 Any <u>dwelling</u> which does not comply with Rule 4.9.27 shall be a non-complying activity.
- 4.9.44 Erecting any new dwelling in the Countryside Area or the 'Odour Constrained Area' identified on the Outline Development Plan in Appendix 39 and 40.

4.10 RELOCATED BUILDINGS

Note: Any <u>relocated building</u> in the Living zones at Arthur's Pass or Castle Hill is also subject to Rule 11.1.

Permitted Activities — Relocated Buildings

- 4.10.1 The erection of any <u>relocated building</u> shall be a permitted activity if one or more of the following conditions are met:
 - 4.10.1.1 The relocated building is a garage or accessory building; or
 - 4.10.1.2 The <u>building</u> is moved from one position to another within the same <u>site</u>; or
 - 4.10.1.3 The <u>building</u> is relocated on to a <u>site</u> for a <u>temporary activity</u> and is removed from the <u>site</u> within 2 days of the activity ceasing; or
 - 4.10.1.4 The <u>building</u> is relocated on to a <u>site</u> to provide <u>temporary</u> <u>accommodation</u> during a construction <u>project</u> on the <u>site</u>, and the <u>building</u> is removed from the <u>site</u> within the lesser of a 12 month period or when the construction work ceases.
 - 4.10.1.5 The <u>building</u> is being relocated within or between schools.

Controlled Activities — Relocated Buildings

- 4.10.2 Any activity which does not comply with Rule 4.10.1 shall be a controlled activity which shall not be notified and shall not require the written approval of affected parties. The matters the Council has reserved control over are:
 - 4.10.2.1 The time period within which the <u>building</u> is to have its new foundations established and covered; and
 - 4.10.2.2 The time period within which any repair work to the exterior of the <u>building</u> is to be repaired; and
 - 4.10.2.3 The standard to which the exterior of the building is to be finished; and
 - 4.10.2.4 Whether any bond is required to cover the cost of reinstatement works in relation to matters listed under Rules 4.10.2.1 to 4.10.2.3, and the type of bond.

4.11 COMPREHENSIVE RESIDENTIAL DEVELOPMENT IN PREBBLETON

Discretionary Activities — Comprehensive Residential Development in Prebbleton

- 4.11.1 In the Living 1A5 Zone in Prebbleton, <u>comprehensive residential development</u> shall be a discretionary activity where Council shall take into account, but not be limited to, the following:
 - 4.11.1.1 Effects associated with the width, location, form and layout of accesses and <u>roads</u> on the amenity of the area;
 - 4.11.1.2 Effects of vehicle parking and garaging on the amenity of the area or the enjoyment of neighbouring properties;
 - 4.11.1.3 The ability to provide adequate vehicle parking and manoeuvring on the site;
 - 4.11.1.4 Impacts on the road network in traffic generation and traffic safety;
 - 4.11.1.5 The extent to which levels of traffic generation or pedestrian activity will result that are incompatible with the character of the surrounding living environment:
 - 4.11.1.6 Effects on the sense and spaciousness of the immediate area and wider neighbourhood;
 - 4.11.1.7 The extent to which the scale, form, modulation, design, colours and materials of <u>buildings</u> will be compatible with other <u>buildings</u> in the surrounding area and will not result in visual dominance or incongruency;
 - 4.11.1.8 The extent to which <u>site</u> layout and <u>buildings</u> have been designed to avoid adverse effects on the privacy, outlook, access to sunlight and daylight and other <u>amenity values</u> of neighbouring properties;
 - 4.11.1.9 The amount of variety in design and size of <u>dwellings</u> on the <u>site</u>, in order to provide a choice of living accommodation;
 - 4.11.1.10 Whether the <u>dwellings</u> are clustered in larger or smaller groups and the extent to which the grouping or spacing of <u>dwelling</u> units on the <u>site</u> leads to an attractive and varied <u>development</u> rather than a monotonous one;
 - 4.11.1.11 The need for a 'step in plan' to be provided at 20 metre intervals along a continuous <u>building</u> wall in order to mitigate adverse effects of continuous 'building bulk' being close to the <u>boundary</u> of a neighbouring property;
 - 4.11.1.12 The attractiveness of the street frontages of the site;
 - 4.11.1.13 The extent to which mature vegetation is retained and the character of the <u>site</u> remains dominated by tree and garden plantings;
 - 4.11.1.14 Privacy between habitable rooms of neighbouring dwellings;

- 4.11.1.15 The quality of landscaping and its effectiveness in mitigating adverse effects:
- 4.11.1.16 Impacts on the sense of spaciousness of the immediate area and wider neighbourhood;
- 4.11.1.17 The extent to which outdoor living space remains open and not contained or partitioned by fencing;
- 4.11.1.18 Whether the amount of outdoor living space is accessible to, and adequate for, the occupants of all <u>dwellings</u> and whether it will receive direct sunlight on the shortest day of the year.

4.12 COMPREHENSIVE RESIDENTIAL DEVELOPMENT IN LIVING Z MEDIUM DENSITY AREAS LOCATED WITHIN AN OUTLINE DEVELOPMENT PLAN

Restricted Discretionary Activities — Comprehensive Residential Development in Living Z Medium Density areas located within an Outline Development Plan

4.12.1 In a Living Z Medium Density area located within an Outline Development Plan, comprehensive residential development shall be a restricted discretionary activity, which shall not be notified and shall not require the written approval of affected parties. Under Rule 4.12.1 the Council shall restrict the exercise of its discretion to consideration of:

4.12.1.1 Context and Spaciousness

The extent to which comprehensive development responds to the existing context through:

- (a) Providing compatibility in scale between the new development and any neighbouring buildings;
- (b) Being oriented towards adjoining public spaces such as roads, parks, or reserves and presents a front façade with a good level of glazing. Visible pedestrian front entrances and low front fencing;
- (c) Providing dwellings which relate to each other and surroundings in terms of regularity of features such as window height and detailing and a consistency in roof slope and form.

4.12.1.2 Attractive Street Scene

The extent to which the public interface and external appearance of buildings in comprehensive developments:

- (a) Provides dwellings with visual interest when viewed from any public spaces through articulation, roof form, openings and window location:
- (b) Provides visible entry to the dwelling when viewed from the road or the main public access to the development;

- (c) Provides a good level of glazing and overlooking from habitable rooms towards the road and any adjacent public open spaces;
- (d) Building design provides a balance of consistency and variety in the street scene:
- (e) Provides open frontages which will not be enclosed by fences over 1m in height.

4.12.1.3 Dwelling design, position and orientation

The extent to which the dwelling design, position and orientation of buildings in comprehensive developments:

- (a) Locates and orientates dwellings to define external spaces, to allow adequate sunlight and daylight into main living rooms and private outdoor spaces;
- (b) Positions dwellings to ensure that dwellings front on to, and are accessed from, the road, private Right of Way, or shared accessways;
- (c) Positions dwellings to capitalise on any views or natural features;
- (d) Minimises the visual dominance of garaging and vehicle parking areas, especially as viewed from the street or public open spaces. The use of rear courtyards for parking is encouraged;
- (e) Incorporates attractive detailed design including provision of mailboxes and space for bin storage and collection;
- (f) Provides attractive and efficient shared parking where required.

4.12.1.4 Visual and acoustic privacy

The extent to which buildings in comprehensive developments achieve visual and acoustic privacy through:

- (a) Avoiding or minimising direct views from the windows of one dwelling into another at distances less than 20m through the use of the following design devices:
 - The shape and position of the buildings
 - The location of windows e.g. offset windows and high sill windows
 - Intervening screening e.g. 1.8 metre high fences (not on road boundary or frontage with accessways), hedges, trees
 - Screening devices on balconies to ensure that they do not overlook windows or private spaces
- (b) The provision of acoustic treatment between dwellings through enhancing separation between openings, effective solid acoustic screening and by locating noise sensitive spaces from noisy

activities (e.g. separation of bedrooms from service areas and garages).

4.12.1.5 Private outdoor living spaces

The extent to which comprehensive developments provide private outdoor living spaces that:

- (a) Have the primary outdoor living space directly accessible from an internal living room;
- (b) Have any secondary outdoor living spaces such as balconies directly accessible from living rooms or bedrooms;
- (c) Are located so that the principle private outdoor living space will receive sunshine for a reasonable portion of the day in winter;
- (d) Are located so that the principle outdoor living space is not directly overlooked by windows or balconies of neighbouring dwellings;
- (e) The extent to which communal outdoor living space is provided within a comprehensively designed development and the functionality of that space for meeting the likely needs of future occupants:
- (f) Are located to the side or rear of the dwelling and not adjacent to the road boundary.

4.12.1.6 Safety and security

The extent to which comprehensive developments are designed to reduce the fear and incidence of crime through:

- (a) The avoidance of narrow alleyways and places of entrapment:
- (b) A clear definition between public and private spaces;
- (c) The ability to provide casual surveillance of public space from private property and vice versa.

4.12.1.7 Accessibility and connectivity

The extent to which comprehensive developments are designed for accessibility and connectivity through:

- (a) Providing for the safe and efficient movement of pedestrians, cyclists and motorised vehicles within and through the development and to surrounding residential areas and commercial and community facilities;
- (b) Providing direct pedestrian and cycle linkages from developments to and between any adjoining reserves and open spaces.

4.13 BUILDINGS AND STREETSCENE

Permitted Activities — Buildings and Streetscene

For all residential development located within the Lowes Road Outline Development Plan area (Appendix 34) or a Living Z zone

- 4.13.1 That any fence between the front façade of the dwelling and the street boundary or Private Right of Way or shared access over which an allotment has legal access which is parallel or generally parallel to that boundary shall be a maximum height of 1m. For allotments with frontage to more than one road, any fencing on the secondary road boundary is to be no higher than 1.8m.
- 4.13.2 Any other fence shall be a maximum height of 1m if it is located within 3m of the street boundary or Private right of Way or shared access over which allotment has legal access.

Restricted Discretionary Activities - Buildings and Streetscene

- 4.13.3 Any activity which does not comply with Rule 4.13.1 shall be a restricted discretionary activity.
- 4.13.4 Under Rule 4.13.1 the Council shall restrict the exercise of its discretion to consideration of:
 - 4.13.4.1 The degree to which an open streetscene is maintained and views between the dwelling and the public space, private Right of Way or shared access are retained.
 - 4.13.4.2 The extent to which the visual appearance of the site from the street, or private Right of Way or shared access over which the lot has legal use of any part, is dominated by garden planting and the dwelling, rather than front fencing.
 - 4.13.4.3 The extent to which the proposed fence is constructed out of the same materials as the dwelling and incorporates steps in plan, landscaping, and see-through materials such as railings or trellis.

4.14 BUILDINGS AND PRIVATE OUTDOOR LIVING SPACE

Permitted Activities — Buildings and Private Outdoor Living Space

Living Z Medium Density areas located within an Outline Development Plan

- 4.14.1 (a) In Living Z Medium Density areas located within an Outline Development Plan, each dwelling shall be provided with a private outdoor living space with a minimum area of 50m² and a minimum dimension of 4m.
 - (b) Any area provided by balconies with a minimum dimension of 1.5m counts towards the minimum required area of outdoor living space.
 - (c) The outdoor living space (excluding balconies) is not to be located between the front building façade and the road boundary.

Restricted Discretionary Activities — Buildings and Private Outdoor Living Space

- 4.14.2 Any activity which does not comply with Rule 4.14.1 shall be a restricted discretionary activity which shall not be notified and shall not require the written approval of affected parties. Under Rule 4.14.1 the Council shall restrict the exercise of its discretion to consideration of:
 - 4.14.2.1 The degree to which any reduction in outdoor living space will adversely affect the ability of the site to provide for the outdoor living needs of residents of the site.
 - 4.14.2.2 The extent to which any outdoor living space intrudes in front of any residential unit such that it would be likely to give rise to pressure to erect high fences between the dwelling and the street, to the detriment of an open street scene.
 - 4.14.2.3 The degree to which large areas of public open space are provided within very close proximity to the site.
 - 4.14.2.4 The degree to which any communal outdoor living areas are proposed where individual dwellings form part of a comprehensive residential development.
 - 4.14.2.5 The degree to which a reduction in outdoor living space would contribute to a visual perception of cramped development or over-development of the site.

4.15 SETBACKS FROM WATERBODIES

Permitted Activities — Setbacks from Waterbodies

- 4.15.1 The siting of any <u>dwelling</u> or <u>principal building</u> or any other structure shall be a permitted activity if it is <u>setback</u> not less than either:
 - 4.15.1.1 20m from the edge of any waterbody listed in Appendix 12; or
 - 4.15.1.2 10m from the edge of any other waterbody (excluding aquifers).

Discretionary Activities — Setbacks from Waterbodies

4.15.2 Any activity which does not comply with Rule 4.15.1 shall be a discretionary activity.

Notes

- 1. Rule 4.15 does not apply to walkway facilities; <u>utility structures</u> attached to existing <u>buildings</u> or structures; or <u>signs</u> which are permitted activities under Rule 7.
- 2. The edge of any <u>waterbody</u> is measured from the edge of the bed of the <u>river</u>. The bed is defined in section 2 of the <u>Act</u> as "the space of land which the waters of the <u>river</u> cover at its fullest flow, without overtopping its banks".
- 3. Rule 4.15 shall not apply on any <u>allotment</u> adjoining an <u>esplanade</u> reserve or strip along a <u>waterbody</u> where the reserve or strip has previously been vested in the Council.

4.16 BUILDINGS AND SITES OF SIGNIFICANCE TO TĀNGATA WHENUA (WĀHI TAONGA MANAGEMENT AREAS)

Permitted Activities — Buildings and Sites of Significance to Tāngata Whenua (Wāhi Taonga Management Areas)

- 4.16.1 Erecting any building or any additions or alterations to, or modification or demolition of, any building shall be a permitted activity if all of the following conditions are met:
 - 4.16.1.1 In the area listed in Appendix 5 and shown on the Planning Maps as <u>Wāhi</u>

 <u>Taonga Management Area</u> C39(b), any earthworks associated with the building are limited to the disturbance of soil over areas where that soil has been previously disturbed by cultivation, planting (trees, pasture or crops), building or earthworks. Any disturbance within those areas shall be limited to a maximum depth of 20cm
 - 4.16.1.2 For the part of <u>Wāhi Taonga Management Area</u> C48 zoned Living 1 at Rakaia Huts, any earthworks does not involve the disturbance, damage to, removal of or destruction of any object, artefact or other symbol of pre-European settlement, occupation or use of that site.

Controlled Activities — Buildings and Sites of Significance to Tangata Whenua (Wāhi Taonga Management Areas)

- 4.16.2 Any activity which does not comply with Rules 4.16.1.1 and 4.16.1.2 shall be a controlled activity if the written consent of the local rūnanga has been obtained; and in the case of <u>Wāhi Taonga Management Area</u> C39(b), the written consent of the New Zealand Historic Places Trust Pouhere Taonga, has also been obtained.
- 4.16.3 In assessing any application made under Rule 4.16.2, Council shall restrict its control to consideration of the following matters:
 - 4.16.3.1 For the part of <u>Wāhi Taonga Management Area</u> C48 zoned Living 1 at Rakaia Huts any damage to, destruction or removal of any object, remnant or artefact contained within <u>Wāhi Taonga Management Area</u> C48, as advised by local rūnanga; and
 - 4.16.3.2 In <u>Wāhi Taonga Management Area</u> C39(b), any inappropriate disturbance or other potential adverse effects on any site of significance, object, remnant or artefact, as advised by local rūnanga and the New Zealand Historic Places Trust Pouhere Taonga; and
 - 4.16.3.2 Any monitoring or review conditions.

Restricted Discretionary Activities — Buildings and Sites of Significance to Tangata Whenua (Wāhi Taonga Management Areas)

- 4.16.4 Any activity which does not comply with Rule 4.16.2 shall be a restricted discretionary activity.
- 4.16.5 Under Rule 2.1.6 the Council shall restrict the exercise of its discretion to all of the following matters:
 - 4.16.5.1 Any inappropriate disturbance or other potential adverse effects on any site of significance within a Silent File area, as advised by local rūnanga;
 - 4.16.5.2 Any damage to, destruction or removal of any object, remnant or artefact contained within a <u>Wāhi Taonga Site</u> or <u>Wāhi Taonga Management Area</u> as advised by local rūnanga, and in the case of <u>Wāhi Taonga Management Area</u> C39(b), the New Zealand Historic Places Trust Pouhere Taonga;
 - 4.16.5.3 Any potential costs to the landholder of not being able to undertake the proposed activity on that site;
 - 4.16.5.4 Any alternative options available to undertake the activity in another form or on another site and the costs and practicality of these options;
 - 4.16.5.5 Any positive effects which may offset any adverse effects; and
 - 4.16.5.6 Any monitoring or review of conditions.

4.17 FENCES ADJOINING RESERVES

Permitted Activities – Fences Adjoining Reserves

4.17.1 All development located within the Living Z zone that shares a boundary with a reserve or walkway shall be limited to a single fence erected within 5m of any Council reserve that is at least 50% visually transparent where it exceeds 1.2m in height (which shall be applied to the whole fence in its entirety).

Restricted Discretionary Activities – Fences Adjoining Reserves

- 4.17.2 Any activity which does not comply with Rule 4.17.1 shall be a restricted discretionary activity. Council shall restrict the exercise of its discretion to the following:
 - 4.17.2.1 The extent to which the proposed fencing promotes passive surveillance to reduce the fear and incidence of crime;
 - 4.17.2.2 The extent to which the fencing design and materials complement the open space amenity of the reserve;
 - 4.17.2.3 The extent to which the orientation of the section and aspect of the outdoor living areas within the section is able to reduce the effects of the non-complying fence on the open space amenity o the adjoining reserve; and
 - 4.17.2.4 The need to avoid adverse cumulative effects arising from the number of non-complying fences being established along a reserve boundary and the extent to which the incremental reduction of the open space amenity of the reserve is mitigated through appropriate fencing design and construction materials and the layout of future dwellings and yard space.

Reasons for Rules

Natural Hazards

Rules 4.1.1 and 4.1.2 identify Tai Tapu as a <u>township</u> where there is a significant known risk of damage to people or property from flooding and ponding. Rule 4.1.1 does not necessarily prevent <u>earthworks</u> and <u>building</u> in the <u>township</u> of Tai Tapu; rather, the rule requires a resource consent application for a restricted discretionary activity for larger scale activities, so the nature and level of any risk of hazard, and any mitigation measures proposed, can be assessed.

Rule 4.1.4 prohibits <u>dwellings</u> and <u>principal buildings</u> from being erected between a <u>waterbody</u> and its stopbank. This is due to the high risk in this area of the flooding or ponding of water occurring. Other <u>townships</u> will be affected by this rule if they contain certain land between a <u>waterbody</u> and an associated stopbank.

Rule 4.1.3 restricts <u>dwellings</u>, parts of <u>dwellings</u> and other <u>principal buildings</u> in an area known to be subject to flooding from the Rakaia River.

It is known by the Council that other <u>townships</u> in Selwyn District are likely to be affected by natural hazards. While information is lacking, these include Whitecliffs and Hororata. The objectives and policies section for Natural Hazards (Part B, Section 3.1) identify that the preferred methods to address this issue in such <u>townships</u> is through reliance on the Building Act and

section 106 of the Resource Management Act, and by relevant hazard information held by the Council being provided as part of a request for Land Information Memoranda.

Landscaping

A common feature of residential areas is a tidy area between the house and <u>road</u> frontage. There is a 'market' incentive for home owners to keep this area tidy, as it can add value to the house and property. There is not, however, such a direct market incentive for other activities, such as businesses and community facilities, to retain a tidy 'front yard'. The objective of Rule 4.2 is to ensure non-residential activities locating in Living zones maintain this feature of residential areas.

Water Supply

Every house is required to have a potable water supply and effluent <u>disposal</u>, in order to be 'habitable' under the Building Act 2004. The rules in the District Plan set out additional conditions, such as whether the service must be reticulated, to avoid effects on natural and physical resources such as groundwater and <u>amenity values</u>.

Sewage Treatment and Disposal

The <u>townships</u> listed in Rule 4.5.1 either have a reticulated sewerage treatment and <u>disposal</u> system, or need such a system to avoid adverse effects on groundwater.

A reticulated sewerage scheme for West Melton Township to cater for the anticipated <u>township</u> growth is now available. It was considered that this was necessary given the position of Environment Canterbury in relation to the rezoning of land at West Melton for residential <u>development</u>.

Rule 4.5 should not be interpreted as an indication that reticulated sewerage will not be needed in other <u>townships</u> in the future, such as Darfield or Kirwee. Environment Canterbury (the Regional Council) is responsible for issuing discharge permits to allow on-site effluent treatment and <u>disposal</u>. If, in the future, permits are no longer issued for a particular <u>township</u>, a reticulated sewerage system may be required.

Building Density

The <u>site</u> coverage rules only apply to the proportion of an <u>allotment</u> at <u>ground level</u>, that is covered in <u>building</u>. <u>Site</u> coverage rules therefore are unable to control the effects on <u>amenity values</u> of higher density forms of residential <u>development</u> above <u>ground level</u>. Rule 4.6 is intended to manage the effects of such <u>developments</u> as multi-storey block of flats and apartments.

The District Plan recognises that there may be a demand of higher density forms of residential <u>development</u> in <u>townships</u> of the District, particularly in the form of student accommodation at Lincoln. The Plan provides for this is a permitted activity in Business 1 zones, where higher building density is more compatible with commercial amenity values.

The District Plan is not intended to preclude, in Living 1 zones, the erection of medium density housing <u>developments</u> such as small blocks of flats or townhouses. Rule 4.6 therefore provides for this intensity of <u>development</u> as restricted discretionary or discretionary activities, subject to the relevant assessment matters and objectives and policies of the Plan, and the mitigation of any adverse effects on the environment.

In some Living 1 zones, however, the provision of more than one <u>dwelling</u> on an <u>allotment</u> is a non-complying activity. This is because these zones were created as a result of plan changes to the former District Plan. An outcome of the plan change process in those cases was the decision

that residential density should be limited to one <u>dwelling</u> per <u>allotment</u>. The new District Plan carries over these decisions.

Due to the lower density environments of the Living 2 zones, it is generally considered inappropriate for there to be more than one <u>dwelling</u> per <u>allotment</u>.

Site Coverage

Rule 4.7 is designed to maintain 'spaciousness' in the Living zones of Selwyn District. It does this by controlling the ratio of land to <u>building</u> rules. The rule does not affect the size of an <u>allotment</u> (only the extent of the lot that can be covered in <u>buildings</u>), therefore a variety of <u>allotment</u> sizes may be provided for. An exemption has been made for Dunsandel Primary School from the maximum <u>building</u> floor space requirement of the Living 2 Zone to bring it into line with other schools which are subject to a percentage of <u>site</u> covered, rather than a maximum floor area figure.

Rule 4.7.3 provides for some <u>allotments</u> to have higher <u>site</u> coverages, as restricted discretionary activities. This enables the Council to meet the demand for small, easy care sections while managing the number of such <u>allotments</u>, so as to maintain overall spaciousness. The rule only applies in Living 1, 1A, Living WM zones and Living Z Medium Density areas located within an Outline Development Plan, because Living 2 zones are distinguished from Living 1 zones by their lower residential density.

Higher levels of <u>site</u> coverage have also been provided for <u>emergency services</u> recognising their importance to the community. Their general one-off locations throughout the district's <u>townships</u> will ensure any impact of increased density on the overall character of an area is minimal.

Significant new <u>development</u> in West Melton will adjoin State Highway 73. The volume of traffic using this <u>road</u>, mainly at "open <u>road</u>" speed limits, requires a degree of physical separation which (in combination with noise bunding) is intended to partly mitigate the effects of traffic noise.

Building Height

Rule 4.8 sets maximum <u>height</u> requirements for <u>buildings</u> and structures, to ensure they are in keeping with the visual character of the Living zones.

Part of the <u>amenity values</u> of <u>townships</u> in Selwyn District is relatively low density of <u>buildings</u> and views across the <u>townships</u> to rural areas, the Southern Alps/Kā Tiritiri o te Moana and the Port Hills. Rule 4.8 helps to maintain those values by limiting the <u>height</u> of <u>buildings</u> and structures.

In consultation on the district plan (township surveys and public workshops), residents and ratepayers identified that 'sky scrapers', 'office towers' and other multi-storey <u>buildings</u> are not part of the landscape and <u>amenity values</u> of the District. However it was identified that some tall structures, such as power poles and grain silos, are. To that end, Rule 4.8 differentiates between the <u>height</u> of <u>buildings</u> and the <u>height</u> of other structures.

<u>Buildings</u> or structures that cannot comply with Rule 4.8 may be able to be erected in Living zones under an application for a discretionary activity if potential adverse effects on visual character and amenity values are able to be adequately mitigated, remedied or avoided.

Building Position

Rule 4.9 is intended to give property owners maximum flexibility over the use of space on their properties, while affording neighbours adequate protection in relation to sunlight, privacy and outlook. <u>Setback</u> distances are not used to maintain 'spaciousness'. This is done by <u>site</u> coverage while recession planes control shading, and setback distances control privacy and outlook.

In the Living Z Medium Density areas included within an Outline Development Plan, buildings are required to be set back a minimum of 3m from the road boundary, or shared Rights of Way and shared accessways by which the site is accessed. The road boundary setback is smaller than that required for sites in the lower density Living zones in recognition of the higher density character anticipated in the Medium Density areas and the need to enable smaller sites to be used in an efficient manner. Some setback is however still necessary to provide the opportunity for some landscaping and tree planting along the road frontage, Rights of Way, and shared accessways and will help to maintain an open and pleasant street scene and to provide adequate levels of amenity and the opportunity for landscaping along shared driveways. The need for buildings to be set back from private Rights of Way and shared accessways is in recognition that these areas function as private lanes and as such there is a need to maintain adequate levels of amenity along such areas and to ensure that they are not overly dominated by buildings and blank high boundary fencing.

Garages are required to be set no further forward than the front façade of the dwelling so that they are not visually dominant from the street, private Rights of Way, and shared accessways. Front facing garage doors are required to be set back at least 5.5m from road boundaries, private Rights of Way, and shared accessways to enable vehicles to be parked in front of the doors without intruding over footpaths, road reserve, or shared driveways, and to provide a second informal car parking space for occupants or visitors.

In the Medium Density areas, provision is also made for dwellings and primary buildings to be setback a minimum of 2m from internal boundaries to ensure a degree of daylight is available to ground floor windows. Garages are however able to be erected along the internal boundary in recognition that they are inherently single storey structures and do not contain habitable space, with a tighter recession plane control in place to ensure that they are single storey where they adjoin the boundary. Buildings are able to be built to an internal boundary where they will share a wall with a neighbouring building, in order to facilitate more intensive forms of housing such as semi-detached or terraced typologies. No recession planes apply over that portion of the boundary where a common wall has been built as there will be no shading effects where buildings share the same wall. To facilitate more intensive forms of housing and to enable two storey dwellings to be designed on relatively small sites, the recession plane requirements start at 4m rather than 2.5m, with a standard 45 degree angle required from all boundaries. This will encourage buildings to be oriented towards the street and rear garden, rather than towards side boundaries.

In the Medium Density areas, balconies at first floor level and above are only permitted in facades that face road or reserve boundaries due to the potential for balconies to have a significant adverse effect on privacy if they are located overlooking internal boundaries. In more intensive residential areas where dwellings are in closer proximity to one another than low density zones and where there is a much greater likelihood of dwellings being higher than single storey, there is an increased need to maintain acceptable levels of privacy. Windows at first floor level or above are therefore only permitted if they either face a road or reserve boundary, are set back a minimum of 10m from an internal boundary (typically the rear garden boundary), or have a high level internal sill or are obscure glazed and are associated with bathrooms or hallways (thereby providing light but preventing overlooking).

Controls on side and front yard spaces apply to sites in the Living 3 Zone at Rolleston identified on the Outline Development Plan in Appendix 39 and 40 in order to retain views between

residences and to assist in retaining elements of rural character and provide visual integration and visual attractiveness.

Building within the Countryside Area identified on the Outline Development Plan in Appendix 39 and 40 is a non-complying activity. The purpose of the Countryside Areas is to provide open space and a visual link to the surrounding rural landscape. These corridors bisect the residential activity and are to be managed in productive rural use.

<u>Setbacks</u> do not apply to <u>utility structures</u> because these structures have small bulk and are not occupied by people. However, they do apply to <u>utility buildings</u> which have a larger floor area, as such <u>buildings</u> have the potential to cause shading and loss of outlook. They are more likely to be occupied by people and, therefore, can affect privacy.

If the conditions for permitted activities are unable to be met, the proposed <u>building</u> may be able to proceed by application for a discretionary or restricted discretionary activity. The assessment matters for restricted discretionary activities are listed. A reverse sensitivity issue may arise if the 20 metre <u>setback</u> in Rule 4.9.9 for the Living 2A zone at Prebbleton is not met, due to the nature of adjoining rural land uses at the southern zone <u>boundary</u>. This rule was adopted in the decision on a plan change under the previous District Plan. Similarly, Rules 4.9.6, to 4.9.8 were adopted as a consequence of plan changes made operative under the previous District Plan, and those provisions have been carried over.

The siting of a <u>dwelling</u> less than 150m from the Lincoln Sewage Treatment Plant is a non-complying activity, and is therefore not generally considered appropriate. This rule was also adopted as a consequence of a plan change made operative under the previous District Plan.

In the case of Rolleston Sewage Treatment Plant and Resource Recovery Park an "Odour Control Setback Area" has been imposed on the Holmes Block (as shown on the Outline Development Plan in Appendix 39). Building within this area is a Non-Complying Activity as reverse sensitivity issues may arise if this setback area is not applied.

In regard to the Poultry Farm identified on Lot 3 DP 20007 at Rolleston a 300m setback has been imposed in relation to the northern boundary of the Skellerup Block (as shown on the Outline Development Plan in Appendix 40). Building within this area is a non-complying activity as reverse sensitivity issues may arise if this setback area is not applied.

Relocated Buildings

Generally, the district plan does not control the design and standard of <u>buildings</u>, except for in the alpine villages at Arthur's Pass and Castle Hill (refer to Part B, Section 1.4). <u>Buildings</u> which are relocated as a whole, or in parts, on to a <u>site</u> have to be set on a <u>building</u> pad or foundations and often require restoration work to repair minor damage. If <u>relocated buildings</u> are left sitting on blocks or unrepaired for long periods of time, they can detract from the <u>amenity values</u> of Living zones.

Rule 4.10 identifies minor activities involving <u>relocated buildings</u> for which resource consent is not required. Also set out are the matters the Council will consider for <u>relocated buildings</u> of a large scale or of a permanent nature. The matters include the nature of reinstatement works, the time needed to complete them, and the mechanism to give Council the surety they will be completed.

A controlled activity may not be declined by the Council. It may be approved subject to conditions relating to the matters over which the Council has reserved control.

Note: Rule 4.10 does not affect the ability of a subdivider to impose any private covenant on the new Certificate of Titles related to the style and age of <u>buildings</u> able to be established, or the relocation of existing <u>buildings</u> into their subdivision.

Comprehensive Residential Development in Living Z Medium Density areas located within an Outline Development Plan

Within comprehensive residential developments in Medium Density areas covered by an Outline Development Plan, the Plan enables higher density forms of development to occur. Due to their higher density, such developments need to be planned in a comprehensive manner to ensure that adequate levels of urban design and amenity are achieved. The Plan therefore requires resource consent as a restricted discretionary activity with the Council's discretion limited to the urban design, appearance, and amenity of the development. This provision has been inserted to ensure that the widespread community concerns that have been voiced in other Districts regarding poor quality high density developments are able to be avoided in Selwyn, whilst still enabling the provision of a wide range of housing choice for residents. This provision includes an extensive set of assessment matters to ensure that higher density development provides a good level of amenity for residents, neighbours and views from public places including streets.

Buildings and Streetscene

In the Living Z zones, solid, high screening structures such as fences and walls that are erected on road boundaries of properties have the potential to cause adverse visual impacts on a pleasant and open streetscene along with other associated effects such as disconnection and reduction in the potential for passive security between dwellings and the street. The rule acknowledges that where sites have frontage to more than one road, the rule only applies to the façade that includes the front entrance to the allotment so that a reasonable degree of privacy is able to be achieved for corner sites or allotments that back onto a second road. On other sites, it is expected that subdivisions will be designed to ensure that privacy can be achieved on the lot and that buildings will have been positioned with the need for private outdoor space in mind.

Another key element in maintaining a pleasant and attractive streetscene is to ensure that developments are not visually dominated by garaging when viewed from the road, private Rights of Way, or shared accessways. The rule therefore limits the width of garages to no more than 50% of the front façade of the dwelling to ensure that front facades contain visual interest, and the opportunities for glazing and passive surveillance. In medium density areas this provision works in tandem with the requirement that garages not project forward of the front building façade to further minimise the visual impact of garaging.

Buildings and Private Outdoor Living Space

A minimum area and dimension of outdoor living space has been required for dwellings in Living Z Medium Density areas located within an Outline Development Plan to ensure that an area of each site is set aside which is sufficient to meet the outdoor living needs of current and future residents of the site. The rule only applies to Medium Density areas as smaller allotment sizes could result in relatively narrow strips of unbuilt land around dwellings that do not provide a useable outdoor living area, unlike larger allotments in the other living zones. The provision of an outdoor area also means that sites retain the opportunity for tree and garden planting which helps to balance the built appearance of higher density areas and visually soften the built environment.

A smaller area of outdoor living area is required for dwellings that form part of a comprehensive residential development due to such units generally being smaller than stand alone dwellings and in recognition that comprehensive residential developments require detailed plans to be assessed as part of a design and appearance consent where the location and functionality of outdoor living spaces can be assessed. Balconies can be counted towards the required outdoor living area in recognition of the positive role that well-located balconies can play in meeting the amenity needs of occupants and providing an alternative outdoor living area that may receive sun at a different time of the day. Balconies can also be a positive design element and are encouraged where they provide passive surveillance of the street or public open spaces without

resulting in subsequent pressure from landowners to fence these boundaries to ensure privacy which can happen with ground level outdoor living areas.

Setbacks from Waterbodies

Locating structures close to waterbodies (excluding aquifers) may:

- Cause bank erosion: either on the <u>site</u>; or elsewhere if works have been undertaken on the <u>site</u> to prevent bank erosion;
- Create a potential natural hazard, should the bank be unstable or the <u>waterbody</u> overflow;
- Affect habitat values, and the natural character of the edge of waterbodies; and
- Restrict public access, where public access is provided for.

Rule 4.15 sets the distances for structures from waterbodies (excluding aquifers) as a permitted activity. The distance varies with the type of <u>waterbody</u>, its flood potential and its ecological and recreational values. Structures may be able to locate closer to the <u>waterbody</u> with a resource consent. However, the closer siting (less than 20 metres) of <u>dwellings</u> and <u>principal buildings</u> is generally recognised as inappropriate.

Note: An additional consent may need to be obtained from Environment Canterbury.

Sites of Significance to Tāngata Whenua (Wāhi Taonga Management Areas)

Rule 4.16 manages the effects of erecting buildings in <u>Wāhi Taonga Management Areas</u> at Rakaia Huts. Activities which may disturb this area and sites require a resource consent, so the Council can assess whether the activity will affect any culturally important site, as advised by local rūnanga and in the case of Wāhi Taonga Management Are C39(b), the New Zealand Historic Places Trust Pouhere Taonga.

The Council has a policy to consider reducing or waiving resource consent processing fees for activities in such areas (see Part B, Section 3.3). For further information on these areas and sites, refer to the Reasons for the Earthworks Rules.

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Township	Zone	Average Allotment Size Not Less Than
Darfield	Living 1	650m ²
	Living 2	5,000m ²
	Living 2 (Deferred)	Refer to Subdivision – General Rules. 5,000m² if criteria met.
	Living 2A (Deferred)	Refer to Subdivision – General Rules. 1 ha if criteria met.
	Living 2A1	2 ha
	Living X (Deferred)	Refer to Subdivision – General Rules. What the subdivider nominates, but not less than the average for the Living 1 Zone in the township (650m²) if criteria met.
Doyleston	Living 1	650m ²
Dunsandel	Living 1	The size needed for on-site effluent disposal bu not less than 800m2
	Living 2	1 ha
	Living (Area A) (Deferred)	Refer to Subdivision – General Rules. Final density still to be determined.
	Living (Area B) (Deferred)	Refer to Subdivision – General Rules. Final density still to be determined.
Glenntunnel	Living 1	The size needed for on-site effluent disposal bun ot less than 800m ²
Hororata	Living 1	The size needed for on-site effluent disposal but not less than 800m ²
Kirwee	Living 1	800m ²
	Living 2	1 ha
	Living 2A	1 ha
		2 ha for lots along the northern and eastern boundaries of the zone that abuts a Rural Zone
Lake Coleridge Village	Living 1	800m ²
Leeston	Living 1	650m ²
	Living 1 (Deferred)	4 ha until deferral lifted, then 650m ²
	Living 2	5,000m ²
	Living 2 (Deferred)	4 ha until deferment lifted, then 5,000m ²
	Living 2A	5,000m ²
	Living XA	What the subdivider nominates, but not less than the average for the Living 1 Zone in the township (650m²)
Lincoln	Living 1	650m ²

Township	Zone	Average Allotment Size Not Less Than
	Living 1A	850m ² Minimum of 31 lots for any subdivision plan
	Living 1A1	650m ² Minimum of 31 lots for any subdivision plan
	Living 1A2	650m² Minimum of 31 lots for any subdivision plan
	Living 1A3	500m ²
	Living 1A4	1,500m ²
	Living 2	3,000m ²
	Living X	2,000m ²
	Living Z	Low Density: Average allotment size of 600m ² and a minimum individual allotment size of 500m ²
		Medium Density: Maximum average allotment size of 450m², with a minimum individual allotment size of 350m²
		Medium Density (Comprehensive): Maximum average allotment size of 350m², with no minimum site size.
		 Comprehensive residential development will be identified by a consent notice on the subdivision consent and will be located within Medium Density areas as identified on the ODPs – Appendix 37; and
		 Within a comprehensive residential development, a section 224 certificate shall only be issued following the erection (to the extent that the exterior is fully closed in) of the dwellings that are to be subdivided.
		Overall development within an ODP area shall achieve the net density target contained in the relevant ODP shown on appendix 37 of the township volume of the District Plan.
Prebbleton	Living 1	800m ²
	Living 1A	Area A: 1,250m²;
		Area b: 1,000m ²
		Area C: 800m ²
		In all cases development shall proceed in accordance with the ODP contained in Append 19 and shall achieve a minimum net density of 8 households/per hectare once the entire site has been developed.
		2,000m ² shall apply to the balance of the zon

Township	Zone	Average Allo	otment Size Not Less Than
	Living 1A2	800m ² and no	o more than 10 % at less than
	Living 1A3	800m ² and no	o more than 10 % at less than
	Living 1A4	800m ² and no	o more than 10 % at less than
	Living 1A5	700m². For <u>c</u>	o more than 10 % at less than comprehensive residential the minimum average area shall
	Living 1A6	_	Area A: 1000m² minimum net allotment area;
		_	Area B: 600m² minimum net allotment area and 900m² maximum net allotment area;
		-	Area C: 550m² minimum average allotment area and 450m² minimum net allotment area; and
		-	In all cases development shall proceed in accordance with the ODP and shall achieve a minimum density of 10 lots/ha once the entire site has been developed.
	Living 2	5,000m ²	
	Living 2A	5,000m ²	
			mber of allotments is 32, and on e of Trices Road the maximum otments is 8
	Living 2A (Blakes Road)	5,000m ²	
			hall proceed in substantial vith the development plan in
	Living 2A (The Paddocks)	1.5 ha minim	um allotment size
	Living X		divider nominates, but not less age for the Living 1 Zone in the Om²)

Township	Zone	Average Allotment Size Not Less Than
	Living Z	Low Density: Average allotment size of 700m ² with a minimum individual allotment size of 550m ²
		Medium Density: Maximum average allotment size of 450m², with a minimum individual allotment size of 350m²
		Medium Density (Comprehensive): Maximum average allotment size 350m², with no minimum site size.
		 Comprehensive residential development will be identified by a consent notice on the subdivision consent and will be located within Medium Density areas as identified on the ODP's – Appendix 42; and
		 Within a comprehensive residential development, a section 224 certificate shall only be issued following the erection (to the extent that the exterior is fully closed in) of the dwellings that are to be subdivided.
		Overall development within an ODP area shall achieve the net density target contained in the relevant ODP shown in Appendix 42 of the Township volume of the District Plan.
Rakaia Huts	Living 1	The size needed for on-site effluent disposal but not less than 800m ²
Rolleston	Living 1	750m²
	Living 1A	Minimum lot area of 300m ²
	Living 1B	1,200m ² with a minimum lot area 750m ²
	Living 1C	2,000m ² with a minimum lot area of 1,000m ²
	Living 2	5,000m ²
	Living 2A	1 ha
	Living 3 (Appendix 39 & 40)	At least 20ha of the land within the area defined by the Outline Development Plan at Appendix 39 and 40 shall be developed as a Lower Density Area in the location shown on the Outline Development Plan with a minimum and an average allotment size of no less than 4ha.
		The balance of the land on the Outline Development Plans at Appendix 39 and 40 outside the above area shall be developed with an average allotment size of no less than 5000m ² with a minimum allotment size of 4000m ² .
		The maximum number of allotments within the area defined by the Outline Development Plan at Appendix 39 shall be 97.
		The maximum number of allotments within the area defined by the Outline Development Plan at Appendix 40 shall be 51.

Township	Zone	Average Allotment Size Not Less Than
	Living Z	Low Density: Average allotment size of 650m ² with a minimum individual allotment size of 550m ²
		Medium Density: Maximum average allotment size of 450m², with a minimum individual allotment size of 350m²
		Medium Density (Comprehensive): Maximum average allotment size 350m ² -, with no minimum site size.
		 Comprehensive residential development will be identified by a consent notice on the subdivision consent and will be located within Medium Density areas as identified on the ODPs – appendix 38; and
		 Within a comprehensive residential development, a section 224 certificate shall only be issued following the erection (to the extent that the exterior is fully closed in) of the dwellings that are to be subdivided.
		Overall development within an ODP area shall achieve the net density target contained in the relevant ODP shown in Appendix 38 of the Township volume of the District Plan.
Sheffield	Living 1	The size needed for on-site effluent disposal bu not less than 800m ²
	Living 1A	The size needed for on-site effluent disposal burnot less than 800m ²
Southbridge	Living 1	650m²
Springfield	Living 1	The size needed for on-site effluent disposal bu not less than 800m ²
Springston	Living 1	800m ²
	Living 1A	800m ²
Tai Tapu	Living 1A	800m ²
	Living 2A	5,000m ²
Waddington	Living 1	The size needed for on-site effluent disposal bu not less than 800m ²
West Melton	Living 1	1,000m²
	Living 1B	2,800m ²
	Living 2	5,000m ²
	Living 2A	Maximum number of allotments is 10, and a minimum <u>allotment</u> size of 1 ha.
	Living WM Medium Density	Minimum lot area of 500m ² and maximum lot

Township	Zone	Average Allotment Size Not Less Than	
	Living WM Low Density	Minimum lot area of 3000m² and maximum lot area of 5000m² (Appendix 20A)	
	So that a total of 292 allotme Zone	nts must be achieved across the whole Living WM	
Whitecliffs	Living 1	The size needed for on-site effluent disposal but not less than 800m ²	
All <u>Townships</u>	Heritage Buildings listed in Appendix 3	The <u>building</u> , curtilage and any other area needed to:	
		- mitigate adverse effects; or	
		- maintain the heritage values of the site	
	Community Reserves, Community Facilities and Point Strips	No minimum area	
All Living Zones	Calculating <u>Allotment</u> Sizes		
	The average <u>allotment</u> size shall be calculated as a mean average (total area of allotments divided by the number of allotments). The total area and number of allotments used to calculate the mean shall exclude areas used exclusively for <u>access</u> , reserves or to house <u>utility structures</u> or which are subject to a designation. Any <u>allotment</u> which is twice or more the size of the average <u>allotment</u> required the zone, shall be calculated as being: 2 x average <u>allotment</u> size for that zone – 10 m ² ; or as its actual size, if a covenant is placed on the Certificate of Title to prevent any further subdivision of that land.		
All <u>Townships</u>	Allotment sizes for Flats/Towr	nhouses – Living 1	
	In any Living 1 Zone, where two or more <u>dwellings</u> have been erected on an <u>allotment</u> the average <u>allotment</u> size per <u>dwelling</u> shall be 0.5 x the average <u>allotment</u> size listed in this table for the Living 1 Zone in that <u>township</u> .		

12.1.4 Matters over which the Council has restricted the exercise of its discretion:

Access

- 12.1.4.1 If any <u>allotment</u> has <u>access</u> on to a State Highway or Arterial Road listed in Appendix 7:
 - (a) Any adverse effects, including cumulative effects, on traffic safety and traffic flow on the <u>Strategic Road</u>; and
 - (b) The design and location of the <u>vehicular accessway</u> and vehicle crossing; and
 - (c) Whether <u>access</u> to the <u>allotment(s)</u> can be obtained off another <u>road</u> which is not a <u>Strategic Road</u> either directly or by an easement across other land.
- 12.1.4.2 If access by a private accessway is proposed, whether the land the accessway serves has capacity for any intensification of density under District Plan averages for the zone and, if so, whether provision of a

Discretionary Activities — Subdivision – General

- 12.1.6 The following activities shall be discretionary activities:
 - 12.1.6.1 Any subdivision subject to Rule 12.1.1 which does not comply with Rules 12.1.3.9 or 12.1.3.10.
 - 12.1.6.2 Any subdivision subject to Rule 12.1.1 which does not comply with Rule 12.1.3.13.
 - 12.1.6.3 Any subdivision in the Living 2 Zone at Coalgate or Dunsandel with an average <u>allotment</u> size of less than 1 hectare.
 - 12.1.6.4 Any subdivision in a Living 2 zone other than at Kirwee, Coalgate or Dunsandel with an average <u>allotment</u> size of less than 5000m².
 - 12.1.6.5 Any subdivision in a Living 1C zone with an average lot size between $1.200m^2 2.000m^2$.
 - 12.1.6.6 Any subdivision in the Living 2 zone at Lincoln with an average allotment size of less than 3,000m².
 - 12.1.6.7 Any subdivision in a Living Z Zone that is not in general compliance with an operative Outline Development Plan.

In the event that a medium density residential subdivision is proposed outside a Medium Density area shown on an operative Outline Development Plan, and is assessed by the Council as being acceptable, then a consent notice or similar mechanism shall be registered on the title of those lots indicating that the District Plan controls relating to those sites are to be those applying to the Living Z Medium Density areas. Conversely, in the event that lower density subdivision within an area shown on an operative Outline Development Plan as a Medium Density area is assessed as being acceptable then a consent notice or similar mechanism shall be registered on the title of those lots indicating that the District Plan controls relating to those sites are to be those applying to the Living Z lower density areas.

Non-Complying Activities — Subdivision – General

- 12.1.7 Except as provided for in Rules 12.1.5 and 12.1.6, the following activities shall be non-complying activities:
 - 12.1.7.1 Any subdivision subject to Rule 12.1.1 which does not comply with Rule 12.1.3.
 - 12.1.7.2 Upon deposit of a Plan of subdivision, any further subdivision (other than by way of <u>boundary</u> adjustment) of any <u>allotment</u> within that Plan of subdivision (other than a balance lot) in the Living 1A Zone at Lincoln.
 - 12.1.7.3 In the Living 1A5 Zone at Prebbleton, subdivision for <u>comprehensive</u> <u>residential development</u> shall be a non-complying activity where a land use consent for a <u>comprehensive residential development</u> has not been obtained.

- 12.1.7.4 Any subdivision in the Lowes Road Outline Development Plan area that is not in general accordance with the Outline Development Plan.
- 12.1.7.5 Any subdivision in a Living 1C zone with an average lot size below 1,200m2.
- 12.1.7.6 Any subdivision within a Living Z (deferred) Zone shown on the Planning Maps shall be a non-complying activity where it does not comply with the provisions of the Rural (Inner Plains) Zone.
- 12.1.7.7 In a Living Z Medium Density area located within an Outline Development Plan, any subdivision to create an allotment less than 350m2 that is not part of a comprehensive residential development shall be a non-complying activity. Subdivision of a comprehensive residential development to create individual lots smaller than 350m2 shall be a non-complying activity prior to the erection (to the extent that the exterior is fully closed in) of the dwellings that are to be subdivided.
- 12.1.7.8 Any subdivision that does not comply with Rule 12.1.3.34.
- 12.1.7.9 The subdivision of land within the Living 1 and Living 2A Zones within the Darfield Outline Development Plan within Appendix 41 shall be a non-complying activity until:
 - a) A potable water supply is available which is capable of serving the lots within the subdivision; and
 - b) A wastewater disposal system is available which is capable of serving the lots within the subdivision.

12.2 SUBDIVISION — BOUNDARY ADJUSTMENTS

Note: If a <u>boundary</u> adjustment completed under 12.2 creates a land use activity which no longer complies with the district plan provisions for a permitted activity, a separate land use consent may be required for the land use activity.

Restricted Discretionary Activities — Subdivision – Boundary Adjustments

- 12.2.1 Any subdivision to adjust the boundaries between existing allotments shall be a restricted discretionary activity if it complies with the following standards and terms:
 - 12.2.1.1 All allotments subject to the <u>boundary</u> adjustment are adjoining or separated by a <u>road</u>, railway line, <u>vehicular accessway</u> or waterbody (excluding aquifers); and
 - 12.2.1.2 No additional allotments are created as a result of the <u>boundary</u> adjustment; and
 - 12.2.1.3 The area of any <u>allotment</u> after the <u>boundary</u> adjustment has not decreased the smallest <u>allotment</u> existing after the <u>boundary</u> adjustment by an area greater than 15% of that of the smallest <u>allotment</u> prior to the <u>boundary</u> adjustment (except where any such <u>allotment</u> is for the purpose of corner rounding or access to a road); and

Mining: has the same meaning as "mining" in section 2 of the Crown Minerals Act 1991. *

Monofill: Includes any landfill which is designed and operated to accept waste associated with one specific activity or form of waste. For the purposes of Rule VII, a monofill does not include any landfill which accepts hazardous waste.

N

Neighbourhood Centre: is a grouping of principally convenience stores (in the order of 6-15 stores) predominantly servicing the local communities weekly and day-to-day retail requirements.

Net Density: is the number of lots of household units per hectare (whichever is the greater). The area (ha) includes land for:

- Residential purposes, including all open space and on-site parking associated with residential development;
- Local roads and roading corridors, including pedestrian and cycle ways, but excluding State
 Highways and major arterial roads;
- Local (neighbourhood) reserves.

The area (ha) that excludes land that is:

- Stormwater retention and treatment areas;
- Geotechnically constrained (such as land subject to subsidence or inundation);
- Set aside to protect significant ecological, cultural, heritage, or landscape values;
- Set aside for esplanade reserves or access strips that form part of a larger regional or subregional reserve network;
- For local community services and retail facilities, or for schools, hospitals or other district, regional or sub-regional facilities.

Network Infrastructure: has the same meaning as in section 197 of the Local Government Act 2002.

Noise Limit: means a L10, Leq or Lmax sound level in A-frequency-weighted decibels that is not to be exceeded during a measurement sample time in a specific time-frame. See NZS 6801:1999 Acoustics – Measurement of Environmental Sound.

Noise Sensitive Activities:

- Residential activities other than those in conjunction with rural activities that comply with the rules in the plan;
- Educational activities including pre-school places or premises;
- Travellers' accommodation except that which is designed, constructed and operated to a standard that mitigates the effects of noise on occupants;
- Hospitals, healthcare facilities and elderly persons housing or complex.

Noticeboard: means a structure or device upon which notices can be placed or attached that are of community interest, which are intended to be read by people stopping at the noticeboard rather than by people passing by.

Notional Boundary: means a line 20m from any side of a rural dwelling or the legal boundary where this is closer to the dwelling.

NZTA: the New Zealand Transport Agency

0

Office: a place where administrative, business, clerical or professional and, or management activities are conducted.

On-site Public Space: means de-facto public space occurring on private sites. This includes all places where the public may be present including visitor car parking areas, private lanes and accessways to buildings. It excludes areas such as service lanes for the delivery of goods.

Operating Speed: The speed at which motor vehicles generally operate (85th percentile speed) on a particular road. Where operating speeds are unknown they are assumed to be the posted speed limit plus 10km/hr.

Outdoor Display Area: For the purpose of calculating car parking requirements, outdoor display area shall include the area of any land within a site where goods are on display for sale.

P

Parking Areas: means a continuous portion of a site, sites, allotment, allotments or part of any site or allotment on which parking for motor vehicles is provided and includes associated access.

Pathway: means a formed path for pedestrians and, or cyclists.

Pedestrian-cyclist link: means a green transport corridor for pedestrians and, or cyclists that for example links a road to a road, or a road to a reserve or facility. They are also known as 'walkway/cycleway links'. There is generally a pathway provided within the corridor for pedestrians and cyclists to share.

Place of Assembly: means any land and building used for gathering of people. It does not include residential accommodation or places of work.

Plantation: means any group of trees planted on a site, whether intended to be harvested or not, which are not classed as amenity plantings or shelterbelts. A plantation may be comprised of exotic or indigenous species and includes but is not limited to: forests planted for harvesting, conservation, soil erosion control, pest or wilding tree management, and any orchard, vineyard or woodlot which does not comply with the definition of amenity planting.

Point Strip: means a strip of land adjoining the side or end of a road, the purpose of which is to prevent access to that road from land adjoining the point strip. Point strips may also be for the purpose of protecting the opportunity to achieve future transport links between adjoining blocks of land. Such a strip is usually (although not necessarily) about 200mm in width.

Posted Speed Limit: the legal and sign posted speed limit that applies to the road as per the Selwyn District Council Speed Limits Bylaw.

Preschool: means land or buildings used for care and/or education of more than 3 children who do not reside on-site and are under the age of 6 years.

Principal Building: includes any building or buildings which is/are used as part of the primary activity or activities on the site. Principal buildings include dwellings but do not include accessory buildings.

Professional Staff: includes registered medical practitioners, dentists, veterinary surgeons, physiotherapists, practice nurses or any other specialist employed at a health facility who patients may consult for medical advice.

Project: for the purpose of the land use rules for earthworks, the term 'project' includes all earthworks undertaken as part of, or ancillary to, the completion of one particular activity of the construction of a building or structure, whether that activity is undertaken or the building or structure is erected continuously or in discrete stages, and whether it occurs in one continuous area or is separated by land which is not disturbed by earthworks. For example: the construction of one road or track from destination (a) to (b) is one project, as is the disturbance of soil for geological surveys over an identified area.

Protected Tree: a tree or group of trees, which are of significance to the District, and are listed in the protected tree schedule in Appendix 4.

These trees fall into two categories:

- Category A: trees that have particularly high levels of significance, particularly in relation to heritage values, and score the highest under the evaluation criteria.
- Category B: trees that have significance for a range of reasons (e.g. landscape importance)
 and have a moderate score under the evaluation criteria.

Public Floor Area (PFA): means the sum of all floor areas or outdoor area available for the use of the general public in association with an activity, but excludes lift wells, stairwells, toilets, bathrooms and parking areas.

Q

Quarrying: means to take, mine or extract, by whatever means any rock, stone, gravel or sand existing in its natural state in land. "To quarry' has a corresponding meaning.

R

Radio Communication Facility: means any transmitting or receiving devices such as aerials, dishes, antenna, cables, lines, wires and associated equipment/apparatus, as well as support structures such as towers, masts and poles, and ancillary buildings.

Recreational Facility or Recreational Activity: includes the use of any land, building or structure for the primary purpose of recreation or entertainment and is available to be used by members of more than one household.

Relocated Building: includes any building that is removed from one site and relocated to another site, in whole or in parts. It does not include any new building which is designed for, or intended to be used on, a site but which is erected off the site, in whole or in parts, and transported to the site.

Research: means the use of land and buildings for the purpose of scientific research, inquiry or investigation, product development and testing, and consultancy and marketing of research

information; and includes laboratories, quarantines, pilot plant facilities, workshops and ancillary administrative, commercial, conferencing, accommodation and retail facilities.

Residential Activity: means the use of land and buildings for the purpose of living accommodation and ancillary activities. For the purpose of this definition, residential activity shall include:

- a) Accommodation offered to not more than five guests for reward or payment where the registered proprietor resides on-site
- b) Emergency and/or refuge accommodation
- c) Supervised living accommodation and any associated caregivers where the residents are not detained on the site

Residential Activity does not include:

- a) Travelling accommodation activities (other than those specified above)
- b) Custodial and/or supervised living accommodation where the residents are detained on site.

Retail Activity: the use of land or buildings for displaying or offering goods for sale or hire to the public, including service stations. For the purposes of calculating car parking requirements, slow trade and bulk goods retail shall mean large goods which typically have a low turn-over such as building supplies, white wares, furniture and vehicles.

River: has the same meaning as defined in section 2 of the Act.

Road: shall have the same meaning as defined in section 315 of the Local Government Act 1974. * and shall include roads to be vested.

Road Boundary: refer to Boundary.

S

Secure: with respect to cycle parking, means that cyclists and their bicycles are not exposed to danger or harm; for example cycle parking is not located where manoeuvring vehicles could collide with a cyclist and or their parked bicycle; also that the cycle parks are not located where there is no visibility from a public space (i.e. a road or car parking area).

Service Station: means any site where the dominant activity is the retail sale of motor vehicle fuels (including petrol, LPG, CNG and diesel) and may also include any one or more of the following:

- The sale of kerosene, alcohol based fuels, lubricating oils, tyres, batteries, vehicle spare parts and other accessories normally associated with motor vehicles;
- Mechanical repair and servicing of motors (including motor cycles, caravans, motor boats, trailers);
- Warrant of fitness testing;
- The sale of other merchandise where this is an ancillary activity to the sale of the motor fuel and vehicle accessories;
- Truck stops.

Except that for the purposes of calculating car parking requirements, the following may be separately assessed: Mechanical repair and servicing of motors (including motor cycles,

caravans, motor boats, trailers), Warrant of fitness testing and or the sale of other merchandise where this is an ancillary activity to the sale of the motor fuel and vehicle accessories;

Setback: means the minimum prescribed distance between the exterior face of the building and the boundaries of its site. The following intrusions are permitted into any setback area:

- a) Eaves being no more than 600mm wide.
- b) Any porch, windbreak, chimney, external stairway or landing being no more than 1.8m long and extending no more than 800mm into the setback area.
- c) Any utility structure attached to an existing building or structure located in a setback from a waterbody provided that it does not protrude more than 1.5m from that existing building or structure.

Shelterbelt: means any trees planted primarily to provide shelter for stock, crops or buildings from the prevailing wind(s). Shelterbelts are not more than 20 metres in width and are not clearfelled.

Sign: means any device or structure which is visible from any public space and is used to: identify any site or building; provide directions or information; or promote any goods, services, or forthcoming event. A sign does not include any window display, or property identification signs which do not exceed an area of 0.2 m² (including rural numbers, dairy company numbers, street/road numbers/property names and property owners' names as long as those property identifiers do not include any advertising).

Silent File Area: includes any land which is listed in Appendix 5 and shown on the Planning Maps as a Silent File Area.

Site: means an area of land or volume of space:

- Held in a single certificate of title, or
- Comprised of two or more adjoining certificates of title held together in such a way that they
 cannot be dealt with separately without the prior consent of the Council; or
- For which a separate certificate of title could be issued without further consent of the Council.

Solid Waste: includes any material which is discarded as being spent, useless, worthless or in excess, and includes liquid or gaseous waste which is stored in containers.

Sound Exposure Level: means the A-frequency weighted sound pressure level in decibels which, if maintained consistent for a period of 1 second, would convey the same sound energy to the receiver as is actually received from a given noise event over the same period. See NZS 6801:1999 Acoustics – Measurement of Environmental Sound.

Spiritual Activity: means land and/or buildings used for the public and/or private assembly of people primarily for worship, meditation, spiritual deliberation and ancillary community facilities of a non-commercial nature.

State Highway: means any road that is identified as a State Highway in the road hierarchy classification as listed in Appendix 7 and managed by the New Zealand Transport Agency. They are high capacity roads of national importance providing inter-district and regional links between significant transport destinations such as towns, cities, ports and other places of significance. State Highways are maintained to high standards to ensure they operate efficiently, including managing both road and property access to them through the New Zealand Transport Agency's powers under the Government Roading Powers Act. They are also subject to access controls in this Plan.

Subdivision Consent has the meaning set out in section 87(b) of the Act.

Subdivision of Land and to subdivide land have the meanings set out in section 218 of the Act.

T

Telecommunication Facility: means any telecommunication line, telephone exchange, telephone booth, telephone cabinet or pay phone, or any other structure, facility or apparatus intended for the purpose of effecting telecommunication.

Telecommunication Line: means a wire or wires or a conductor of any other kind (including a fibre optic cable) used or intended to be used for the transmission or reception of signs, signals, impulses, writing, images, sounds, or intelligence of any nature by means of any electromagnetic system; and includes any pole, insulator, casing, fixture, tunnel or other equipment or material used or intended to be used for supporting, enclosing, surrounding, or protecting any such wire or conductor and also includes any part of a line.

Temporary Accommodation: includes the use of any building to house any person for residential or business activities on a site, while construction work is being undertaken on the site. Temporary accommodation may be provided for persons occupying the site on which construction work occurs, or for persons involved in the construction work.

Temporary Activity: includes any activity which occurs on any site for a period of not more than 15 consecutive hours in any one time and occurs on no more than 12 times in any 12 month period; or any activity which does not last longer than a total of 7 consecutive days in any one time and occurs on not more than 3 times at any one site in any 12 month period.

Temporary Military Training Activity: means a temporary activity undertaken for Defence Purposes. Defence purposes are those in accordance with the Defence Act 1990.

Temporary Sign: includes any sign erected to advertise an upcoming event of interest to the community, or any sign which advertises the products or services of a business or organisation which is associated with an activity on these sites on which the sign is erected. Any temporary sign shall not be erected on any site for more than 6 consecutive calendar months at any one time, or for a collective period of more than 6 calendar months in any 2 year period, except that for any school or church anniversary event the temporary sign may be displayed for up to 12 months prior to the event.

Tertiary Education: means the use of land and buildings for the purpose of facilitating tertiary education, training, development and instruction and/or related research and laboratories; and includes ancillary and accessory administrative, cultural, commercial, communal, conferencing, accommodation, retail and recreational facilities.

Township: means an urban area within the District that comprise a Living zone(s) and in some situations a Business zone(s).

Tree: any woody perennial plant, typically with a distinct trunk (but sometimes multi-stemmed) from which branches arise well above ground level to form a crown, and includes other plants of a tree-like size and form such as palms.

U

Utility: includes the use of any structure, building or land for any of the following purposes;

- (a) The generation, transformation and/or transmission of energy;
- (b) Any telecommunication facility or telecommunication line;

- (c) Any radio communication facility;
- (d) The conveyance, storage, treatment or distribution of water for supply, including (but not limited to) irrigation and stockwater;
- (e) The drainage, reticulation or treatment of stormwater, waste water or sewage;
- (f) Transport infrastructure, including (but not limited to) roads, accessway, railways, airports and navigational aids;
- (g) Work to mitigate potential natural hazards, including (but not limited to) stopbanks, groynes and gabions;
- (h) Meteorological facilities for the observation, recording and communication of weather information.

Utility Building: includes any building or part of any building which is a utility or which is used principally to house or support a utility; and that building is $10m^2$ or more in gross floor area, and greater than 2.5m in height.

Utility Structure: includes any device, equipment or other facility which is used principally to house or support a utility including any antenna, mast, pole or pylon; or any structure housing a utility which is less than $10m^2$ in gross floor area, or less than 2.5m in height.

V

Vehicle Accessway: means any area of land, the primary purpose of which is to provide access between the body of any allotment(s) or site(s) and any road reserve. Accessway includes any rights of way, access lot, access leg or private road.

Vehicle Crossing: means the area within the road reserve over which vehicles move from the carriageway to a site. The width of a vehicle crossing shall be defined as the formed width at the property boundary. The length of the crossing is the distance from the edge of the carriageway to the property boundary.

Vehicle Crossing: includes any formed vehicle entrance or exit point from any site on to any road, and includes that part of the road boundary across which the vehicle access is obtained and any culvert, bridge or kerbing.

Vehicle Movement: means a single motor vehicle journey to or from a particular site. "Vehicle trip" has the same meaning.

Visitor Accommodation: means the use of land and buildings for transient accommodation offered on a daily tariff, except as provided for under the definition of a residential activity. Visitor accommodation may involve the sale of food and liquor to in-house guests.

W

Wāhi Taonga Management Area: includes any land, building or structure which is listed in Appendix 5 and shown on the Planning Maps as a Wāhi Taonga Management Area.

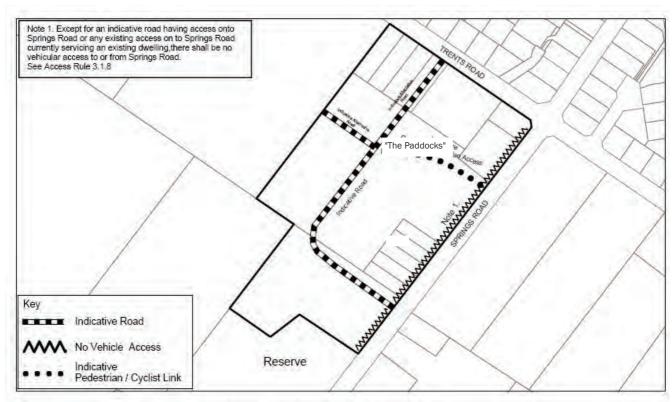
Wāhi Taonga Site: includes any land, building or structure which is listed in Appendix 5 and shown on the Planning Maps as a Wāhi Taonga Site.

Walkway: See Pedestrian-cyclist Link

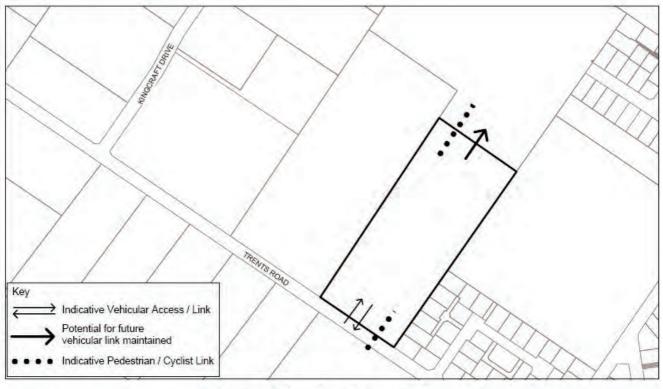
Waterbody: means fresh water or geothermal water in a river, lake, stream, pond (but excluding any artificial pond), wetland, or aquifer, or any part thereof that is not located within the coastal marine area.

Wetland: has the same meaning as defined in section 2 of the Act. *

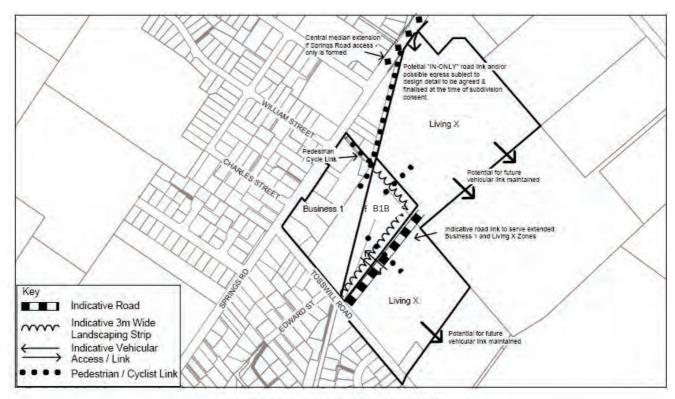
Workbay: for the purposes of calculating parking requirements, shall be the area required for each motor vehicle in a space where it can be serviced and any area immediately surrounding the vehicle required for lifts / hoists that enable the vehicle to be worked upon. It is noted that any other floor area within the building surrounding the work bay shall be considered as retail, office or industrial as appropriate.



Selwyn District Council Outline Development Plan Banham & Tapp Appeal 27/04/06



Selwyn District Council Outline Development Plan Tricat Appeal 27/04/06



Selwyn District Council Outline Development Plan Bates and Rossendale Wines Appeal 27/04/06

LIVING 1A ZONE OUTLINE DEVELOPMENT PLAN – PREBBLETON

INTRODUCTION

This Outline Development Plan (ODP) applies to the undeveloped balance of the Living 1A zone in Prebbleton. The ODP covers 15.02ha of land comprising seven separate properties that either front or have legal access to either Toswill Road or Trices Road. The development block is contained by the Prebbleton Domain to the north-west, Rural (Inner Plains) zone to the north-east and south, and the Stonebridge Way development (being the developed portion of the Living 1A zone) to the west.

The ODP provides an overarching urban design framework to guide the future development of the land. The ODP includes Land Use, Movement, Green and Blue Networks and incorporates the wider strategic and community outcomes expressed in the Prebbleton Structure Plan.

URBAN DESIGN

The design principles that underpin this ODP are in line with the New Zealand Urban Design Protocol and accord with the Selwyn District Council Subdivision Design Guide (September 2009). The following environmental outcomes are to be achieved:

- Development that meets the District Plan policies, realises an overall increase in residential density, applies urban consolidation principles and assists in achieving a compact concentric settlement pattern for Prebbleton.
- Provision for a range of section sizes and housing typologies to respond to the wider needs of the community, whilst achieving the prescribed minimum household densities and minimum average allotment sizes.
- Subdivision layouts that integrate with adjoining neighbourhoods and incorporate existing land uses
 where appropriate. The wider context of the development area should influence the subdivision
 layout by protecting and enhancing cultural, ecological, heritage and tangata whenua values and
 existing built features, such as amenity trees and water races.
- Layouts and urban design treatments that create a distinguishable sense of place, assist in enhancing the wider character and amenity of Prebbleton and deliver safe, vibrant and healthy living environments. Layouts should apply Crime Prevention through Environmental Design (CPTED) design principles.
- Integrated and legible road hierarchy that supports safe and efficient connections and promotes walking and cycling. Road design and landscape treatments should contribute to the overall character of Prebbleton and assist in connecting residential development with open space reserves and other public assets and services within the township, such as the Domain, Primary School, Nature Park and the town centre.
- Sustainable methods to treat and dispose of stormwater that protect groundwater resources from contamination, while integrating with open space and reserves where appropriate.
- Installation of all the necessary infrastructure services within the ODP area, and the cost effective and efficient connection of those services to the wider network.

LAND USE

The ODP Area shall achieve a minimum net density of 8 households per hectare.

Dwellings must front Trices Road and Tosswill Road to enhance passive surveillance and safety, while preserving the semi-rural streetscape. Appropriate design layouts should take into consideration the shape, orientation and aspect of sections, with internal roads supporting access that avoids housing from

backing onto Trices Road and Tosswill Road. An exception is made for the parcels that are affected by the limited access requirements onto Trices Road and Tosswill Road, which will need to be access from the internal road network.

The ODP supports three different densities, which respond to the context of the site and support a range of sections sizes, housing typologies and land use activities. Lower density sections should be established along the Tosswill Road and Trices Road boundaries to support an appropriate scale of development at the sensitive rural/urban interface. A 10m building setback for dwellings and utilities is necessary along Trices Road to provide separation and to distinguish the residential neighbourhood from the rural land holdings to the south.

Appropriate interface treatments, and methods to protect these treatments in the long term, need to be established along the Trices Road and Tosswill Road boundaries, which form a gateway to the township and transition from rural to urban. These treatments are to ensure the development integrates with the wider area and addresses any amenity conflicts that may arise at this sensitive residential/rural boundary. Treatments could include appropriate bunding fencing, retention of a portion of the existing macrocarpa hedgerows or landscaping to avoid long lengths of solid fencing or screening.

Residential housing adjacent to Prebbleton Domain must front the reserve. These lots will be accessed off a Local Minor Road. This will promote passive surveillance, support front yards facing towards the Domain and avoid a streetscape that is comprised of tall fencing or screening that may undermine the amenity afforded by the reserve. Suitable methods, such as fencing controls, landscape treatments and set backs, should be formalised to ensure all future residential development that overlooks Prebbleton Domain optimises the high amenity and open space outlook provided by the reserve.

MOVEMENT NETWORK

The overall aim is to create an integrated transport network that incorporates all modes of transport.

A Local Major Road connects Tosswill Road with Trices Road. Provision should be made for walking and cycling within this Local Major Road to support access to Prebbleton Domain and the wider circular pedestrian and cycling network identified in the Prebbleton Structure Plan.

Local Intermediate Roads provide secondary connections to Tosswill Road and Trices Road and support an integrated network to service the south-eastern portion of the development block. There is a limitation on direct access onto Tosswill Road and Trices Road in between the intersection of these roads and the Local Intermediate Roads. This restriction will assist in achieving a safe and transport network, responds to the future upgrades identified for Trices Road and enables a stepped speed reduction as vehicles enter Prebbleton from the south-east. Interface treatments along the portion of Trices Road and Tosswill Road where access is limited will need to be established at subdivision to achieve a high amenity gateway to the township as the land use activities transition from rural to urban. A Local Minor Road supports links to the Prebbleton Domain and adjacent community facilities.

The open space corridor is an important component of the overall movement network within the Township as it provides an off-road walking and cycling connection between the Domain and Trices Road. This corridor secures walking and cycling connections to the existing Domain and the proposed extensions on the opposite side of Tosswill Road, which provides an alternative to the existing alignment of the Christchurch to Little River Rail Trail. Two green link reserves support further connectivity between the Domain and the primary road network within the development block.

The remaining internal roading layout must be able to respond to the possibility that this area may be developed progressively over time. Road alignments must be arranged in such a way that long term interconnectivity is achieved once the block is fully developed.

GREEN NETWORK

The ODP indicates a 10m wide green space corridor based around the existing water race and walking and cycling network, connecting Prebbleton Domain to Tosswill Road. The future subdivision scheme should utilise this feature as a focal point to optimise the amenity that it provides and the opportunity it presents in achieving a well connected living environment.

A 2,000m² reserve is required to accord with the reserve provision calculations and to ensure the development block is served by sufficient and appropriate open space. The location of this reserve is identified as being indicative only pending confirmation at subdivision of the extent and location of any stormwater treatment areas. The future location of the reserve must ensure people living within the development block have access to an open space reserve within a 400m radius of their homes. Two green space links connect the proposed future development with the existing domain and the town centre to facilitate walking and cycling connections.

Further investigations shall be undertaken at subdivision to determine the practicalities of retaining the following existing specimen trees within any future layout:

- Four oak/Quercus trees on the frontage of 102 Tosswill Road (Lot 1 DP 71108).
- Various border plantings on the north-western boundary of 104 Tosswill Road (Lot 2 DP 71108) where the section adjoins Prebbleton Domain, which is identified as an appropriate interface treatment between any future development of the property and the high amenity presented by the domain.

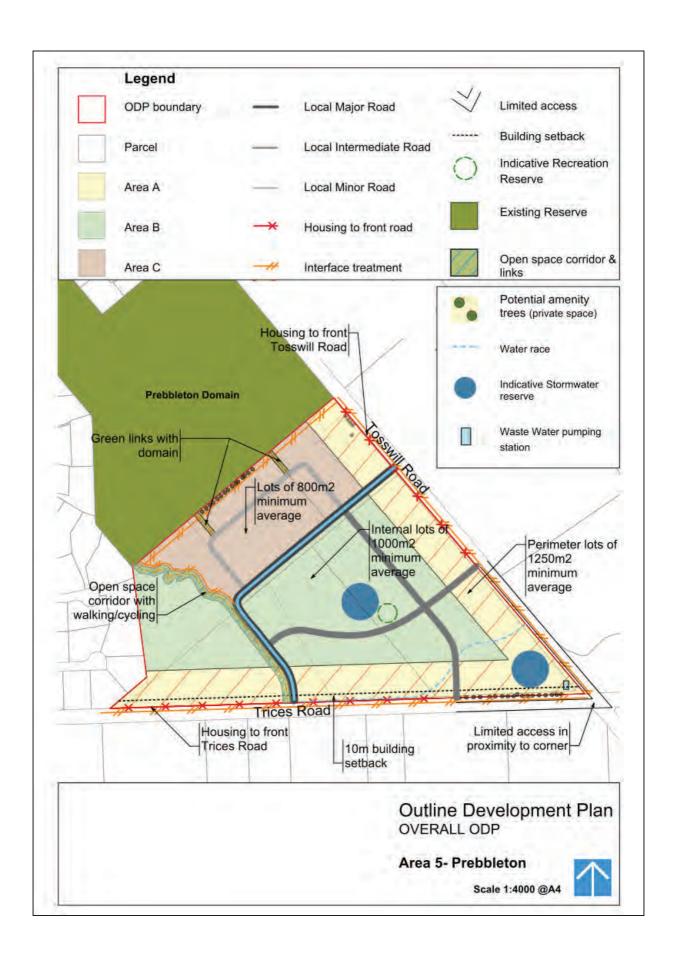
These trees are a link to the historic use of the land, compliment the streetscape and assist in preserving and enhancing the character of Prebbleton. Options to secure the on-going protection of these specimens if retained include consent notices or private covenants to assure the trees longevity.

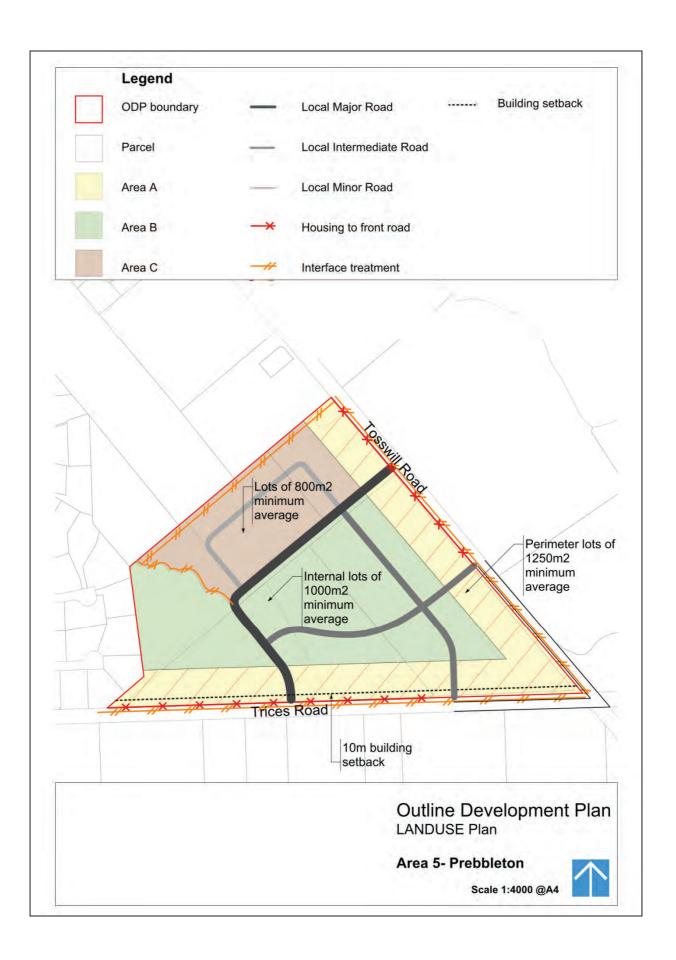
BLUE NETWORK

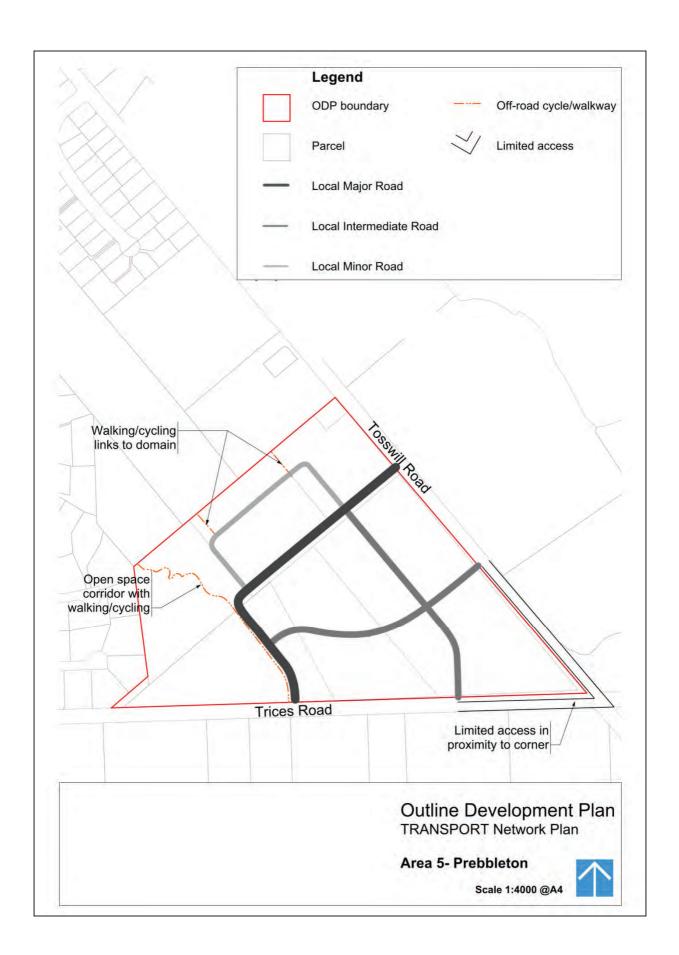
The current 50mm diameter water main that services the general area is identified to be at full capacity. A new 150mm water main will be required to service the development block. There is sufficient capacity in both the Tosswill Road and Trices Road water main and wider network to support the additional water connections required to service the area, with works having to accord with the upgrades identified for the township.

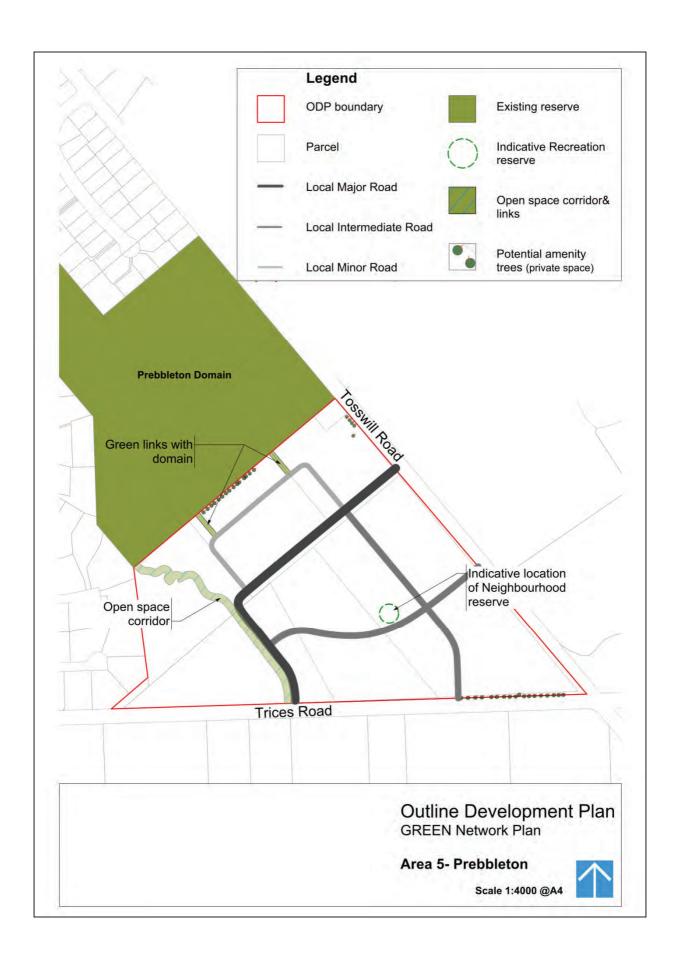
A new pumping station is required to be established at the eastern corner of the development as the Tosswill Road pumping station and sewer main are at full capacity. The developer will need to extend the network to the Springs Road wastewater main.

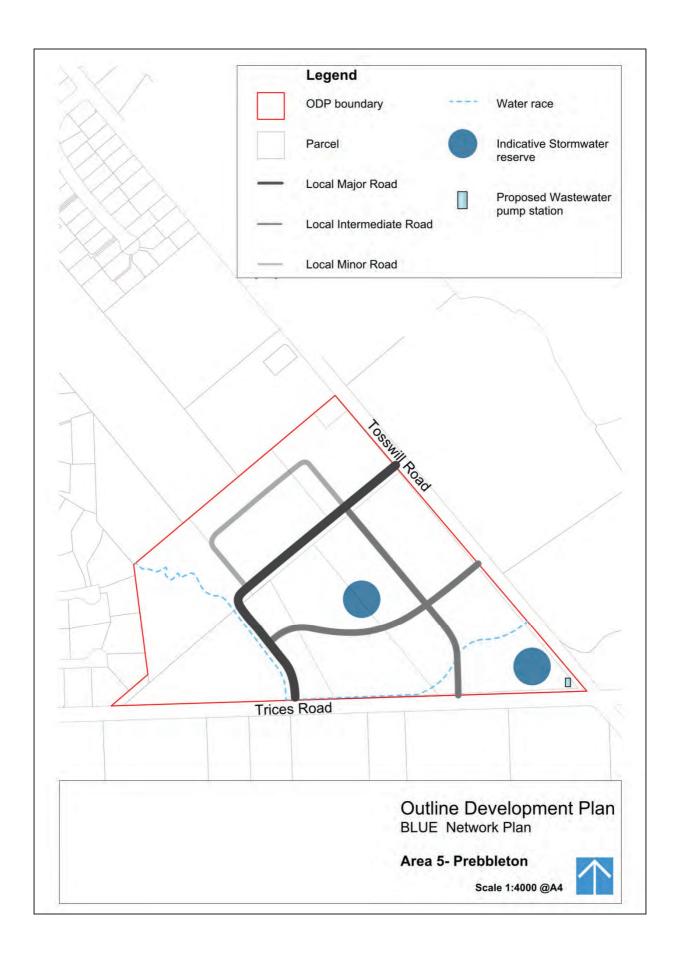
There are a range of methods available to collect, treat and dispose of stormwater. Options include the discharge of hard surface and roof run-off within residential sections to be disposed on site via soakage pits. Stormwater run-off from the road network is to be directed to basins requiring an estimated area of 1 na. The water race should be developed in an integrated manner that accommodates pedestrian and cycling connections within the site and with Prebbleton Domain. The stormwater reserves referenced on the ODP plan are indicative only. Detailed stormwater solutions are to be determined by the developer in collaboration with Council at subdivision stage and in accordance with Environment Canterbury requirements. Stormwater solutions should integrate into both the road and reserve environments where practicable. The establishment of riparian margins and low impact stormwater management techniques are encouraged where appropriate to establish and enhance ecological corridors, habitats and tangata whenua values attributed to the water resource.











Living 1A6 Zone Prebbleton, Outline Development Plan



Planting Requirements for Living 1A6 Zone (Appendix 19) at Prebbleton

Deciduous Trees

Scientific Name	Common Name
Alnus sp. (ie. Cordata, glutinosa)	Alders
Betula sp.	Birches
Castanea sativa	Sweet Chestnut
Carpinus Betulus	European Hornbeam
Fraxinus sp.	Ash
Populus sp.	Poplars
Quercus robur	English Oak
Quercus robur fastigata	Upright Oak

Understorey Planting - Natives

Scientific Name	Common Name	
Coprosma sp.		
Cordyline australis	Ti Kouka / Cabbage Tree	
Corokia sp.	Korokio	
Griselinia littoralis	Kapuka / Broadleaf	
Olearia sp.		
Pittosporum sp.		
Sophora microphylla	Kowhai	

Understorey Planting – Exotics

Scientific Name	Common Name
Acacia (shrub species)	Wattle
Lonicera Nitida	Box Honeysuckle
Photinia sp.	
Viburnum tinus	

OUTLINE DEVELOPMENT AREA 2

INTRODUCTION

This Outline Development Plan (ODP) is for Area 2. The ODP employs sound urban design principles to establish a framework from which future development of the area will be guided. In its formulation, the ODP has considered the provisions of the Selwyn District Plan, the Canterbury Regional Policy Statement, the Lincoln Structure Plan (LSP), and the Ministry for the Environment's Urban Design Protocol.

The ODP has been broken down into four plans. (Density, Movement Network, Green Network and Blue Network)

DENSITY PLAN

The ODP Area shall achieve a minimum net density of 10 households per hectare.

MOVEMENT NETWORK

Overall objectives:

- Design primary roading layout in general accordance with Lincoln Structure Plan.
- Connect Southfield Drive to provide a connection between Lincolndale and Ryelands subdivisions at the western end of ODP Area 2, and Ellesmere Road at the eastern end.
- Provide efficient through-road linkages between properties within ODP Area 2.
- Construct a road bridge across the LII creek to provide the "southern linkage" to Ellesmere Road
- Align main north-south road with adjoining ODP Area 3.

The Road Network Plan is consistent with the roading network in the Lincoln Structure Plan and shows predominantly the Primary Roading layout through ODP Area 2. The location of the secondary road network has already been designed in some parts of ODP Area 2, but not across the entire ODP Area. The location of secondary routes will be determined at a later stage in conjunction with detailed urban design for specific areas.

The primary route layout shows connections to major arterial roads (Edward Street and Ellesmere Road) with intersections, and corner splays in those locations. This includes the bridging across the LII creek which will enable a primary east west route across the area to be created (Southfield Drive to Ellesmere Road). This will connect southern areas of Lincoln township to Ellesmere Road providing another main vehicular route to supplement the existing Edward Street route.

The proposed east west connection will also allow an alternative route for public transport services in a logical manner rather than requiring a focus on Edward Street.

The main north-south primary route through Property 4 has been positioned so that it lines up directly with the main entry road servicing land directly to the north of Edward Street (ODP Area 3).

No roading is shown through Property 3 as this property is only part of the ODP Area 2 for stormwater treatment, retention, and disposal reasons. No residential development will occur on this property and therefore no road connections are required.

The ODP provides efficient and well connected on-road and off road cycle and pedestrian linkages within the site with connections to existing surrounding residential areas such as Lincolndale and Ryelands, together with adjoining ODP areas such as ODP Area 3 to the north. Other characteristics include:

- Provision for the Christchurch to Little River Rail Trail alongside the LII Creek from Edward Street to Moirs Lane within planned esplanade and other reserves. This includes linkages to other internal pedestrian and cycle routes developed in the ODP area and wider connections to the Lincoln University and other southern growth areas of the township via Southfield Drive.
- Providing an integrated network of off street pedestrian and cycling facilities alongside the LI and LII Creeks, and within planned stormwater treatment areas capitalising on any amenity opportunities as may be provided by Council as part of this. On road cycling will be provided on primary roads using cycle lanes where appropriate.

OPEN SPACE NETWORK

Overall objectives:

- Utilise the existing natural features of ODP Area 2 and integrate these features into the Open Space Network.
- Integrate the stormwater management system with the Open Space Network System.
- Provide an Open Space Network centred on the LI and LII creeks, stormwater management area in Property 3, and an additional reserve area in the centre of Property 4.
- Provide a landscape setback and buffer along Ellesmere Road.
- Provide local reserves with in ODP Area 2 within an approximate 400m walking radius.

The esplanade reserve system along the LI and LII creeks will be reinforced by additional open space areas alongside the esplanade reserves. The additional areas will be contributed as part of the reserve contribution required for residential subdivision.

A reserve will be provided in the centre of Property 4. This reserve will be pedestrian accessible from all parts of Property 4 i.e. within a maximum of 380 metres from all parts of Property 4. A walkway system will be provided between the central reserve area on Property 4 and the reserve system along the LII creek.

The stormwater management system treatment system on Property 3 will be integrated into the reserve system and allow public access to the LII creek.

Diversity and adaptability	— Mixes of uses — Flexibility of buildings	To promote choice through the provision of a diverse mix of compatible activities and uses, so built environments can adapt over time. This facilitates the ability to respond efficiently to social, technical and economic changes.
Legibility and identity	— Town form — Visual character	To promote environments that are easily understood by their users, and that display a strong local identity and appropriate visual character. This facilitates an enhanced usage, enjoyment and pride in local places.
Environmental responsiveness	— Ecosystems — Green network — Urban water — Waste — Energy	To promote urban environments that are responsive to natural features, ecosystems, water quality, reduced energy usage and waste production, and balance the spatial needs to achieve this with those required for urbanisation. This facilitates improved ecological outcomes.

DENSITY PLAN

Higher intensity residential development with smaller lots, should be located near proposed open space areas that intersect with primary roads, and in close proximity to walkway / cycleway links to Edward St, which in turn provides access to the town centre. This allows for more compact development in the areas where services and amenities are accessible. Lower intensity with larger lots on the periphery will allow for greater setbacks and landscaping along the northern boundary and the Ellesmere Road and Edward Street boundaries.

More intensive density development concentrated around a central open space network will provide greater amenity and high quality development in these areas.

The neighbourhood centre (new commercial/ retail node) should be located with frontage onto the primary route, and in close proximity to Edward Street to maximise potential patronage from existing surrounding residential areas. A primary school could be located within this ODP, subject to confirmation by the Minister of Education/Ministry of Education and the landowners.

The ODP area shall achieve a minimum net density of 10 households per hectare.

MOVEMENT NETWORK

A legible hierarchy should be created for roads within Area 3, providing integrated connections to adjacent neighbourhoods and future development blocks. The primary routes should be consistent with the Lincoln Structure Plan and support strong connectivity to Edward Street, Birchs Road and Ellesmere Road. The road network should provide efficient and convenient connections between destination points such as community facilities, including the proposed neighbourhood centre and school, and allow for public transport. A logical choice of direct routes should be provided to disperse traffic volumes and reduce travel distances.

Provision of primary and secondary routes will enable safe pedestrian and cycle movements through the site, movement paths for stormwater, and green corridor connections with large centralized areas of open space. Design strategies for these roads should be integrated while minimising conflict between vehicles, pedestrians and cyclists. Primary routes will create a 'spine' for the site with commercial and community areas having direct frontage to these routes.

The north-south oriented primary routes will cross the drainage corridor, while other routes will follow the edges of the centralised reserve areas, providing safe, memorable and legible movement patterns that relate to natural site features and vistas.

All other local streets are not included in the ODP to ensure that the layout remains flexible and responds to built form.

The length, location, geometry, width and overall design of these streets should reduce traffic speeds to allow for safe, pedestrian accessible environments. The patterns created by the streets and open space network should connect neighbourhoods, and in general this will mean that most roads will be through-routes, and any cul-de-sacs should be limited in number and length.

The cycle and pedestrian network has a relationship with the underlying road pattern and open space network. Safe and convenient access for pedestrians and cyclists should be provided for all routes, including footpaths and in some cases cycle lanes. Footpaths should be provided on all roads, and widths may vary depending on the road hierarchy and anticipated use. Barrier free access should be provided along and across all roads for all users including those with mobility restrictions. Along Ellesmere Road and Edward Street additional road reserve may need to be vested in council to accommodate infrastructure positioning such as footpath / cycleway in relation to the appropriate design of these frontages.

Dedicated on-street cycle lanes should be provided for all primary routes. All other streets should be slow speed, low traffic volume environments which are pedestrian and cycle friendly, and create liveable public spaces for the community. In some cases these may be shared surfaces.

Roads edging open spaces should provide safe and direct alternative routes for night use that complement open space movement patterns. These roads also provide passive surveillance and multiple entry and exit points to open space areas. These roads should adequately provide for walking and cycling and have good lighting.

The street pattern should provide direct and convenient access so that a maximum 400m walking distance is provided between all dwellings and open space areas within the site. An off- street cycle path is proposed along Edward Street in accordance with the Lincoln Structure Plan. Other off-street walkways and cycleways are proposed through Browns Lane (which will be deferred until such time as the Duncan Block is developed for residential purposes) and the connected open space network, including the central drainage reserve, as well as along Ellesmere Road. Pedestrian links should be provided from Edward Street and Ellesmere Road to connect to the street network, and pedestrian access should be provided to an existing walkway on the western boundary of the site. Any paths crossing the Liffey Fields stormwater facility should be avoided as far as practicable.

Along the lane from Edward St (Browns Lane) as indicated on the plan fencing is to be restricted to a maximum height of 1m max height for a solid fence, or maximum 1.5m for a semi-transparent fence (i.e. pool style fencing) to provide for surveillance opportunities. Fencing will be along the rear of the adjoining properties so there is a need to retain some flexibility to design for privacy and security in outdoor yards as well as to maintain surveillance.

GREEN NETWORK

The existing natural drainage patterns within the southern portion of the site underpin the connected system of accessible open space. Utilising and enhancing the natural features of the site will contribute to overall character and sense of place, offer more varied recreational experiences, and create a focal point for the community. Reserves should be consolidated around this central core to create large, well proportioned areas for a range of active and passive recreational activities.

In terms of the northern portion of the site, neighbourhood reserves will be developed as part of the overall subdivision design. These will integrate with drainage channels and natural features to provide for a high level of connectivity, recreational value and neighbourhood character.

For the site as a whole, open space areas (including the stormwater reserves) should be accessible from primary and secondary routes within the site, and within a 400m walking distance of all new dwellings. In general, open space areas bounded by roads are more secure because of informal surveillance from passers-by and overlooking from windows and outdoor areas of nearby houses. Reserves that are clearly visible are likely to attract more users and be more valued by the community.

The design of streets will contribute to the overall character and connectedness of the public network of open space. Wide berms with footpaths and avenues of trees can create a boulevard effect to assist with navigation to open space areas. Vistas and key road junctions that terminate with open spaces and landscape features provide opportunities for good user legibility within the site. The junctions between primary routes should also be considered to ensure that long vistas terminate with landmark features such as trees within a landscape setback, and/or landmark buildings.

Integration with the surrounding green network should be provided, including the existing reserve to the west of the site. Appropriate landscape setbacks should be provided along Edward Street and Ellesmere Road for trees and buffer planting, and to facilitate an off-road cycle and pedestrian connection.

BLUE NETWORK

Additional water supplies will be obtained from the installation of new bores. These bores will be located to suit the required pressures and flows. All water mains will follow the road network or pedestrian routes. There are a number of existing bores over the site that could be upgraded to potable standard.

The stormwater network incorporates existing natural drainage patterns that meander east-west through the site, reserves and streets with above- ground stormwater management devices such as swales. Any requirements for stormwater detention and attenuation in the southern catchment should generally be accommodated within the Integrated Stormwater Management Plan (ISMP). The northern catchment is in the Halswell catchment and subject to specific other parameters dictated by Environment Canterbury. The southern catchment discharges to Lake Ellesmere. Stormwater reserves should also be designed and used for recreational purposes such as walking and cycling, and integrated with the open space network. Along with riparian management techniques, these reserves can create valuable ecological corridors and habitats, as well as significant amenity and localized character for the neighbourhood.

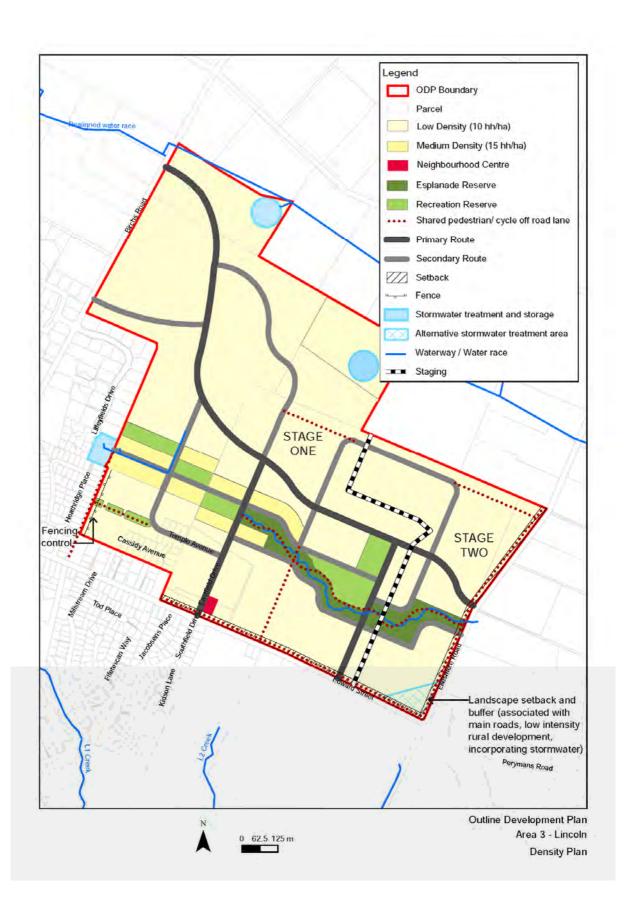
An alternative facility may be available for the southern catchment of Area 3 at the corner of Edward Street and Ellesmere Road. Part of the southern catchment can drain to the Liffey Fields stormwater facility.

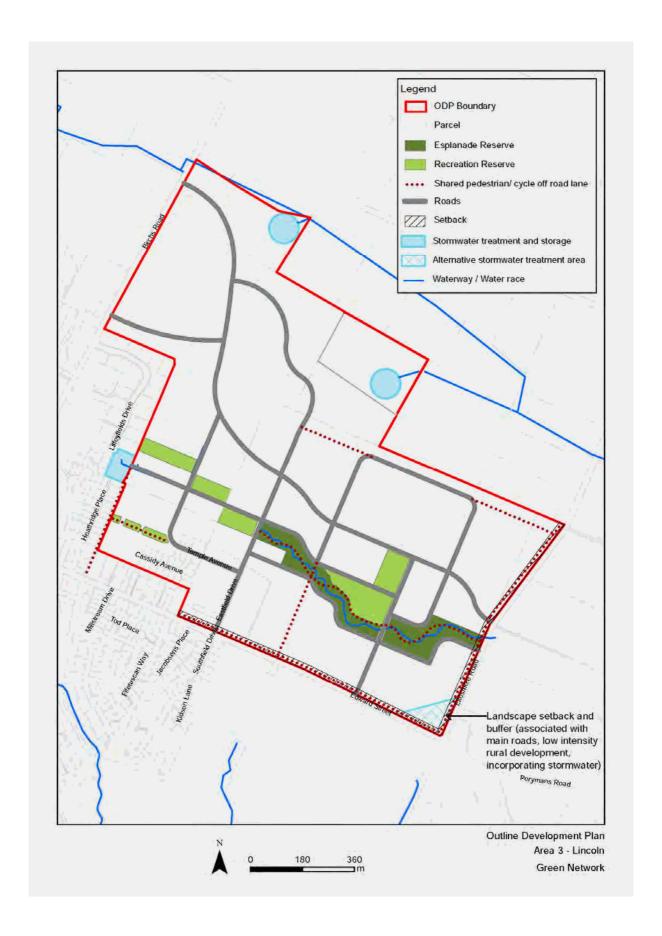
Sewer connection from the southern catchment to the Lincoln Storage Facility will be via the existing sewage pump station in Lincoln Dale.

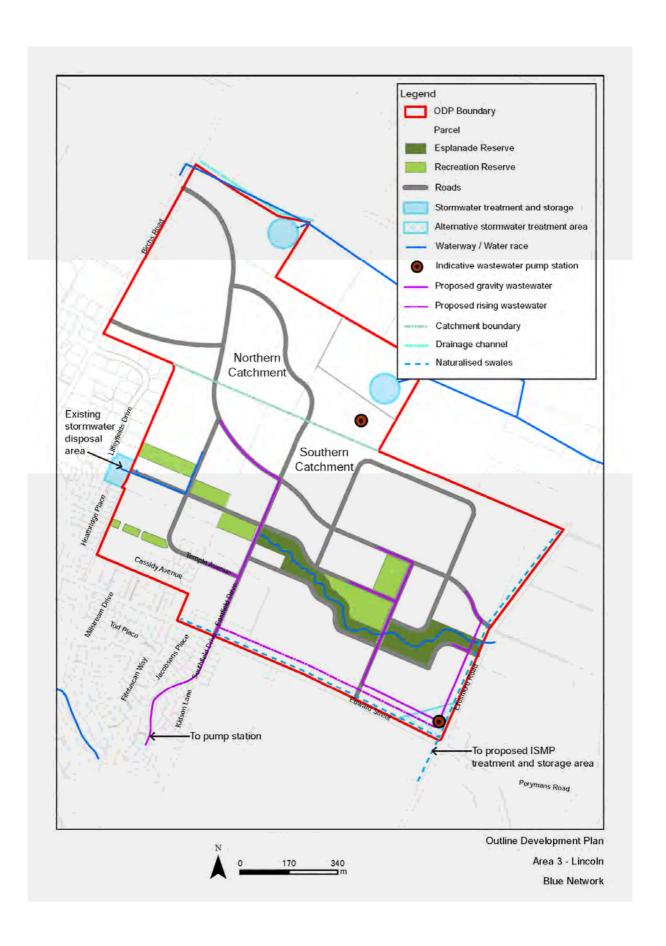
The northern catchment will need to be pumped to the Lincoln storage Facility via proposed pump station (2). The route of the rising sewer from pump station (2) can be agreed at a later date as options exist.

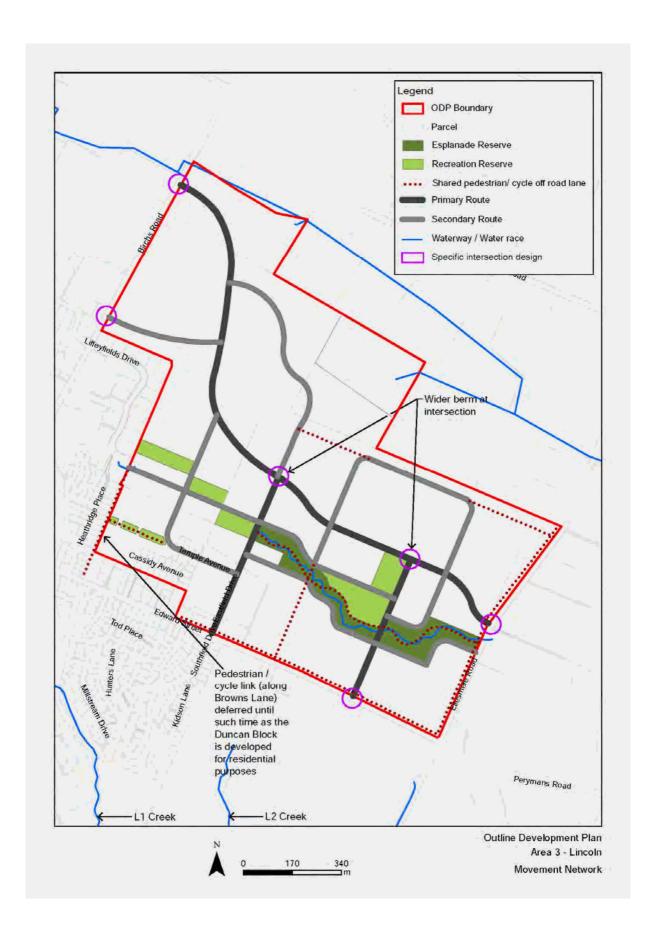
SPECIFIC ODP STANDARDS (REFER TO RULE 12.1.3.38)

 Fencing erected adjacent to Browns Lane (as indicated on ODP Area 3) shall not exceed a height of 1m for a solid fence, or 1.5m for a semi-transparent fence.









OUTLINE DEVELOPMENT PLAN 4

INTRODUCTION

This Outline Development Plan (ODP) is for Area 4. This ODP provides an overarching urban design framework to guide future development of the land. It provides the principles and design intent that will underpin development of the masterplan. Development applications will need to demonstrate consistency with the final ODP that is formulated for the area.

Consistent with the Lincoln Structure Plan, the ODP supports the principles of strong connectivity to Birchs Road an integrated transport walking network, protection of valued drainage paths through the site.

The ODP has been broken down into four plans. (Density, Movement Network, Green Network and Blue Network)

DENSITY PLAN

The Area 4 Outline Development Plan area will be comprised entirely of conventional residential development in accordance with the Lincoln Structure Plan. The ODP area shall achieve a minimum net density of 10 households per hectare. The Northern extent of the Outline Development Plan area adjoining Tancreds Road, will have a 20m Building Setback requirement. The setback will provide a buffer between the residential development and adjoining rural area. This Setback will be extended onto Birchs Road as far as the first entrance into the area.

MOVEMENT NETWORK

A legible hierarchy should be created for roads within Areas 4, providing integrated connections to adjacent neighbourhoods and future development blocks. The primary routes should be consistent with the Lincoln Structure Plan and support strong connectivity to Birchs Road and Northfield Drive extension in Area 3. The road network should provide efficient and convenient connections between residential areas and destinations such as community facilities and neighbourhood centres as well as allowing for public transport. A logical choice of direct routes should be provided to disperse traffic volumes and reduce travel distances.

Provision of primary and secondary routes will enable safe pedestrian and cycle movements through the site, movement paths for stormwater, and green corridor connections that also provide areas of open space. Design strategies for these roads should be integrated while minimising conflict between vehicles, pedestrians and cyclists. The East-West orientated primary route will provide a collector road link across Area 4. It will cross and link with pedestrian, cycleway routes and areas of open space providing legible movement patterns that relate to natural site features and vistas.

The connection to Boundary Road can be routed through the Golf Course utilising unused parts of the Golf Course.

All other local streets are not included in the ODP to ensure that the layout remains flexible and responds to built form.

There will be no direct property access onto Tancreds Road or Birchs Road. A portion of this frontage will also be subject to a 20m building setback as shown on this plan. The length, location, geometry, width and overall design of these streets should reduce traffic speeds to allow for safe, pedestrian accessible environments. The patterns created by the streets and open space network should connect neighbourhoods and in general this will mean that most roads will be through-routes, and any cul-de sacs should be limited in number and length.

The cycle and pedestrian network has a relationship with the underlying road pattern and open space network. Safe and convenient access for pedestrians and cyclists should be provided for all routes, including footpaths and in some cases cycle lanes. Footpaths should be provided on all roads, and widths may vary depending on the road hierarchy and anticipated use. Barrier free access should be provided along and across all roads for all users including those with mobility restrictions.

Dedicated on-street cycle lanes should be provided along the primary route. All other streets should be slow speed, low traffic volume environments which are pedestrian and cycle friendly, and create liveable public spaces for the community. In some cases these may be shared surfaces.

Road edging open spaces should provide safe and direct alternative routes for night use that complement open space movement patterns. These roads also provide passive surveillance and multiple entry and exit points to open space areas. These roads should adequately provide for walking and cycling and have good lighting. The street pattern should provide direct and convenient access so that a maximum 400m walking distance is provided between all dwellings and open space areas within the site.

The open space corridor with direct connections to the Little River Rail Trail on Birchs Road is proposed in accordance with the Lincoln Structure Plan.

GREEN NETWORK

The existing natural drainage patterns underpin the connected system of accessible open space. Utilising and enhancing the natural features of the site will contribute to overall character and sense of place, offer more varied recreational experiences, and create a focal point for the community.

Neighbourhood reserves will be developed as part of the overall subdivision design. These will integrate with drainage channels and natural features to provide for a high level of connectivity, recreational value and neighbourhood character. This will include the naturalisation of the water race which will be incorporated into the reserves and road network. The approximate location of the proposed water race is shown here.

For the site as a whole, open space areas (including the stormwater reserves) should be accessible from primary and secondary routes within the site, and within a 400m walking distance of all new dwellings. In general, open space areas bounded by roads are more secure because of informal surveillance from passers-by and overlooking from windows and outdoor areas of nearly houses. Reserves that are clearly visible are likely to attract more users and be more valued by the community.

The design of some streets will contribute to the overall character and connectedness of the public network of open space. Wide berms with footpaths and avenue of trees can create a boulevard effect to assist with navigation to open space areas. Vistas and key road junctions that terminate with open spaces and landscape features provide opportunities for good user legibility within the site. The junctions between primary routes should also be considered to ensure that long vistas terminate with landmark features such as trees within a landscape setbacks, and/or landmark buildings. Appropriate building setbacks should be provided along Tancreds Road and part of Birchs Road. Provision of a 20 metre building setback and landscaped non-recreational buffer adjacent to 'Smith's Block', with hedging by slow growing, tightly branched evergreen natives, should also be provided to avoid potential reverse sensitivity effects on research activities undertaken on the Smith's Block.

Connection is also to be made to Golf Course and Cemetery.

OUTLINE DEVELOPMENT PLAN AREA 6

INTRODUCTION

This Outline Development Plan (ODP) is for ODP Area 6 at Lincoln. The area incorporates the balance of land known as the "Vege Block" which is not required for Community Purposes as part of the Notice of Requirement. It has an area of approximately 7500m² and covers the eastern half of Lot 1 DP 301682 with the western part of this lot being designated for Community Facilities. The ODP provides a framework to guide future development of the land.

ODP Area 6 is proposed to be developed for medium density housing consistent with the Lincoln Structure Plan and in accordance with the Selwyn District Council Medium Density Housing Guide. The development of this area in combination with the Community Facilities area will provide extensive linkages with existing and proposed Council reserves and enable a future road link with Boundary Road.

The ODP has been broken down into four plans. (Density, Movement Network, Green Network and Blue Network).

DENSITY PLAN

The site is rectangular and is 32m wide by 230m in length. Access to the site will be by a new road off North Belt which will provide access of the western boundary of the site. The layout of the sections will be undertaken to maximise sunlight and warmth. The lot sizes will provide for medium density housing and will be laid out to achieve combined accessways for back sections. The ODP area shall achieve a minimum net density of 15 households per hectare.

The higher intensity residential development incorporating smaller lots will be directly opposite the Lincoln Events Centre and Lincoln Domain. The site is also close to the local primary and high schools and the Lincoln Shopping Centre. The site backs on to Roblyn Place. A 5m building setback, together with a 12m setback for two-storey buildings, is shown adjacent to this ODP boundary, in order to reduce any privacy, shading and outlook effects on the adjoining Roblyn Place residents.

MOVEMENT NETWORK

The new road within the development is effectively an extension of William Street and is required to access the proposed Lincoln Community Centre and Lincoln Domain and their associated carparks. The Lincoln Structure Plan indicates that the linking of William Street through to Boundary Road would provide significant benefits for local area movement rather than having to backtrack to the village centre. This ODP provides for this linkage through the ODP area and with a connection through to the lot to the north, thus providing the potential for a link through to Boundary Road.

The road is proposed to be a primary road with pedestrian and cycle movements on both verges. The road verges will be 5m wide with provision for landscaping, power, telecom, water, sewer and stormwater services to be contained within them.

Accessibility to the Community Centre and Domain carparks via the new road will alleviate congestion problems along North Belt, particularly on winter sports days. Initially the road will be a cul-de-sac until the balance of the road to Boundary Road is constructed.

In addition to the cycle and pedestrian facilities associated with the new road through to the Community Centre, specific cycle and pedestrian routes from the new road directly to the Domain are to be created.

GREEN NETWORK

Due to the proximity of the Lincoln Domain and the Council's development of the community centre adjacent to this site the provision of separate open space is not warranted. The 5 metre wide street berm will provide for frontages to the residential units to be grassed and planted with trees or for a grassed swale to retain natural elements within the site. The existing pond to the north of the site and the detention basin at the southern end of the community centre as well as landscaping of the centre will all provide a natural fringe to the development providing residents with visual relief.

A linkage to the Lincoln High School may be provided at the northern boundary; however the final location of this link will be determined after the development of the tennis and netball courts in the area.

It is also anticipated that an esplanade reserve will be provided adjacent to the Liffey Stream.

BLUE NETWORK

The northern boundary of this site is adjacent to the Liffey Stream close to where the stream commences. As surface drainage in the area is poor, a comprehensive drainage system for all the land being developed is necessary to provide appropriate treatment and detention of stormwater. The proposed stormwater management system for the area is:

Road, community centre and car parking hardstand areas – capturing rainwater and directing it through swales in the car park for treatment and then into a detention basin prior to entering the Council reticulated stormwater system.

RESIDENTIAL SUBDIVISION - THREE OPTIONS ARE AVAILABLE NAMELY:

- (a) direct stormwater through piped system to a swale at the southern section of the road berm:
- (b) direct stormwater through a piped system to a swale within a section of the road at the southern end of the site; or
- (c) direct stormwater through an open swale within the road berm for the full length of the road.

Sewer connection from the medium density development will be to the Lincoln Storage Facility via the existing reticulated sewerage network.

SPECIFIC ODP STANDARDS (REFER TO RULE 12.1.3.38)

- A 5m setback shall be provided between all buildings and the eastern boundary of ODP Area 6.
- A 12m setback shall be provided between all two-storey buildings and the eastern boundary of ODP Area 6

OUTLINE DEVELOPMENT PLAN AREA 2

INTRODUCTION

This Outline Development Plan (ODP) applies to that area of land in Rolleston which forms a triangle between Norman Kirk Way to the North, Rolleston Drive to the East and Markham Way to the South. The ODP of Area 2 provides a degree of certainty to all parties in the establishment of residential land uses across the site. The key elements of the ODP include the roading network, utilities and densities.

The proposal is consistent with the Greater Christchurch Urban Development Strategy and Proposed Change 1 to the Regional Policy Statement and provides an opportunity for residential infill in proximity to the Rolleston Township and its community facilities. The Area 2 ODP meets the objectives and policies of strategic planning documents and is in keeping with the Rolleston Structure Plan, which applies to this area.

The ODP has been broken down into two plans. (Density and Movement Network.)

URBAN DESIGN

The applied design principles seek to provide good Urban Design solutions with the integration of new housing into established neighbourhoods. This infill development is a way of enabling a choice of housing styles and a mixture of densities. The proposal promotes the following environmental objectives and visions:

- Creating a mixture of residential housing from medium to high density (in the form of comprehensive housing) as and where appropriate;
- The development meets District Plan policies to achieve an overall increase in residential density using infill as a sustainable method to create a density of 20hh per ha on existing residential zoned land;;
- Proximity to Rolleston Town Centre and its community facilities (shops) enables pedestrian and cycling activities and encourages walkable/cycle able neighbourhoods.

DENSITY PLAN

This ODP provides for the two residential uses 'Comprehensive' and 'Medium density' development. Both have been identified as appropriate land uses due to location and character of the site and its surrounds. The ODP area shall achieve a net density of 20 households per hectare. Sections with a minimum of $450m^2$ and single storey houses are required on the boundary with sections along Markham Way (shown as A on the ODP). A 5m setback from this boundary minimises any potential privacy issues between established and new development. All development has to be in accordance with the Selwyn District Medium Density Guidelines. Height, setback and bulk and location elements need to be considered at a subdivision stage.

MOVEMENT NETWORK

The majority of sites in this development will be accessed off an extension of Markham Way. This extension will most likely to be a cul-de-sac that is connected to Rolleston Drive via a Pedestrian/Cycling link. A second pedestrian/cycle link will create a connection between the new residential housing, Rolleston School and the Council Headquarters. Limited access for up to 6 private vehicle access ways is proposed off Rolleston Drive. The access off Rolleston Drive is limited due to its classification and function as a Collector Road.

The intensification of the site is in accordance with the Rolleston Structure Plan and the minor increase in traffic volumes are not expected to give rise to any safety issues, or on the efficient operation of the road network.

GREEN NETWORK (no ODP plan)

Due to the close distance to Rolleston Reserve, no open space is required for this ODP.

BLUE NETWORK (no ODP plan)

All services, including water, sewer, power, and phone are available to the site via the existing road network. Connection to the Sewer lateral will be supplied in the roading network in Dryden Avenue.

Existing servicing will be sufficient to service 20 households per hectare.

There is sufficient electricity and phone capacity to reticulate the proposed site.

Stormwater will be treated in a variety of ways depending on its origin within the development. Runoff from house roofs will be disposed on site via soakage pits. Stormwater runoff from public road and private access ways will be disposed into the Council system via kerb and channel.

SPECIFIC ODP STANDARDS (Refer to Rule 12.1.3.38)

- Single storey houses shall be erected adjacent to the southwest the boundary of ODP Area 2 with a minimum section size of 450m².
- A 5 m setback shall be provided between single storey houses and the southwest boundary of ODP Area 2.

OUTLINE DEVELOPMENT PLAN AREA 3

INTRODUCTION

This Outline Development Plan (ODP) is for the SR3 area as identified in Rolleston Structure Plan. It provides the design principles that will underpin development of the area. Development applications will need to demonstrate consistency with the final ODP that is formulated for the area.

The design principles that underpin this ODP are in line with Policy 7 (Development Form and Design and Policy 8 (Outline Development Plans and Changes of Zoning in District Plan) of Plan Change1 to the Regional Policy Statement and the Development Principles of the Rolleston Structure Plan. Subdivision development of SR3 will need to provide a contextual analysis of this area as outlined in Section 3 of the SDC Subdivision Design Guide.

The ODP has been broken down into four plans. (Density, Movement Network, Green Network and Blue Network).

DENSITY PLAN

The ODP area shall achieve a minimum net density of 10 households per hectare (with a minimum of 484 households). Lots along the rural periphery and Levi Road have an area greater than 1,000m² and in total, the average lot size shown on the ODP is 750m².

MOVEMENT NETWORK

A legible road hierarchy providing integrated connections throughout SR3 and to existing subdivisions in Rolleston is to be achieved, as follows.

Primary access to SR3 from Levi Road through planted wide "Avenues". The avenues provide a connection between the local roads and main collector/arterial roads, eg Levi Road. The proposed treatment of these avenues will accommodate swales and tree planting in a central reserve and generous berms to provide an attractive landscaped entry to the area.

"Local Area Streets" will structure the subdivision area and connect it to the adjacent residential area to the west. These roads are able to accommodate on street car parking and tree planting on both sides. The carriageway width is sufficient to accommodate swales where required and a bus route.

"Neighbourhood Streets" provide access to the remaining pattern of neighbourhood and residents streets. In addition, where necessary, private right of ways are used to access up to 6 lots.

A potential bus route can be accommodated along the large local roads and connect through to the adjacent residential subdivisions. The vast majority of the proposed residential lots would be within 200m of a potential bus route. Dedicated cycleways have been included along the avenues and cyclists can be accommodated within the carriageways of other large local and local roads. The principle of walkability has been incorporated through the use of a connected road pattern, additional pedestrian links and the location of open spaces.

GREEN NETWORK

A number of neighbourhood parks are included within SR3. These are located to ensure open space is provided irrespective of the sequencing of development and the pattern of land ownership. All properties within SR3 will be within five minutes walk or 400m radius of a neighbourhood park.

The neighbourhood parks are located such that pedestrian links can be made to the potential future greenbelt along the eastern side of SR3 and/or the surrounding rural area. These neighbourhood parks measure just over 2000m2 each in area, are overlooked by adjacent housing and are appropriate for children's playgrounds and passive recreation.

The location of the reserves is such that they are surrounded by active fronts, namely the front boundaries of residential properties. This ensures passive surveillance and increased security.

BLUE NETWORK

Water connections will be supplied through the existing water supply in Rolleston. All water mains will follow the road network or pedestrian routes. Within SR3, stormwater is to be accommodated through a combination of swales and below ground management and treatment devices, accommodated within the road reserves.

Sewer connections will require an extension of the existing sewer main in Levi Road into SR3. This will enable sewage for these properties to connect in to the existing sewer network via gravity. A new pump station will be required to collect the follow and pump it into the existing sewer network.

OUTLINE DEVELOPMENT PLAN AREA 6

INTRODUCTION

This Outline Development Plan (ODP) is for Development Area 6. Area 6 comprises 83ha and is bound by Dynes Road to the North, Goulds and East Maddisons Roads to the West and areas recognised by Council as future residential growth areas to the south and east.

The ODP embodies a development framework and utilises design concepts that are in accordance with:

- Policy B4.3.7 and B4.3.68 of PC7
- Canterbury Regional Policy Statement
- The Rolleston Structure Plan
- The Greater Christchurch Urban Development Strategy (UDS)
- The Ministry for the Environment's Urban Design Protocol

The ODP has been broken down into four plans. (Density, Movement Network, Green Network and Blue Network).

DENSITY PLAN

The ODP provides for a variety of allotment sizes from density residential areas of 15 to 20 households/ha through to larger 'standard' residential properties. Generally the density is higher within the central areas of the ODP around key amenity areas and decreases towards the periphery. The highest density housing is to be located in direct proximity to the proposed neighbourhood centre and larger open spaces such as green corridors and neighbourhood parks. The ODP Area shall achieve a minimum net density of 12 households/ per hectare. Given the mix of densities and the likelihood of a staged approach to development, the 12 households per hectare minimum density need not apply to each individual stage. However at the time of subdivision of each stage, assessment and confirmation as to how the minimum density of 12 households per hectare for the overall ODP can be achieved will be required.

A neighbourhood centre will be located in the centre of the ODP adjacent to the central reserve area where the green corridors converge. The Centre will make provision for approximately 1000m2 gross retail floor area to cater to local weekly and day-to-day retail requirements, though other non-retail uses (such as other business and/or community facilities) may also be appropriately located in the Centre. Additional land is also required for surrounding roads, associated car parking and landscaping to service the eventual activities established in the Centre.

MOVEMENT NETWORK

The ODP provides for a range of transport options, including:

- vehicular connections linking to Goulds Road, East Maddisons Road, Dynes Road and Springston-Rolleston Road;
- active transport connections at the site boundaries to adjacent areas and internal pedestrian and cycle corridors to encourage viable alternatives to individual motor vehicles; and
- roads which will enable the provision of public transport routes through the site should such services be deemed appropriate;

Roading connections have been designed so as to balance the permeability of the site against the requirements to minimise the number of new intersections and maintain appropriate intersection spacing. The ODP employs a roading hierarchy that allows for Primary, Secondary and Tertiary Roads; however only the more significant roads (Primary and Secondary) have been shown on the ODP. As the Tertiary Roads' primary function will be to service the residential areas, their eventual layout will respond to the detailed subdivision design of those areas.

Goulds Road and East Maddisons Road provide primary road corridors on the Area's western periphery. Internally, the site contains one primary collector road which links the Neighbourhood Centres envisaged by the Rolleston Structure Plan at East Maddisons and Springston-Rolleston Roads. Though the collector is envisaged to cater for a large portion of through vehicle movements, it is not a high-speed corridor and rather, should provide direct access to adjoining sites. To this end, it is envisaged that the collector will interact with the adjacent neighbourhoods, rather than creating severance between them.

The proposed north-south and east-west secondary road connections perform similar functions to the primary roads, providing ample access throughout the site as well as good external links and connections to the immediate neighbours. Notwithstanding this similarity in function, secondary roads will assume a form which is of a more residential nature, and cater less to through vehicle traffic. In addition to the proposed internal secondary routes, Dynes Road will perform a secondary function along the site's northern boundary.

An integrated network of tertiary roads will facilitate internal distribution of traffic, provide access to properties, connect open spaces within the site and offer future links to the immediate neighbours. The tertiary roads will provide a narrower carriageway to encourage slower speeds and to maintain a residential character.

The overall aim of the pedestrian and cycle network is to encourage active transport use within the site and to enable good connections to the wider Rolleston area. Primary and Secondary Roads will provide footpaths and cycle routes, including designated cycle lanes where appropriate. Tertiary Roads will also provide adequate space for cyclists and convenient pedestrian movements. Shared off-road pedestrian and cycle connections will be provided to achieve safe, attractive active transport corridors and recreational amenity.

GREEN NETWORK

The ODP reflects and adds to the green network anticipated in the Rolleston Structure Plan. A range of reserve types and sizes are proposed to create a connected open space network, whereby parks will be linked via green corridors formed along existing stockwater races and along new roading corridors.

Incorporating new structural vegetation, stormwater swales and 'green' streets, four green corridors and green links will contribute generously to the area's sense of place and overall amenity. The green corridors, as well as the proposed neighbourhood parks, are located immediately adjacent to higher density residences to compensate for reduced private open space on individual allotments. In addition, co-locating green spaces alongside residential sites will allow passive surveillance for enhanced safety and security.

Neighbourhood parks will be located to ensure that an open space is within 400m walking distance from any new dwelling. The parks will be of varying sizes to reflect both their purpose and their neighbourhood's character and will include both active and passive functions.

Portions of the green network will also perform a stormwater conveyance and treatment function, particularly for runoff from roads and hardstand in public spaces. Opportunities for pedestrian and cycle paths will also be realised in the design of future reserves and green links to ensure a high level of connectivity is afforded to residents and visitors, and to maximise the utility of the public space.

Council's open space requirements cited in the LTP and Activity Management Plans should be referred to during subdivision design. Where any additional reserve areas may be proposed at subdivision stage over and above the requirements of Council, alternative arrangements may be made for any additional open space land that the Council does not take towards reserve contributions. These arrangements may include purchase by the Council with a corresponding targeted rate, gifting the land to the Council, or other mechanisms as may be agreed at subdivision stage.

No specific mitigation treatment is required for Area 6 to address potential reverse sensitivity effects. Adjoining land uses to the south and east are not of a nature that would require specific effects management at the rural/residential interface over and above the decreased residential density in this portion of the ODP Area.

Moreover, as these adjoining areas are anticipated by Council strategic planning policy for future urban use, ODP Area 6 makes provision to integrate with them, rather than become isolated from them.

BLUE NETWORK

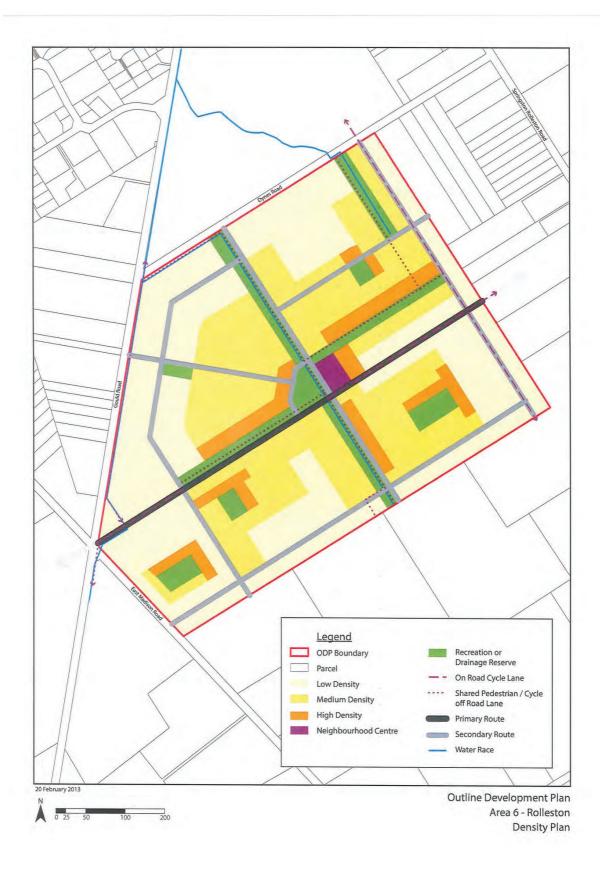
The underlying soils are relatively free-draining (mostly gravels) and infiltration to ground is generally the most appropriate means of stormwater disposal. The public stormwater system will primarily only be required for runoff generated from within the road reserve, as individual buildings will be able to dispose roof water directly to ground within private properties. Where there is potential for the stormwater to be contaminated (e.g. road runoff), treatment will be incorporated in the stormwater system prior to disposal.

There are a range of options available for the collection, treatment and disposal of stormwater. Final treatment solutions will be determined through detailed site investigations during subdivision stages; however, common themes will be achieved throughout the area, including that systems will be designed to integrate into both the roadway and reserve environments. The ability to add amenity value, beyond a pure stormwater function, will also be key component to the overall design.

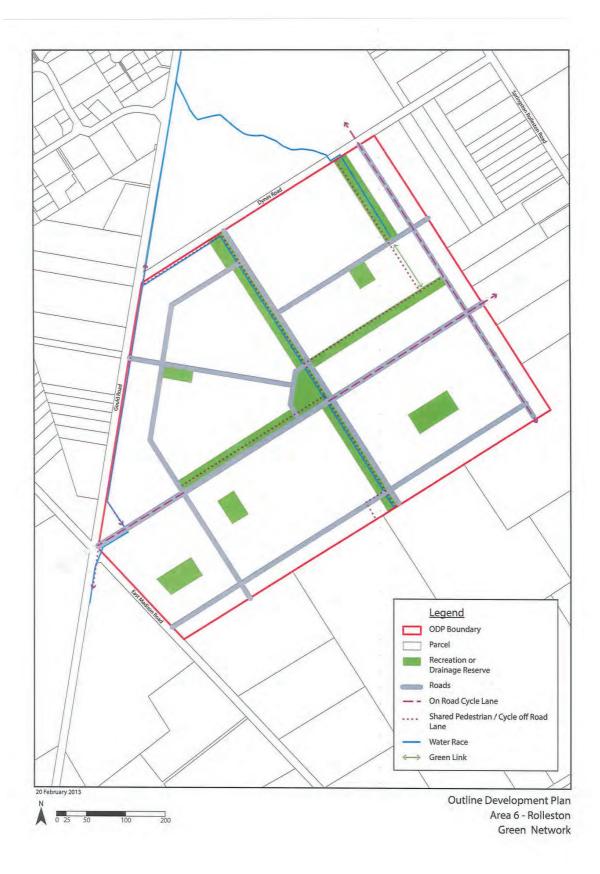
The stormwater conveyance and disposal systems proposed for this site will be consistent with other effective stormwater systems commonly used within Rolleston. The relatively flat nature of the site necessitates the management of stormwater treatment and disposal to be undertaken as a number of discrete catchments, rather than conveying stormwater from the entire site to one central treatment location. However, to minimise on-going maintenance costs and maximise integration potential, the number of these treatment areas will be minimised and main stormwater treatment areas will be located within the major open spaces.

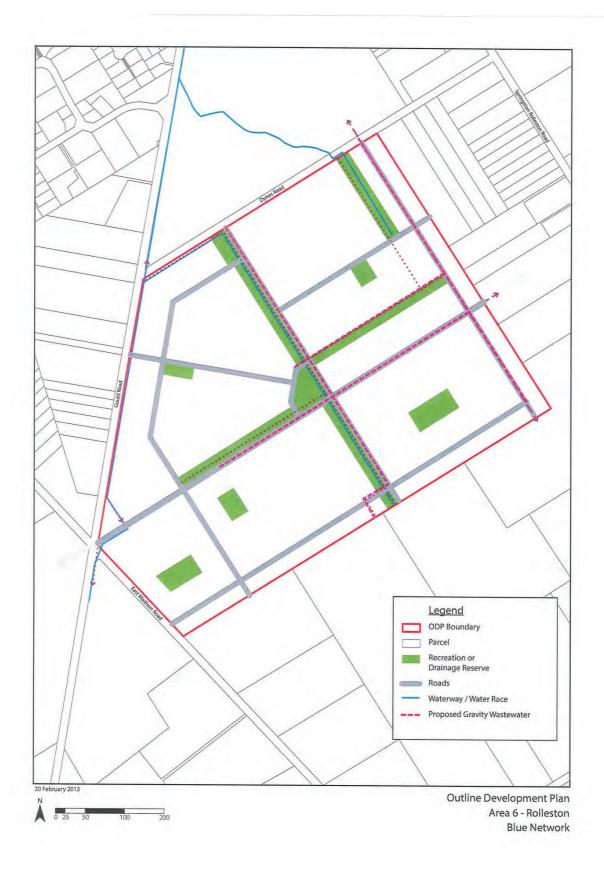
Ground levels slope naturally to the south, making a primarily gravity wastewater network entirely feasible. The area immediately adjacent to Dynes/Goulds Road intersection is currently able to connect, via gravity, to the existing pump station on Goulds Road. However the vast majority of the ODP Area will need to gravitate to the new East Selwyn Sewer Scheme pump station on Selwyn Road. This will be achieved via a route extending south of the ODP area to Selwyn Road and then east to connect with the East Selwyn Sewer Scheme pump station on Selwyn Road.

The water reticulation will be an extension of the existing water supply in Rolleston. Selwyn District Council currently has plans to commission additional water bores, to match the expected increase in water demand from the identified growth areas. If required, an additional bore field could be incorporated within the development of this site to assist servicing requirements for the southern development areas.









INTRODUCTION

The Outline Development Plan (ODP) is for an area of land located in Rolleston situated at the intersection of SH1 and Rolleston Drive with the only access being from dual entry points on Overbury Crescent.

This ODP (known as Park Grove Mews) uses urban design principles to set the general patterns of development over the area to guide future development and provide a degree of certainty for all parties in the establishment of land uses across the site. It provides a design rationale for the key structure elements including the roading network, cycle and pedestrian network, and access to open space of the area.

Consistent with the Greater Christchurch Urban Development Strategy and Proposed Change 1 to the Regional Policy Statement, the ODP provides an opportunity for the creation of an integrated high density development which meets the objectives of those planning documents as well as being in keeping with the strategy set out in the Rolleston Structure Plan.

The ODP has been broken down into three plans. (Density, Movement Network and Green Network).

URBAN DESIGN

Design principles that underpin this ODP are in line in the New Zealand Urban Design Protocol and promote the following environmental outcomes:

- An urban form which creates a distinguishable sense of place and encourages a community to develop.
- A safe, comfortable and healthy living environment (CPTED Crime Prevention through Environmental Design - principles applied).
- Integration of the roads within the neighbourhood area with existing state highway and arterial roads and public transport routes.
- Installation of all the necessary services within the zone, and the connection of those services to all external infrastructure networks.
- Provision of a network of cycle and pedestrian routes.
- Access to an existing recreational reserve for recreational and amenity purposes (located around Dryden Avenue and McCauley Street).
- Opportunity for high density buildings which relate well to each other and are strategically located in relation to open space and amenities.
- Opportunities for a higher density residential development which will provide a use not currently provided for in the immediate surrounding area and offer a living opportunity from difference economic and social groups of future residents.
- Master Planned development integrating house design, colour palette, façade, and front yard landscaping integrated with the street scene.
- A development that meets District Plan policies to achieve an overall increase in residential density, urban consolidation and a compact urban form.

Protection of groundwater resources from contamination.

DENSITY PLAN

The ODP provides for a Master Planned high density development with individual allotments averaging approximately 400 m² per lot.

Each individual site will have a house specifically designed which will form part of the Resource Consent process to enable the site to comply with all required set backs, recession planes and site coverage provided in the District Plan. The ODP area shall achieve a net density of 19.19 households per hectare on a total area of 3.596 ha.

MOVEMENT NETWORK

A specific transport assessment completed by Traffic Design Group Ltd (TDG) has been carried out, which describes the existing transport environment, provides an assessment of the traffic generation associated with the proposed ODP, considers the development in terms of the local, regional and national planning documents, and assesses the potential traffic effects. The assessment also takes into account the Christchurch Rolleston and Environs Transportation Study (CRETS) formalised within the Canterbury Transportation Regional Implementation Plan (CTRIP).

Access into the site is from two existing frontages from Overbury Crescent. The internal road links will use these frontages as the entry points to create the primary spinal traffic road. This primary road will give direct access to the property at the north eastern and south western sections of the property.

A secondary "Living Street" services the majority of the lots to the northern part of the property and will provide for slow two way traffic with a higher level of landscaping linking the street landscaping with that associated with the front yard of individual sites. A pedestrian linkage will be created between the primary and secondary road.

Noise attenuation has been achieved with an established bund along the SH1 boundary which complies with the relevant noise control rules, with any houses constructed within a 40m set back of the highway being required to have noise attenuation.

Pedestrian footpaths will be provided on at least one side of each internal road with a linkage incorporated between the main internal road and the secondary road.

A low speed traffic environment will create pedestrian cycle public friendly spaces.

All lots as a result of the cycle and pedestrian network will have direct access to the existing adjacent reserve to the west linking through to Rolleston Drive.

GREEN NETWORK

The south western access is adjacent to an existing reserve with which the property has a common legal boundary. This reserve also links through to a main open space area (containing a playground) around Dryden Avenue and McCauley Street.

BLUE NETWORK (no ODP plan)

All services including water, sewer, power, and telephone are available to the boundary of the site via the existing roading network. These will be extended into the site via the proposed roading network to provide those services to all individual allotments.

INTRODUCTION

The Outline Development Plan (ODP) is for an area of land located in Rolleston currently being serviced by a current Right of Way known as Park Lane which has access from SH1.

This ODP (known as Park Lane) and uses urban design principles to set the general patterns of development over the area to guide future development and provide a degree of certainty for all parties in the establishment of land uses across the site. It provides a design rationale for the key structure elements including the roading network, cycle and pedestrian network, and access to open space of the area.

Consistent with the Greater Christchurch Urban Development Strategy and Proposed Change 1 to the Regional Policy Statement, the ODP provides an opportunity for the creation of an integrated development which meets the objectives of those planning documents as well as being in keeping with the strategy set out in the Rolleston Structure Plan.

The ODP has been broken down into four plans. (Density, Movement Network, Green Network and Blue Network).

URBAN DESIGN

Design principles that underpin this ODP are in line in the New Zealand Design Protocol and promote the following environmental outcomes:

- An urban form which creates a distinguishable sense of place and encourages a community to develop.
- A safe, comfortable and healthy living environment (CPTED Crime Prevention through Environmental Design - principles applied).
- Integration of the roads within the neighbourhood area with existing state highway and arterial roads and public transport routes.
- Installation of all the necessary services within the zone, and the connection of those services to all external infrastructure networks.
- Provision of a network of cycle and pedestrian routes.
- Access to the existing recreational reserve and provision for recreational and amenity purposes.
- Opportunities for the provision of residential development at a density of a minimum of 10 lots per hectare for which there is current strong demand in the general Rolleston area.
- A Master Planned residential development integrating the roading network with the existing environment and connecting with proposed sub-divisions linking roading, pedestrian, and walking access and linkages to the main local arterial roads.
- A development that meets District Plan policies to achieve an overall increase in residential density, urban consolidation and a compact urban form.
- Protection of groundwater resources from contamination.

DENSITY PLAN

The ODP area shall achieve a minimum of 10 household lots per hectare. The density plan has been calculated taking into account the multiple ownership that currently exists throughout the total ODP area.

MOVEMENT NETWORK

The ODP provides numerous transport options including:

- Restricting access to SH1 as required by NZTA.
- Linkage from Park Lane through to Beaumont Drive through to Levi Road to link with the arterial roads into and out of Rolleston.
- Provide secondary roads that link to the adjacent rezoned land to the east as zoned on ODP Area 3.
- Linkage integral to provide pedestrian, cycle and green linkages within the multiple ownership.

The ODP sets out the roading pattern taking into account the development in terms of the local, regional and national planning documents and assesses the potential traffic effects. The network takes into account the Christchurch Rolleston Environs Transportation Study (CRETS) formalised with the Canterbury Transportation Implementation Plan (CTIP).

The roading network is also in accordance with existing Resource Consents issued by the Selwyn District Council and provides for no alteration to the current traffic network as approved by the Council, other than a minor addition in the north eastern corner of the property.

Access to the site will be from the currently formed and legalised Marlowe Road to the north through Park Lane to Beaumont Drive and Shadbolt Lane to the south. Access to SH1 will be formally stopped both legally and physically once the extension of Marlowe Road is formed, the latter being required to be constructed firstly to enable the balance of the land in the ODP area to be sub-divided.

The roading network provides three connections into ODP Area 3. The secondary and tertiary roading connections are also shown on the ODP.

Legalised pedestrian linkages are shown to coincide with the ODP for ODP Area 3 noting that the location of the pedestrian linkage will be site specific once Resource Consent applications are made for land in and around this area. Servicing the areas of land in between the neighbours and local area streets will be undertaken by small neighbourhood roads or cul-de-sacs complying with the Council's sub-divisional design rules for such roads.

Noise attenuation will be achieved by extending the bund adjacent to the present Park Lane entrance to SH1 to the eastern boundary of the property outlined in blue on the ODP. Landscaping of the bund will be consistent with that already planted and established with the balance of the bund from this point up into Rolleston Drive.

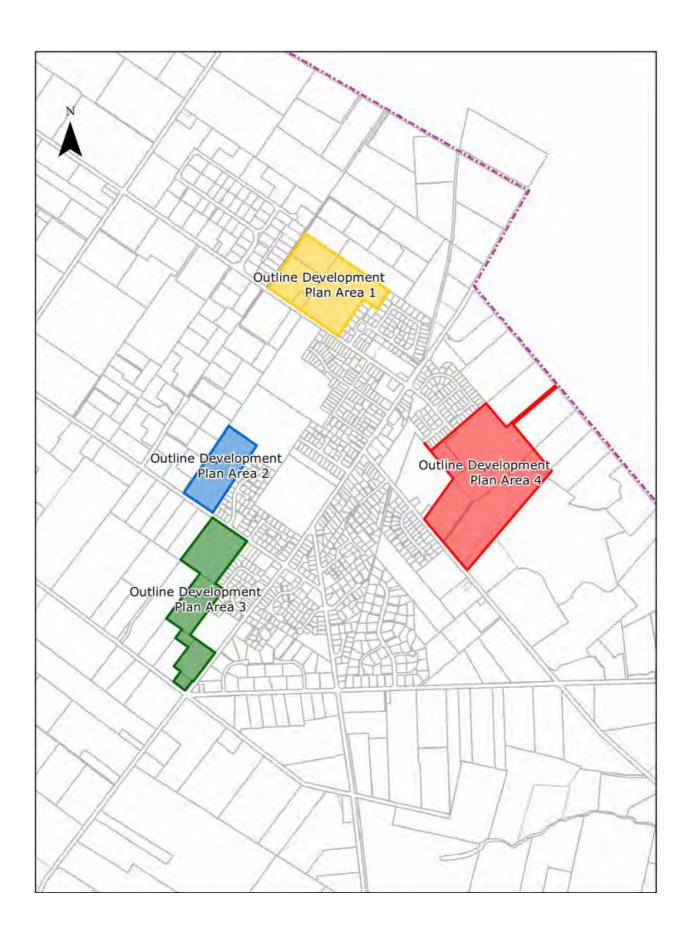
There will be no dwelling erected in a 40 metre set back from the edge of the present seal on SH1.

Pedestrian footpaths will be provided on at least one side of each neighbourhood and local street. The road designed will provide for a cycleway on to the main neighbourhood and local roads as shown on the ODP.

APPENDIX 42

OUTLINE DEVELOPMENT PLAN — LIVING Z ZONE, PREBBLETON

- for locations of these ODP's see overleaf
- ODP Area 1
- ODP Area 2
- ODP Area 3
- ODP Area 4



INTRODUCTION

This Outline Development Plan (ODP) is for Area 1, which is zoned Living Z. Area 1 includes 13.3ha of land, comprising four properties. Three of these properties front onto Blakes Road. The ODP area is contained by The Paddocks development to the north, Aberdeen Drive to the west and Elmwood Drive to the east.

The ODP provides an overarching urban design framework to guide the future development of the land. The ODP includes Land Use, Movement, Green and Blue Networks and incorporates the wider strategic and community outcomes expressed in the Prebbleton Structure Plan.

URBAN DESIGN

The design principles that underpin this ODP are in line with the New Zealand Urban Design Protocol and accord with the Selwyn District Council Subdivision Design Guide (September 2009). The following environmental outcomes are to be achieved:

- Development that meets the District Plan policies, realises an overall increase in residential density, applies urban consolidation principles and assists in achieving a compact concentric settlement pattern for Prebbleton.
- Provision for a range of section sizes and housing typologies to respond to the wider needs of the community, whilst achieving the prescribed minimum household densities and minimum average allotment sizes.
- Subdivision layouts that integrate with adjoining neighbourhoods and incorporate existing land uses where appropriate. The wider context of the development area should influence the subdivision layout by protecting and enhancing cultural, ecological, heritage and Te Taumutu values and existing built features, such as amenity trees and water races.
- Layouts and urban design treatments that create a distinguishable sense of place, assist in enhancing the wider character and amenity of Prebbleton and deliver safe, vibrant and healthy living environments. Layouts should apply Crime Prevention through Environmental Design (CPTED) design principles.
- Integrated and legible road hierarchy that supports safe and efficient connections and promotes walking and cycling. Road design and landscape treatments should contribute to the overall character of Prebbleton and assist in connecting residential development with open space reserves and other public assets and services within the township, such as the Domain, Primary School, Nature Park and the town centre.
- Sustainable methods to treat and dispose of stormwater that protect groundwater resources and surface waterways (including springs, water/stock races) from contamination, while integrating with open space and reserves where appropriate.

Installation of all the necessary infrastructure services within the ODP area, and the cost effective and efficient connection of those services to the wider network.

LAND USE

The ODP Area shall achieve a minimum net density of 10 households per hectare. Lower density allotments are necessary on the majority of the western boundary and the full length of the northern boundary to integrate the site with the adjoining Living 2A (Blakes Road) and Living 2A (The Paddocks) zones, being the Aberdeen and The Paddocks subdivisions respectively. Smaller sections are best established within the centre of the site to reduce the risk of adverse amenity effects on the adjoining Living 1A4 zone to the east and Living 1A6 zone to the south.

Dwellings must front Blakes Road to enhance passive surveillance and safety, while creating a high amenity streetscape. Appropriate design layouts should take into consideration the shape, orientation and aspect of sections, with internal roads supporting access that avoids housing from backing onto Blakes Road.

Residential housing established along the Local Minor Road to the north-east of the reserve must front the reserve. Appropriate interface treatments must promote passive surveillance, support front yards facing towards the reserve and avoid a streetscape that is comprised of tall fencing or solid screening. Other suitable methods, such as fencing controls and set backs, should be formalised at subdivision stage to ensure that all future residential development that overlooks the reserve optimises the high amenity and outlook provided by this open space.

MOVEMENT NETWORK

The proposed roading network consists of a Local Major Road that forms a loop around the development block, supported by Local Intermediate and Local Minor Roads, to achieve an integrated and legible internal road layout. The ODP indicates that the Local Major Road connects to Blakes Road at two points, with the south-eastern connection aligning with the access point being established in the Living 1A6 zone on the opposite side of Blakes Road to form a crossroad.

The Local Minor Road that links between the Local Major Road and Local Intermediate Road shall align with the existing grouping of high quality amenity trees. These trees must be incorporated into the road reserve as an amenity feature. Retaining the trees preserves a link to the previous land use activity and protects high quality specimens. It also contributes to the character of Prebbleton. This grouping contains 7 specimens that are suitable to be retained as street trees within the road reserve, and includes specifically a row of six oak/Quercus that follow a north-south alignment and a single oak/Quercus to the south-east of this row of oaks.

A Local Minor Road situated north of the proposed reserve will provide access to adjacent residential sites. This will ensure that buildings front the road and overlook the reserve. The ODP also supports the establishment of a connection between the development block and the Living 2A zone to the north to support connectivity and to ensure this property does not become land locked.

Two indicative connections are also identified from the Local Major Road into the northern portion of the development block to achieve an integrated internal roading network. Confirmation will need to be established at subdivision that these through connections avoid conflict with the existing dwelling established within this property.

The remaining internal roading layout must be able to respond to the possibility that the area may be developed progressively over time. Road alignments must be arranged in such a way that long term inter-connectivity is achieved once the block is fully developed.

GREEN NETWORK

The ODP indicates a single open space reserve approximately 3,000m2 in size. The reserve is located centrally along the Local Major Road. This location has been identified to optimise a portion of the existing walnut grove to secure a unique amenity feature, while preserving an historic reference to the previous use of the site. The orientation of this reserve supports a high amenity entrance into the site when viewed from Blakes Road.

A portion of the existing walnut/*Juglans* grove is to be retained as part of the reserve. These trees present an opportunity to retain a link to the land holdings past and to create a public space containing a relatively unique amenity feature. This walnut grove is contained within the legal boundaries of 36 Blakes Road (Lot 2 DP 54834).

Further investigations shall be undertaken at subdivision to determine the practicalities of retaining the following existing specimen trees within any future layout:

- Two sycamore/Acer pseudoplatanus and an established walnut/Juglans tree on the frontage of Blakes Road located within the gardens of the existing cottage at 36 Blakes Road (Lot 2 DP 54834).
- Three oak/Quercus trees within the yard at the rear of the Prebbleton Veterinary surgery at 56 Blakes Road (Lot 1 DP 58405).
- Nine oak/Quercus that follow an east-west alignment could also be incorporated into the road alignment along the northern side of the identified Local Major Road and future residential sections to the west of this road. These amenity trees are all currently contained within the legal boundaries of 60 Blakes Road (Lot 1 DP 71538).
- A copse containing two beech/Nothofagus and seven oak/Quercus trees to the north
 of the Local Major Road. The appropriateness of retaining this grouping of trees
 within any future connection to the land holding to the north should be investigated
 at subdivision. These trees are wholly contained within the legal boundary of 60
 Blakes Road (Lot 1 DP DP 71538).

These trees provide amenity to the current and future residents and the community. They are a link to the historic use of the land, compliment the streetscape and assist in preserving and enhancing the character of Prebbleton. Options to secure the on-going protection of these specimens if retained include consent notices or private covenants to assure the trees longevity.

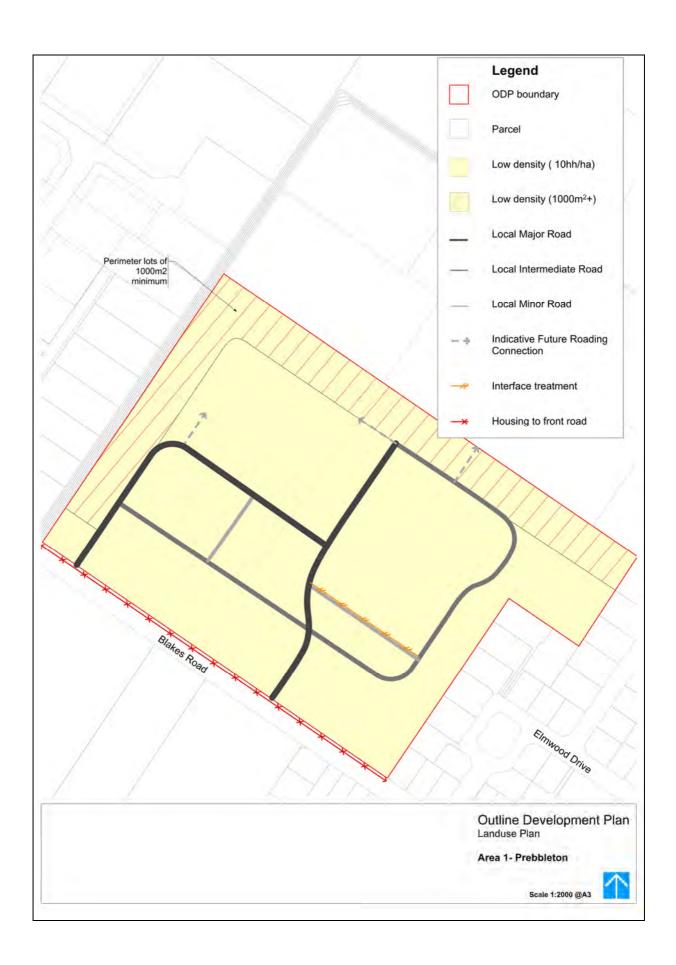
BLUE NETWORK

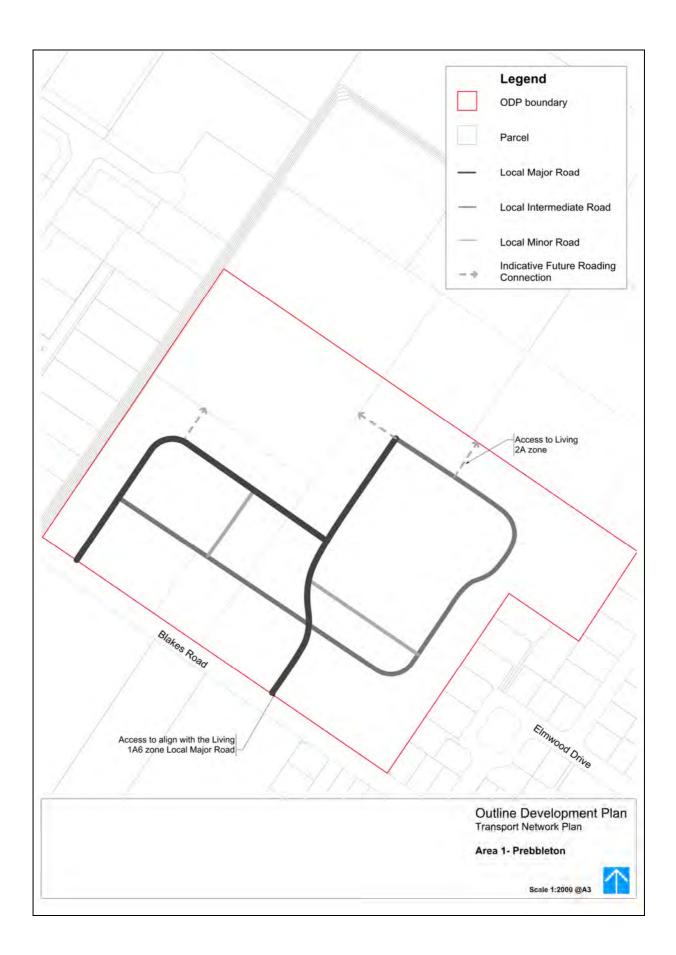
There is sufficient capacity in the Blakes Road water main and wider network to support the additional water connections required to service the area. Any works shall accord with the upgrades identified for the township.

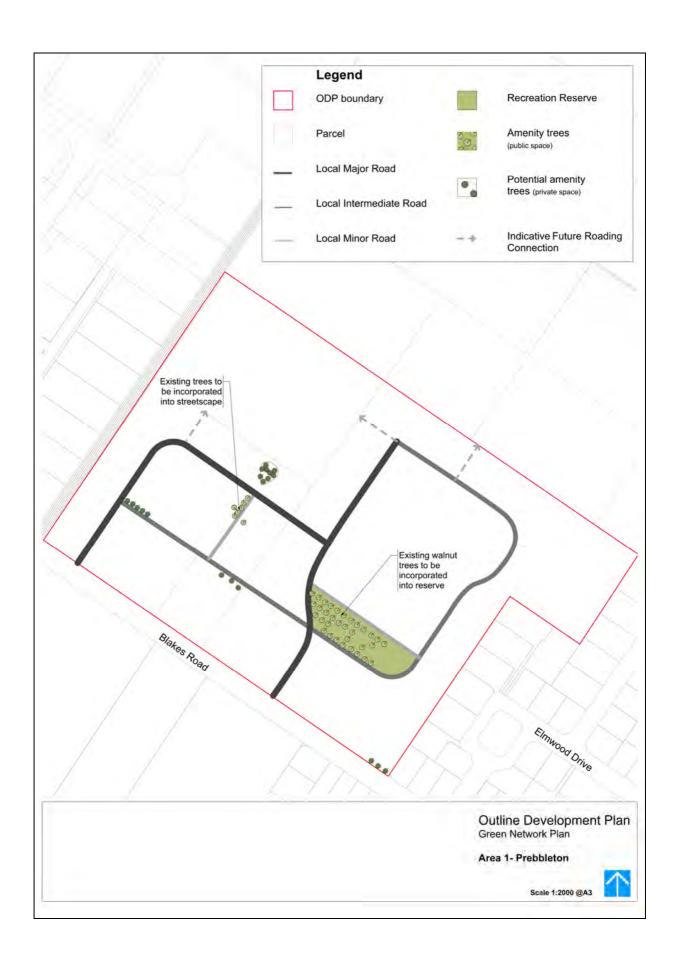
The anticipated wastewater solution is to establish a southern connection to the Living 1A6 zone on the opposite side of Blakes Road to enable access to the sewer gravity main.

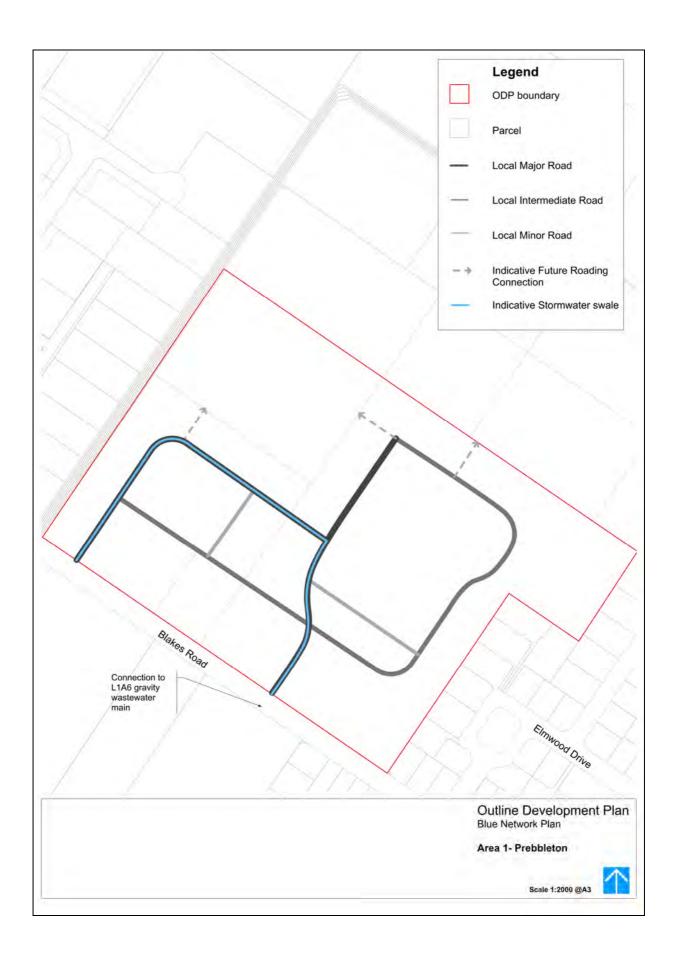
There are a range of methods available to collect, treat and dispose of stormwater. Options include the discharge of hard surface and roof run-off within residential sections to on-site soakage pits and for run-off to be collected in roadside swales and then discharged to ground within the proposed development area. The roadside swales referenced on the ODP Plan are indicative only. Detailed stormwater solutions are to be determined by the developer in collaboration with Council at subdivision stage and in accordance with Environment Canterbury requirements. Stormwater solutions should integrate into both the road and reserve environments where practicable. The establishment of riparian margins and low impact stormwater management techniques are encouraged where appropriate to establish and enhance ecological corridors, habitats and Te Taumutu values attributed to the water resource.











INTRODUCTION

This Outline Development Plan (ODP) is for Area 2, which is zoned Living Z. Area 2 is comprised of a single 6.4ha property that fronts Trents Road. The development block is generally bound by the Kingcraft Drive Existing Development Area zone to the west, Cairnbrae development to the north (Living 1A6 zone) and Waratah Park development to the east (Living X zone).

The ODP provides an overarching urban design framework to guide the future development of the land. The ODP includes Land Use, Movement, Green and Blue Networks and incorporates the wider strategic and community outcomes expressed in the Prebbleton Structure Plan.

URBAN DESIGN

The design principles that underpin this ODP are in line with the New Zealand Urban Design Protocol and accord with the Selwyn District Council Subdivision Design Guide (September 2009). The following environmental outcomes are to be achieved:

- Development that meets the District Plan policies, realises an overall increase in residential density, applies urban consolidation principles and assists in achieving a compact concentric settlement pattern for Prebbleton.
- Provision for a range of section sizes and housing typologies to respond to the wider needs of the community, whilst achieving the prescribed minimum household densities and minimum average allotment sizes.
- Subdivision layouts that integrate with adjoining neighbourhoods and incorporate existing land uses where appropriate. The wider context of the development area should influence the subdivision layout by protecting and enhancing cultural, ecological, heritage and Te Taumutu values and existing built features, such as amenity trees and water races.
- Layouts and urban design treatments that create a distinguishable sense of place, assist in enhancing the wider character and amenity of Prebbleton and deliver safe, vibrant and healthy living environments. Layouts should apply Crime Prevention through Environmental Design (CPTED) design principles.
- Integrated and legible road hierarchy that supports safe and efficient connections and promotes walking and cycling. Road design and landscape treatments should contribute to the overall character of Prebbleton and assist in connecting residential development with open space reserves and other public assets and services within the township, such as the Domain, Primary School, Nature Park and the town centre.
- Sustainable methods to treat and dispose of stormwater that protect groundwater resources and surface waterways (including springs, water/stock races) from contamination, while integrating with open space and reserves where appropriate.
- Installation of all the necessary infrastructure services within the ODP area, and the cost effective and efficient connection of those services to the wider network.

LAND USE

The ODP Area shall achieve a minimum net density of 10 households per hectare. Lower density allotments are necessary on the western boundary to integrate the site with the adjoining lifestyle properties established within the Kingcraft Drive Existing Development Area zone. Appropriate interface treatments at the boundary between residential and rural residential activities, and methods to protect these treatments in the long term, shall be established. Treatments could include appropriate fencing, landscaping and minimum building set backs.

Smaller sections are best established within the centre of the site to reduce the risk of adverse amenity effects on the adjoining Living X zone to the east and Living 1A6 zone to the north. The proposed reserve forms a focal point for residential housing, with layouts and interface treatments optimising the open space amenity and outlook provided by the reserve.

Dwellings must front Trents Road to enhance passive surveillance and safety, while contributing to a high amenity streetscape. Appropriate design layouts should take into consideration the shape, orientation and aspect of sections, with internal roads and access arrangements that support housing that fronts onto Trents Road.

Residential housing established adjacent to any future stormwater basins and the reserve must front these open space areas. Appropriate interface treatments promote passive surveillance and support front yards facing towards the reserve. Streetscapes that are comprised of tall fencing or screening that may undermine the amenity afforded by the reserve are to be avoided. Suitable methods, such as fencing controls and set backs, should be formalised at subdivision stage to ensure all future residential development overlooking these reserves benefit from the high amenity and outlook.

MOVEMENT NETWORK

The proposed roading network consists of one Local Intermediate Road that links Trents Road with the Cairnbrae development to the north (Living 1A6 zone) to achieve an integrated transport network for the wider area. On-road cycling is provided for within the Local Intermediate Road to support the wider circular walking and cycling network identified within the Prebbleton Structure Plan.

The remaining internal roading layout must be arranged in such a way that long term interconnectivity is achieved once the block is fully developed.

GREEN NETWORK

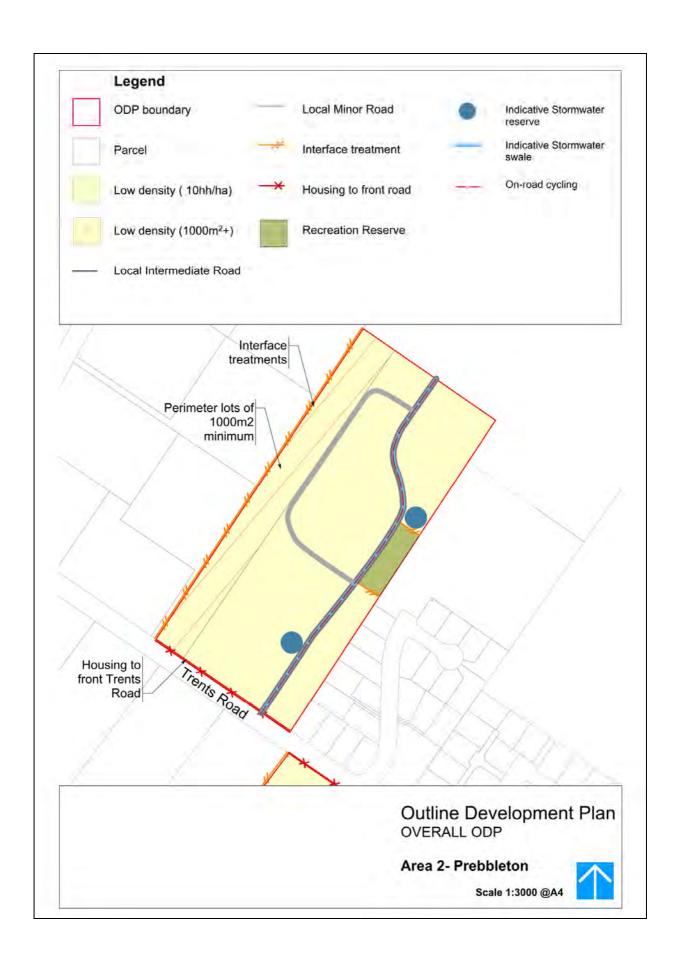
The ODP indicates a single open space reserve that is approximately 2,000m2 in size to accord with the reserve provision calculations for the township and the population base it is serving. The location has been identified to ensure people living within the development block have access to open space within a 400m walking radius of their homes.

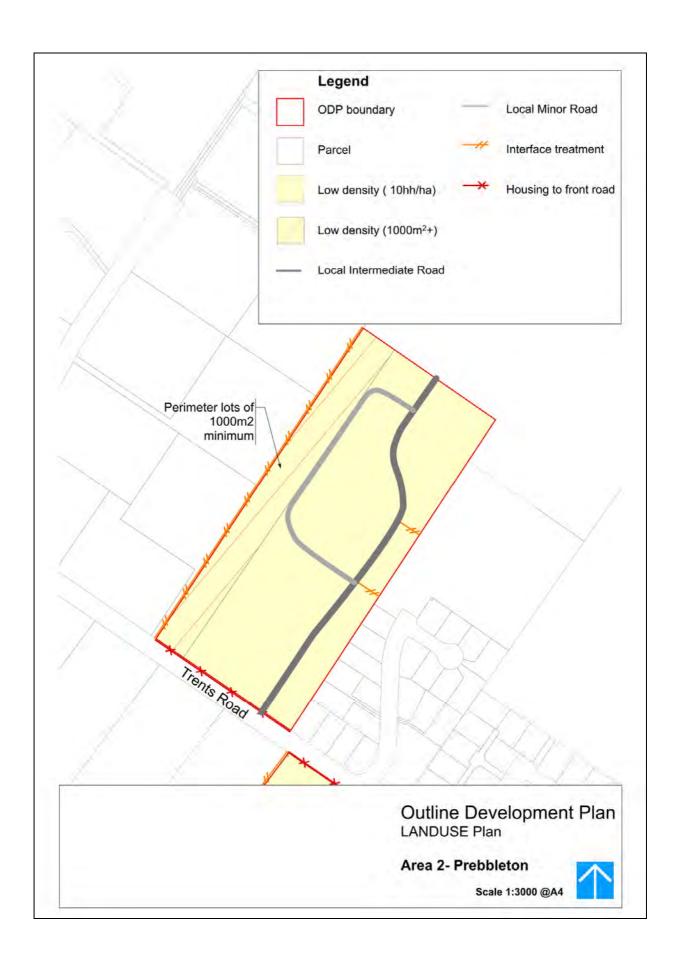
BLUE NETWORK

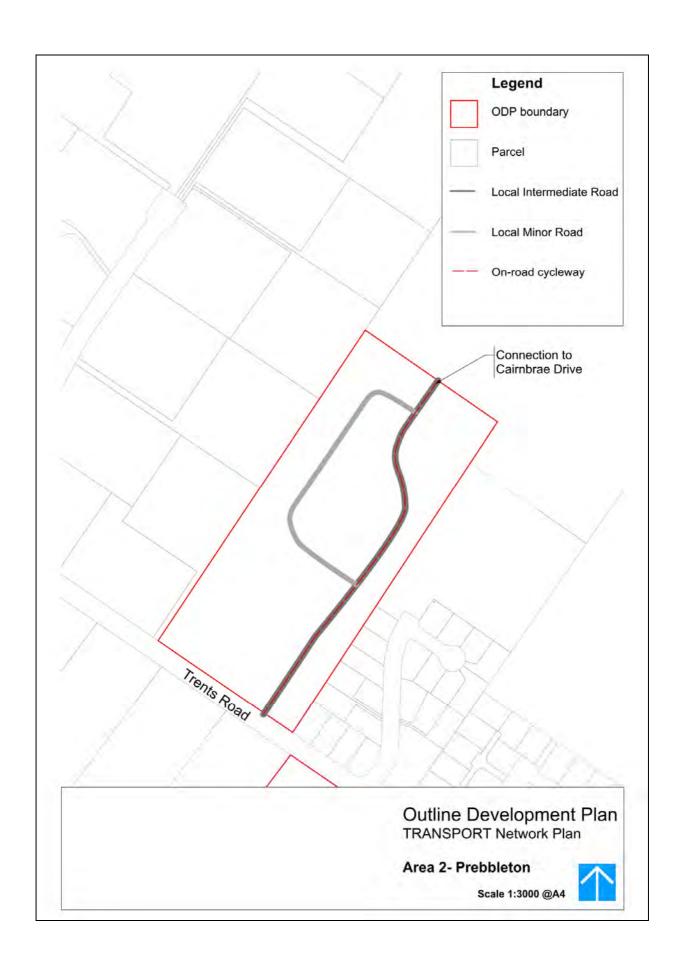
There is sufficient capacity in the Trents Road water main and wider network to support the additional water connections required to service the area, with works having to accord with the upgrades identified for the township.

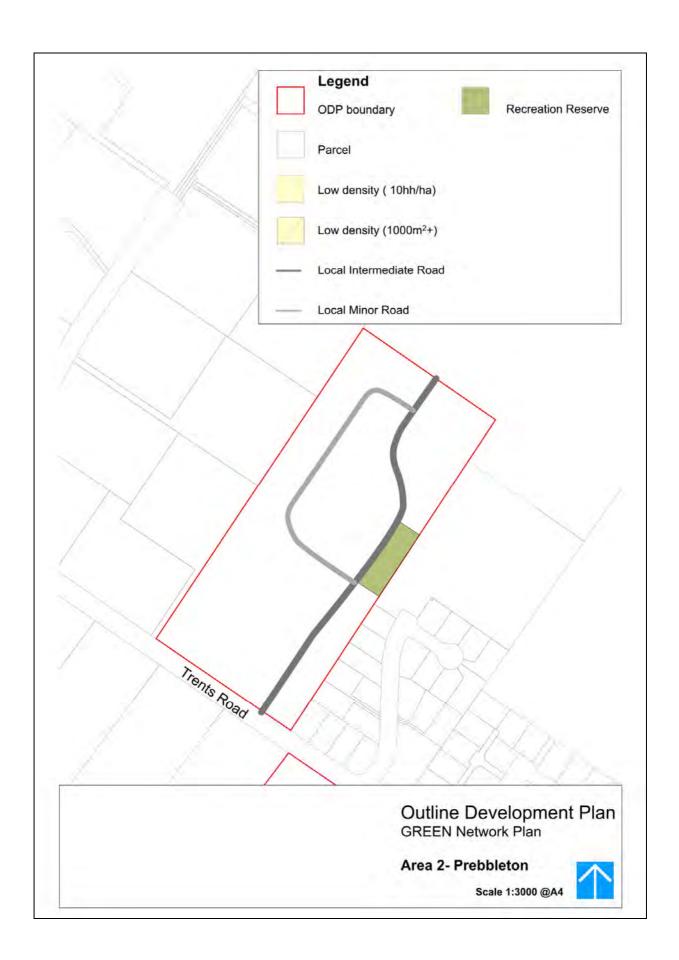
The anticipated wastewater solution is to extend the wastewater network to the pumping station established at the intersection of Trents Road and Lindsay Drive.

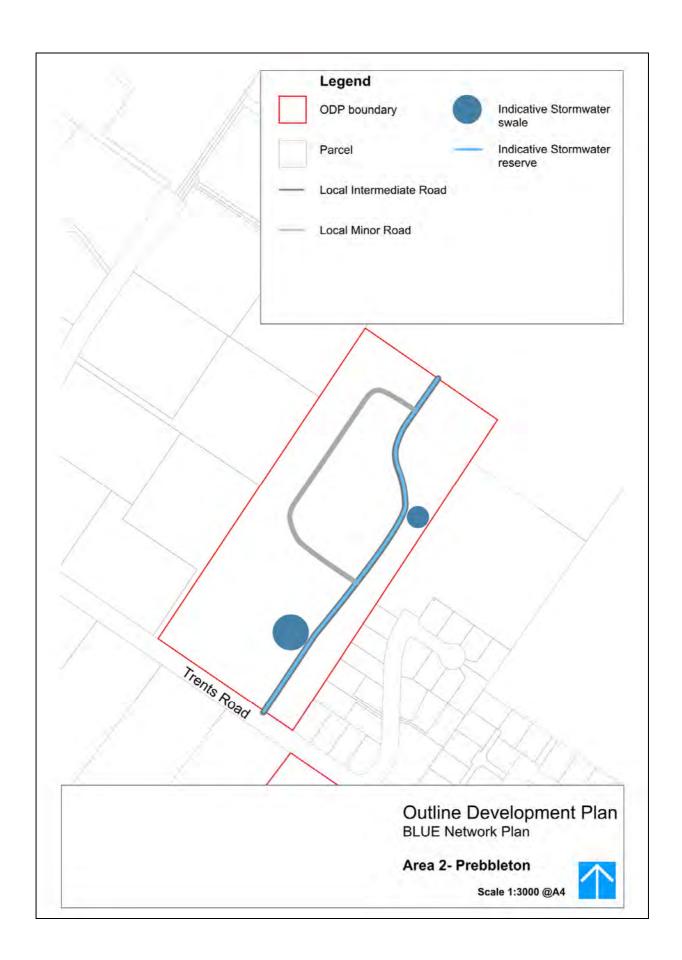
There are a range of methods available to collect, treat and dispose of stormwater. Options include the discharge of hard surface and roof run-off within residential sections to be disposed on site via soakage pits and for run-off to be directed to two stormwater basins within the proposed development area. The stormwater basins and swales referenced in the ODP plan are indicative only. Detailed stormwater solutions are to be determined by the developer in collaboration with Council at subdivision stage and in accordance with Environment Canterbury requirements. Stormwater solutions should integrate into both the road and reserve environments where practicable. The establishment of riparian margins and low impact stormwater management techniques are encouraged where appropriate to establish and enhance ecological corridors, habitats and Te Taumutu values attributed to the water resource.











INTRODUCTION

This Outline Development Plan (ODP) is for Area 3, which is zoned Living Z. Area 3 incorporates 11.7ha of land comprising the five properties that are contained by Trents Road to the north, Hamptons Road to the south, the Sterling Park development (Living X zone) and Prebbleton Nature Park to the east and the Rural (inner Plains) zone to the west.

The ODP provides an overarching urban design framework to guide the future development of the land. The ODP includes Land Use, Movement, Green and Blue Networks and incorporates the wider strategic and community outcomes expressed in the Prebbleton Structure Plan.

URBAN DESIGN

The design principles that underpin this ODP are in line with the New Zealand Urban Design Protocol and accord with the Selwyn District Council Subdivision Design Guide (September 2009). The following environmental outcomes are to be achieved:

- Development that meets the District Plan policies, realises an overall increase in residential density, applies urban consolidation principles and assists in achieving a compact concentric settlement pattern for Prebbleton.
- Provision for a range of section sizes and housing typologies to respond to the wider needs of the community, whilst achieving the prescribed minimum household densities and minimum average allotment sizes.
- Subdivision layouts that integrate with adjoining neighbourhoods and incorporate existing land uses where appropriate. The wider context of the development area should influence the subdivision layout by protecting and enhancing cultural, ecological, heritage and Te Taumutu values and existing built features, such as amenity trees and water races.
- Layouts and urban design treatments that create a distinguishable sense of place, assist in enhancing the wider character and amenity of Prebbleton and deliver safe, vibrant and healthy living environments. Layouts should apply Crime Prevention through Environmental Design (CPTED) design principles.
- Integrated and legible road hierarchy that supports safe and efficient connections and promotes walking and cycling. Road design and landscape treatments should contribute to the overall character of Prebbleton and assist in connecting residential development with open space reserves and other public assets and services within the township, such as the Domain, Primary School, Nature Park and the town centre.
- Sustainable methods to treat and dispose of stormwater that protect groundwater resources and surface waterways (including springs, water/stock races) from contamination, while integrating with open space and reserves where appropriate.
- Installation of all the necessary infrastructure services within the ODP area, and the cost effective and efficient connection of those services to the wider network.

LAND USE

The ODP Area shall achieve a minimum net density of 10 households per hectare. Dwellings must front Springs Road and Trents Road to enhance passive surveillance and safety, while creating a high amenity streetscape. Appropriate design layouts should take into consideration the shape, orientation and aspect of sections, with internal roads and access arrangements that support housing that fronts onto Springs Road and Trents Road.

Residential housing established along the boundary with the recreation reserve and Prebbleton Nature Park must front these reserves. Appropriate interface treatments promote passive surveillance and support front yards facing towards the reserves. Streetscapes that are comprised of tall fencing or screening that may undermine the amenity afforded by the reserve are to be avoided. Suitable methods, such as fencing controls and set backs, should be formalised at subdivision stage to ensure all future residential development overlooking these reserves benefit from the high amenity and outlook provided by the Prebbleton Nature Park and future recreation reserve. Appropriate perimeter planting and fencing must be established along the western boundary of the development block to define the urban/rural interface and avoid adverse reverse sensitivity effects.

MOVEMENT NETWORK

The proposed roading network consists of a Local Major Road that connects Springs Road with Trents Road. On-road cycling is provided for within the Local Major Road to support the wider circular walking and cycling network identified within the Prebbleton Structure Plan, including safe access to the Prebbleton Nature Park. The northern point of this Local Major Road must align with the entrance to Lindsay Drive to create a cross roads and promote connectivity to adjacent neighbourhoods.

The ODP requires a secondary east-west connection to Sterling Drive. Future indicative connections have been identified between Area 3 and the rural land holdings to the west. This will facilitate connectivity and expansion of the network should this land be developed to residential densities in the future.

Local Minor Roads provide access along the north-western boundary of the Prebbleton Nature Park and the southern boundary of the proposed recreation reserve.

Direct through connections from the development block to Hamptons Road are to be avoided to reduce the potential for conflict with future road users. This road is proposed to be upgraded in the future in accordance with the Christchurch Rolleston and Environs Transportation Study 2007.

A detailed engineering assessment is required to establish the proximity roads able to be established within Area 3 where the land adjoins the Prebbleton Nature Park and the former quarry contained within it. This is to ensure that any roads, or subterranean servicing contained within it, do not undermine the stability of the quarry face or contribute to increased road maintenance and upgrade costs in the future.

The remaining internal roading layout must be able to respond to the possibility that this area may be developed progressively over time. Road alignments must be arranged in such a way that long term inter-connectivity is achieved once the block is fully developed.

GREEN NETWORK

The ODP shows a single open space reserve that is 3,000m² in size. The location of the recreation reserve has been determined based on the amount of reserves established in the wider area and to ensure people living within the development block have access to open space within a 400m walking radius of their homes.

An additional open space setback between the Prebbleton Nature Park and the future roads on the north-western and south-western boundaries may be necessary. This is to ensure the roads are sufficiently set back from the former quarry face contained within the reserve. Further detailed engineering investigations will determine the setback needed between the quarry face and the road.

Further investigations shall be undertaken at subdivision to determine the practicalities of retaining the following existing specimen trees within any future layout:

- Plantings and ornamental shrubs along the eastern boundary of the development block with Springs Road, which are contained within the legal boundary of 670 Springs Road (Lot 1 DP 25587). These plantings are recognised as an attractive gateway feature when entering the township and they compliment the amenity of the adjoining Prebbleton Nature Park.
- The black beech/Nothofagus solandri, blue cedar/Cedrus atlantica and copper beech/Fagus sylvatica 'Purpurea' trees contained within the residential section at the corner of Hamptons and Springs Roads, being 146 Hamptons Road (Lot 1 DP 19741).

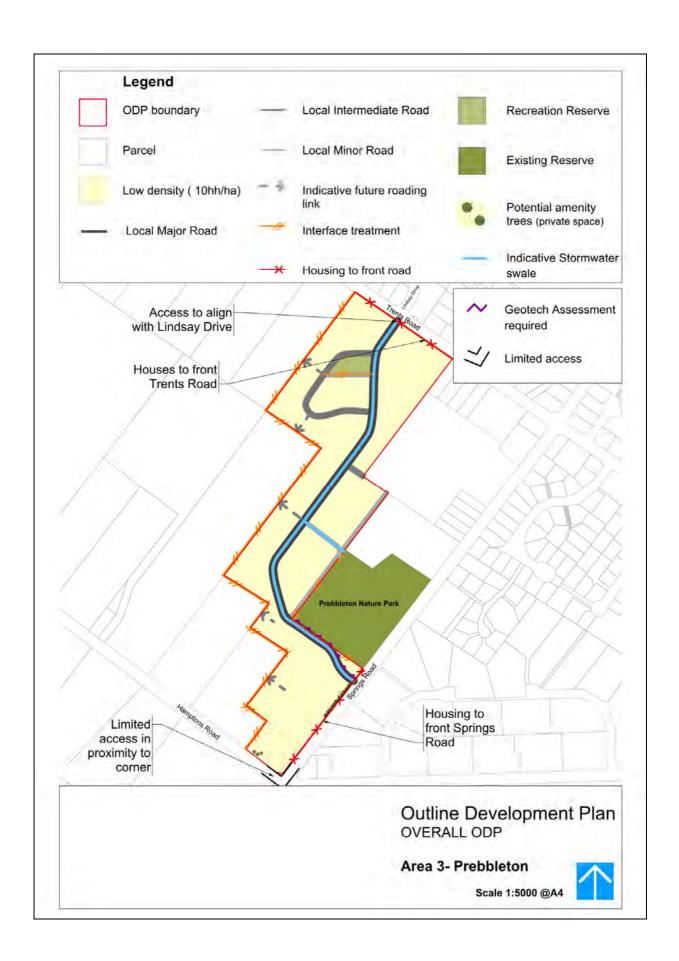
These amenity trees provide a link to the previous use of the land, compliment the streetscape and assist in preserving and enhancing the character of Prebbleton. Options to secure the ongoing protection of these specimens if retained include consent notices or private covenants to assure the trees longevity.

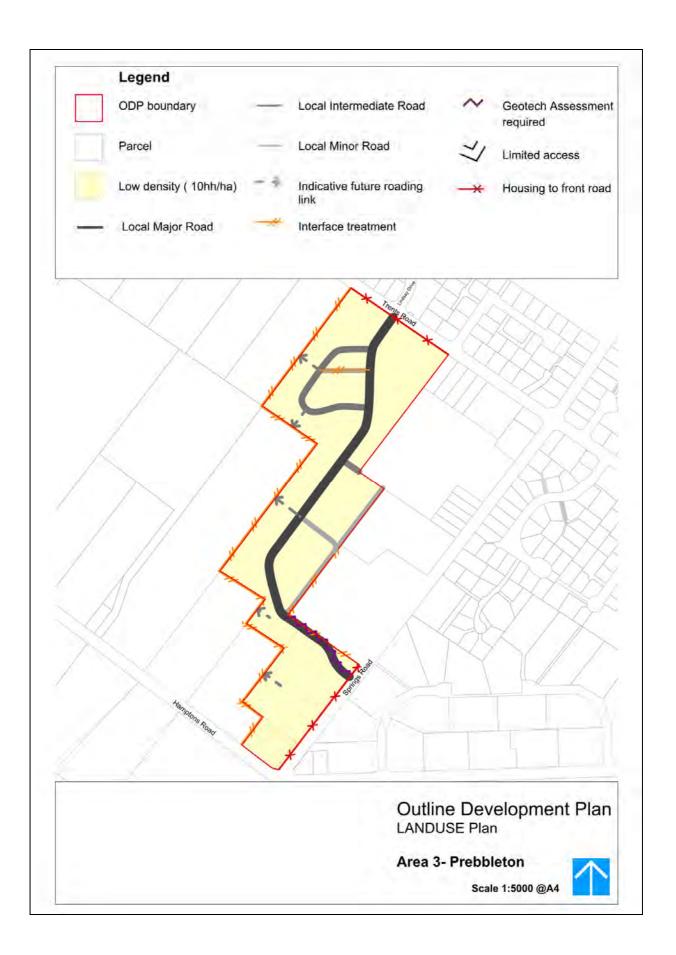
BLUE NETWORK

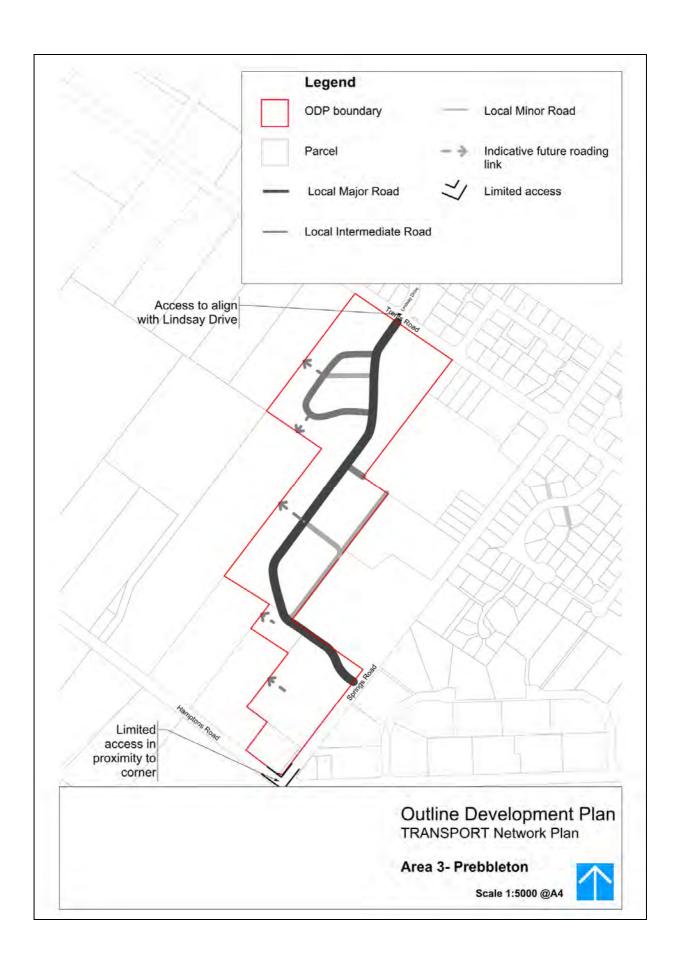
There is sufficient capacity in both the Springs Road and Trent's Road water main and wider network to support the additional water connections required to service the area, with works having to accord with the upgrades identified for the township.

An existing pumping station is located on the corner of Trent's Road and Lindsay Drive. Access to the Springs Road sewer main is restricted. The anticipated wastewater solution is to establish a northern connection to the Trent's Road and Lindsay Drive pumping station.

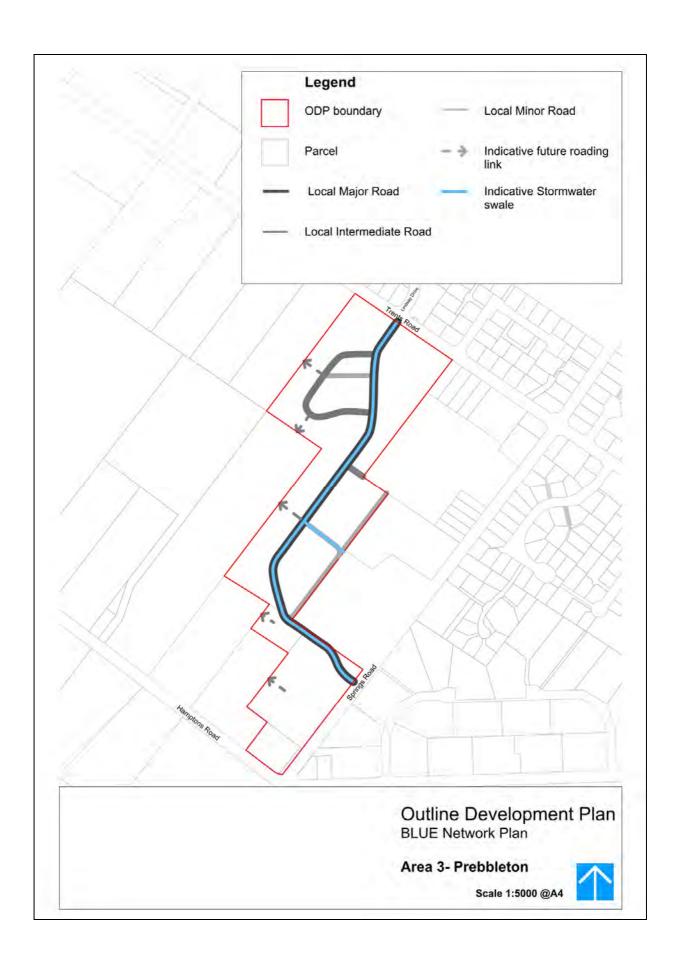
There are a range of methods available to collect, treat and dispose of stormwater. Options include the discharge of hard surface and roof run-off within residential sections to be disposed on site via soakage pits and for run-off being collected in roadside swales and then discharged to ground via soakage pits within the proposed development area. Stormwater associated with large rainfall events may be directed to the Prebbleton Nature Park and discharged to ground. Account will need to be made for the amenity and ecological values of this reserve and the extent to which this would not be undermined by the intermittent stormwater discharges if the option to discharge into the Prebbleton Nature Park is pursued. The roadside swales referenced on the ODP plan are indicative only. Detailed stormwater solutions are to be determined by the developer in collaboration with Council at subdivision stage and in accordance with Environment Canterbury requirements. Stormwater solutions should integrate into both the road and reserve environments where practicable. The establishment of riparian margins and low impact stormwater management techniques are encouraged where appropriate to establish and enhance ecological corridors, habitats and Te Taumutu values attributed to the water resource.











INTRODUCTION

This Outline Development Plan (ODP) is for Area 4, which is zoned Living Z. Area 4 includes 25.5ha of land generally comprising three properties that front onto Tosswill Road. The development block is contained by the Prebbleton Central development to the west (Living X zone), Rural (Inner Plains) zone to the north and east, and the Oakwood Mews development (Living 1A1 zone) to the south.

The ODP provides an overarching urban design framework to guide the future development of the land. The ODP includes Land Use, Movement, Green and Blue Networks and incorporates the wider strategic and community outcomes expressed in the Prebbleton Structure Plan.

URBAN DESIGN

The design principles that underpin this ODP are in line with the New Zealand Urban Design Protocol and accord with the Selwyn District Council Subdivision Design Guide (September 2009). The following environmental outcomes are to be achieved:

- Development that meets the District Plan policies, realises an overall increase in residential density, applies urban consolidation principles and assists in achieving a compact concentric settlement pattern for Prebbleton.
- Provision for a range of section sizes and housing typologies to respond to the wider needs of the community, whilst achieving the prescribed minimum household densities and minimum average allotment sizes.
- Subdivision layouts that integrate with adjoining neighbourhoods and incorporate
 existing land uses where appropriate. The wider context of the development area
 should influence the subdivision layout by protecting and enhancing cultural,
 ecological, heritage and Te Taumutu values and existing built features, such as
 amenity trees and water races.
- Layouts and urban design treatments that create a distinguishable sense of place, assist in enhancing the wider character and amenity of Prebbleton and deliver safe, vibrant and healthy living environments. Layouts should apply Crime Prevention through Environmental Design (CPTED) design principles.
- Integrated and legible road hierarchy that supports safe and efficient connections and promotes walking and cycling. Road design and landscape treatments should contribute to the overall character of Prebbleton and assist in connecting residential development with open space reserves and other public assets and services within the township, such as the Domain, Primary School, Nature Park and the town centre.
- Sustainable methods to treat and dispose of stormwater that protect groundwater resources and surface waterways (including springs, water/stock races) from contamination, while integrating with open space and reserves where appropriate.
- Installation of all the necessary infrastructure services within the ODP area, and the cost effective and efficient connection of those services to the wider network.

LAND USE

The ODP Area shall achieve a minimum net density of 10 households per hectare. Lower density allotments are necessary on the north-eastern boundary of Area 4. This will assist to integrate the site with the adjoining Rural (Inner Plains) zone, to preserve views towards the Port Hills and to increase the separation between future housing and Transpower's 220kV electricity pylons and lines located further to the north-east. Appropriate interface treatments at the boundary between residential and rural activities, and methods to protect these treatments in the long term, shall be established, including appropriate fencing, landscaping and minimum building set backs.

The ODP identifies a Medium Density area situated along the alignment of an open space corridor. The open space corridor provides a high amenity feature that compliments more intensive housing typologies. Its location within the centre of the development area ensures that any effects arising from this form of development on established living environments are internalised to within the development block. The development blocks proximity to the town centre and the establishment of the open space corridor incorporating a pedestrian and cycling connection promotes ready access to the town centre, Prebbleton Primary School, proposed Domain extension and other services established within the township. Provision must be made for appropriate layouts and housing designs that accord with Council's Medium Density Housing Guide and the District Plan. The ODP requires the open space corridor to form a focal point for residential housing. Permeable fencing (a minimum of 50% transparency along the full length of the fence) and fencing setbacks (5m) are to be formalised to ensure a high quality living environment is established. Road layouts and the size, shape and orientation of these Medium Density sections need to be designed in such a way as to protect the amenity of the open space corridor. CPTED principles should also be applied to promote passive surveillance.

Residential housing established along the boundary with the proposed Domain extension must front this reserve. Appropriate interface treatments must promote passive surveillance, support front yards facing towards the reserve and avoid a streetscape that is comprised of tall fencing or screening that may undermine the amenity afforded by the future reserve. Suitable methods, such as fencing controls and set backs, should be formalised to ensure all future residential development overlooking these reserves benefit from the high amenity and outlook at subdivision stage.

Dwellings must front Tosswill Road to enhance passive surveillance and safety, while creating a high amenity streetscape. Appropriate design layouts should take into consideration the shape, orientation and aspect of sections, with internal roads and access arrangements that support housing that fronts onto Tosswill Road.

MOVEMENT NETWORK

The proposed roading network is focused around the Local Major Road that connects Station Masters Way with Tosswill Road and the open space corridor that connects the proposed domain extension with the town centre.

On-road cycling is provided for within the Local Major Road to support the wider circular walking and cycling network identified within the Prebbleton Structure Plan. Off-road pedestrian and cycling connections are also identified along the full length of the open space corridor to support safe connections between the proposed domain extension and the town centre. It also provides an alternative alignment of the Christchurch to Little River Rail Trail. Dedicated walking and cycling links are also identified between the Local Major Road and the western point of the open space corridor, and between the Local Intermediate Road and Hodgens Road to the north-east. This road alignment supports section layouts that optimise the amenity provided by the open space corridor, while promoting a well integrated development.

The Local Intermediate and Local Minor Road network connects with Conductors Way and Platform Way established within the Prebbleton Central (Living X zone) to the west. The ODP

makes specific provision for a Local Minor Road along the alignment of the open space corridor to the north to ensure section layouts facilitate future development that is well integrated into this high amenity feature. A Local Intermediate Road along the south-eastern boundary with the proposed domain extension also aims to support urban design outcomes and preserve the open space amenity associated within this proposed reserve. The same principles apply to the Local Intermediate Road along the south eastern boundary of the proposed Domain extension.

The remaining internal roading layout must be able to respond to the possibility that this area may be developed progressively over time. Road alignments must be arranged in such a way that long term inter-connectivity is achieved once the block is fully developed.

GREEN NETWORK

The ODP requires the establishment of an open space corridor that generally follows the alignment of the existing drain that services the area. The alignment and orientation of this open space corridor has been established to support overland flows for stormwater, secure a green space link that protects easterly views to the Port Hills and accommodate a primary pedestrian and cycling connection that links the town centre with the proposed domain extension.

The open space corridor shall be 20m wide on average along its entire length, although this width is able to vary in places depending upon road layouts, reserve provision and use, stormwater attenuation areas and the alignment of pedestrian and cycle ways.

A 2,200m2 open space reserve is required to be established along the open space corridor to accommodate the large mature macrocarpa/Cupressus macrocarpa tree. This amenity tree is an important land mark and amenity feature that provides a link to Prebbleton's historic past. The tree is contained within the legal boundary of 93 Tosswill Road (Lot 1 and Part Lot 2 DP 5464).

Further investigations shall be undertaken at subdivision to determine the practicalities of retaining the following existing specimen trees within any future layout:

The row of Alder/Alnus trees and hedgerow that extends along a portion of the Tosswill Road frontage of the development block at 55 Tosswill Road (Lot 1 DP 3394 and Lot 2 DP 400006) should be investigated further to confirm that retention is feasible based on road shading, access arrangements and on-going maintenance.

These amenity trees provide a link to the previous use of the land, compliment the streetscape and assist in preserving and enhancing the character of Prebbleton. Options to secure the ongoing protection of these specimens if retained include consent notices or private covenants to assure the trees long term retention.

BLUE NETWORK

There is sufficient capacity in the Tosswill Road water main and wider network to support the additional water connections required to service the area, with works having to accord with the upgrades identified for the township.

The anticipated wastewater solution is to establish a western connection to the pumping station within the Prebbleton Central development (Living X zone) to enable access to the sewer gravity main.

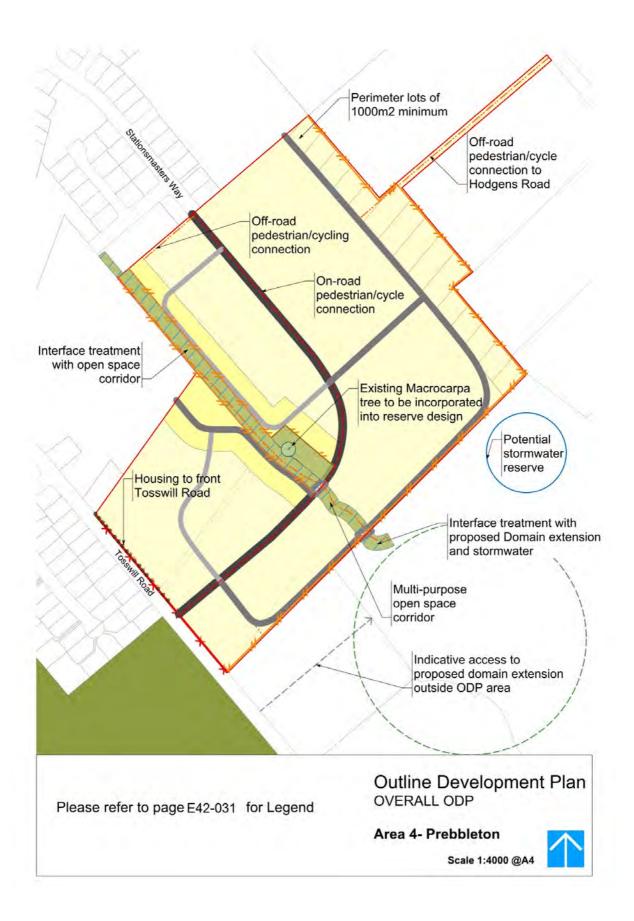
Opportunity exists for a catchment wide approach to manage, treat and dispose of stormwater within an integrated treatment facility to be established to the south-east of the development block. This facility would treat and dispose of stormwater from the areas within the township that are served by the reticulated stormwater network, the Business 1 zone, Prebbleton Central subdivision (Living X zone) and ODP Area 4 to ensure discharges are sufficiently detained within this catchment. An area of 6.4ha north of the proposed Domain extension is anticipated to be

required for the stormwater ponds and riparian margins, with overland flow paths being established within the open space corridor.

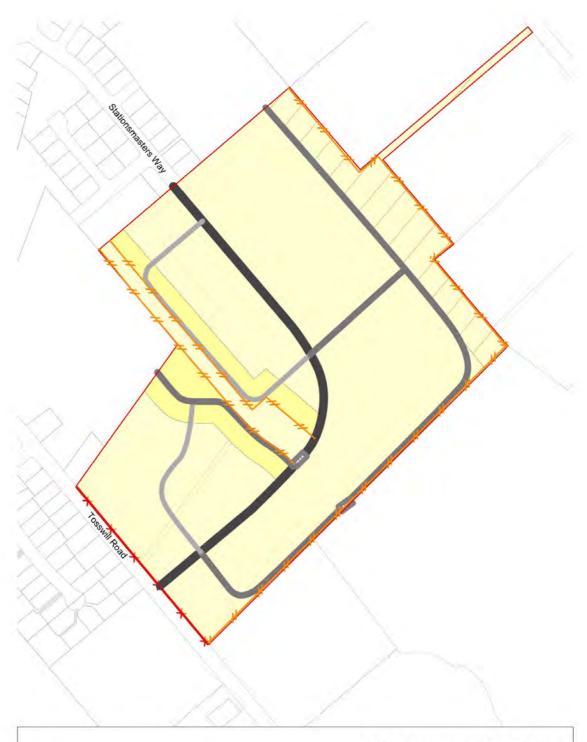
The overall stormwater solution should integrate with the wider transport and reserve network, including the proposed domain extension, walking and cycling network and open space corridor. Detailed stormwater solutions are to be determined by the developer in collaboration with Council and in accordance with Environment Canterbury requirements. This will involve the development of an integrated stormwater management scheme generally located to the east of the development area and north of the proposed domain extension, with stormwater being directed along the open space corridor. Riparian planting along the existing drain and the formation of wetland environments that support ecological, cultural and Te Taumutu values will also be a key component of the overall design. 'Spring reserves' are to be provided where necessary to separate spring water from stormwater flows to protect the Te Taumutu values of the spring water.

The existing stormwater facility that services the Prebbleton Central subdivision (Living X zone) and town centre could be decommissioned once the catchment wide stormwater treatment facility is established. If this was to occur, the resulting land may then be integrated into the open space corridor, with the balance being able to be developed to accommodate Medium Density housing.

On-site stormwater management that satisfies all Environment Canterbury requirements will be necessary until such time as the integrated stormwater management scheme is established.







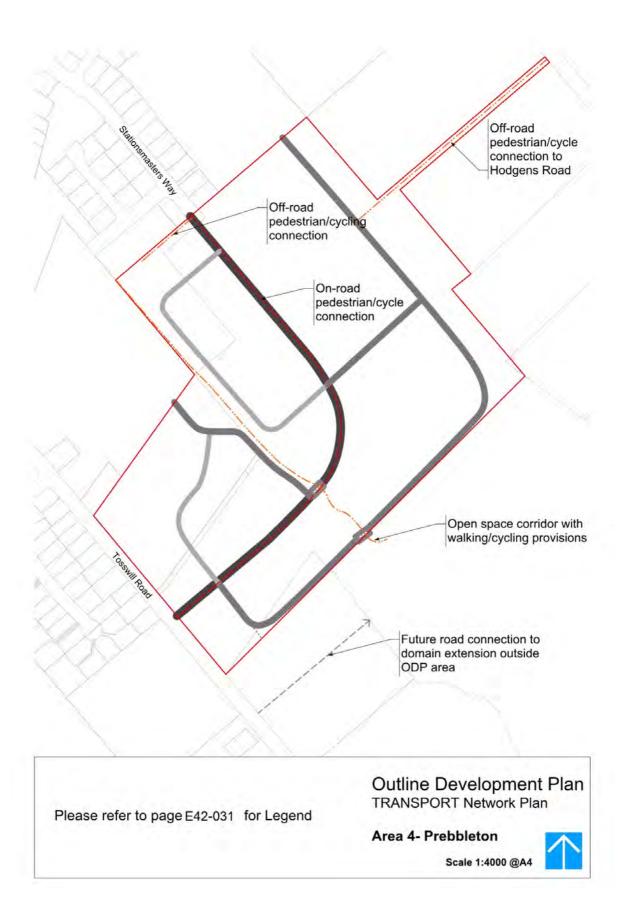
Please refer to page E42-031 for Legend

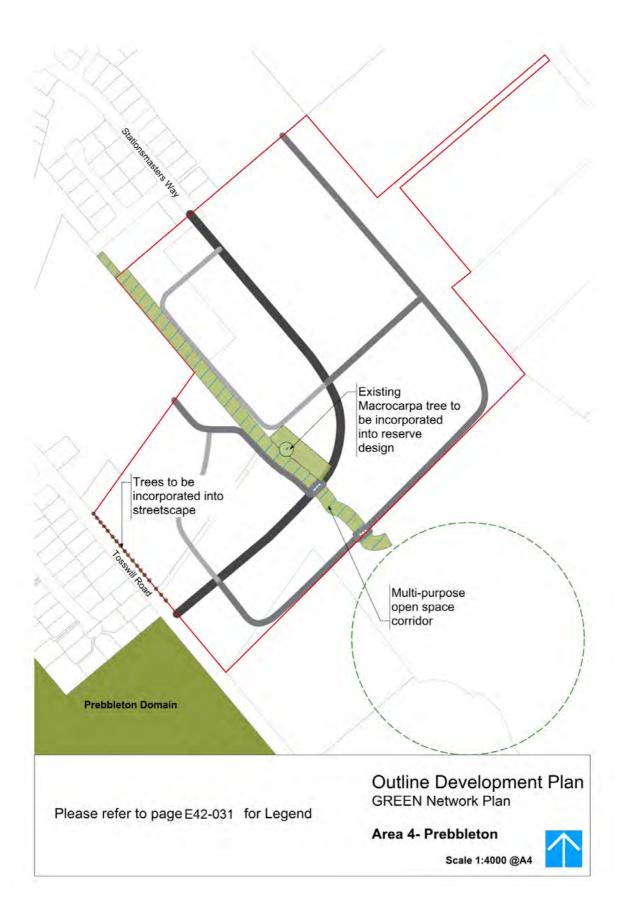
Outline Development Plan LANDUSE Plan

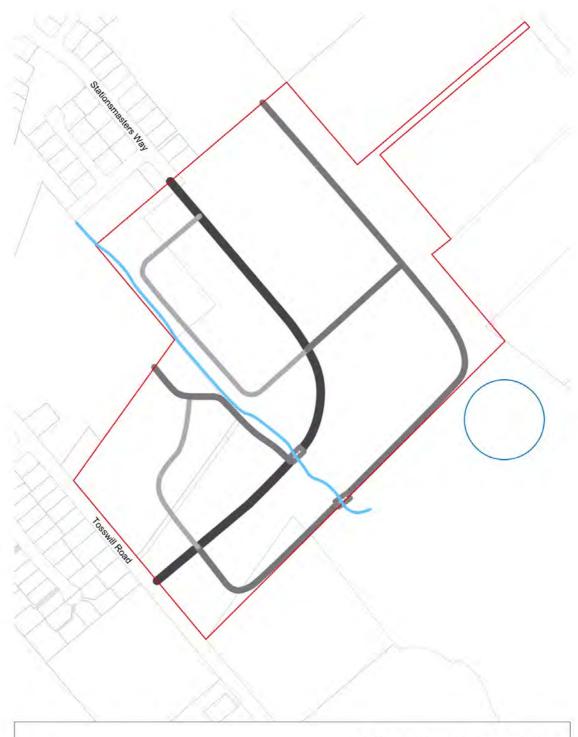
Area 4- Prebbleton

Scale 1:4000 @A4









Please refer to page E42-031 for Legend

Outline Development Plan BLUE Network Plan

Area 4- Prebbleton

Scale 1:4000 @A4



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SCHEDULE OF AMENDMENTS

The amendments to the Selwyn District Plan are due to the following changes:

Name	Description
Land Use Recovery Plan	The Land Use Recovery Plan has been approved by the Minister for Canterbury Earthquake Recovery and was gazetted on 6 December 2013
Clause 20a Amendments	Various minor amendments

Please amend your District Plan by updating the following pages:

Township Volume

Amendments from 20.06.2013 to 06.12.2013

Replace pages - Please recycle all pages removed		
Part AO Contents	A0-005 – A0-006	
nsert new Appendix 42, Living Z Zone, Prebbleton		
art A4 Finding Material	A4-011 - A4-012	
4.5 Townships and Zones, Table 4.4 – Descriptions of Townships and Zones, elete Living Z, and insert new Living Z description		
art B1 Natural Resources	B1-007 - B1-008	
elete Policy B1.1.7 and insert new B1.1.7		
rt B2 Physical Resources	B2-025 - B2-026	
nend Policy B2.1.26 relating to noise sensitive activities		
rt B4 Growth of Townships	B4-003 - B4-004,	
	B4-031 - B4-042,	
elete existing and insert new Policy B4.1.1, Obj B4.3.3, B4.3.5 and B4.3.6, Policy 4.3.1, B4.3.7, B4.3.8, B4.3.9, B4.3.56 and B4.3.76.	B4-069 - B4-070,	
sert new Policy B4.3.68, and renumber subsequent policies accordingly	B4-075 - B4-108	
rt C4 Living Zone Rules – Buildings	Entire chapter	

Table C4.1 Site Coverage Allowance – delete Living Z and insert new Living Z Allowance

Delete existing and replace with new - C4.7.3.3, C4.7.4.1, C4.9.23, C4.13.1, C4.13.2, C4.13.3

Delete C4.13.5

Insert new rules C4.17 Fences Adjoining Reserves, C4.17.1, C4.17.2 and sub rules.

001 Schedule of Amendments

Part C12 Living Zone Rules – Subdivision	C12-011 - C12-016 C12-029 - C12-030
Table C12.1 Allotment Sizes, Delete Living Z zone for Lincoln, Prebbleton and Rolleston, and replace with new description. Delete Living 1A for Prebbleton and replace with new description Delete Rule 12.1.7.6 and replace with new rule.	
Part D - Definitions	D-009 - D-016
Delete existing definition of 'Noise Sensitive Activities' and insert new definition	
Part E – Appendix 19 Prebbleton Living 1A, 1A1, 1A2, 1A3, 1A6, LX and 2A Zones, Prebbleton	E19-005 - E19-016
Delete ODPs marked "Selwyn District Council Outline Development Plan Shaw Appeal 27/04/06" and "Selwyn District Council Outline Development Plan Fowler and Troy enterprises 27/04/06". Add new ODP Area 5	
Part E – Appendix 37 Lincoln	E37-009 - E37-010
ODP Area 2, under Density Plan – delete 1st sentence and insert new	E37-017 - E37-026
ODP Area 3, under Density plan – delete paragraph and insert new ODP Area 4, under Density Plan – delete 1 st and 2 nd sentences and insert new, insert new ODP Maps	E37-039 - E37-040
ODP Area 6, under Density Plan – delete paragraph and insert new	
Part E – Appendix 38 Rolleston	E38-011 - E38-012
ODP Area 2, under Residential Density – rename subheading as Density Plan, delete	E38-015 - E38-016
3rd sentence and insert new	E38-021 - E38-030
ODP Area 3, under Density Plan – delete paragraph and insert new ODP Area 6, under Density Plan – delete ODPs and text and insert new ODPs and text	E38-035 - E38-036
ODP Area 7, under Residential Density – rename subheading Density Plan, delete	
text and insert new ODP Area 8, under Density Plan – delete text and insert new	
Part E – Appendix 42 Prebbleton, Living Z Zone, Prebbleton	Entire chapter
Insert new Appendix 42	

002 Schedule of Amendments LURP

Rural Volume

Amendments from 20.06.2013 to 06.12.2013

Replace pages - Please recycle all pages removed	
Part B2 Physical Resources	B2-025 - B2-026
Amend B2.1.28, Christchurch International Airport	
Part D Definitions	D-007 - D-014
Insert new definition for Noise Sensitive Activities	

Maps Volume

Amendments from 20.06.2013 to 06.12.2013

Replace pages - Please recycle all pages removed

Amended Planning Maps, please replace as follows:

Maps: 008, 009, 013, 014, 095, 096, 097, 098, 099, 101, 102, 103, 104, 105, 107, 108, 109, 110, 111, 113, 114, 115, 116, 117, 118, 119, 120, 121, 122, 123, 125, 126, 127, 128, 129, and 130.

Schedule of Amendments 003

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Policy B2.1.26

Encourage heavy vehicles to use routes which bypass townships, where practical and appropriate, and avoid new residential development along heavy vehicle bypasses.

Explanation and Reasons

Heavy vehicles travelling through townships can adversely affect:

- Residential amenity values through dust, noise and vibration;
- Perceptions of safety, especially for cyclists and pedestrians; and
- Roads, if they are not designed for heavy vehicles.

Policy B2.1.26 encourages heavy vehicles to use routes that bypass rather than bisect townships, where practical and appropriate, in order to avoid these effects. The preferred method to achieve this in these circumstances is to design ring roads and bypasses that are quicker and easier to use, than roads which bisect townships. Consequently, once a bypass or heavy vehicle route is created, it is important that it is not adversely affected by new residential or business activities occurring along the route, and then trying to slow or restrict the traffic using it.

Methods

Selwyn District Council Engineering Code of Practice

Road design

Asset Management Plans

Roads

District Plan Policies

 To assess plan change requests to rezone land for the expansion of townships around heavy vehicle routes or bypasses

Bylaws

- To prohibit heavy vehicle use of roads if necessary.

Policy B2.1.27

Discourage adverse effects from constructing or maintaining roads or railway lines on the natural environment, landscape values, and sites with heritage or cultural values.

Explanation and Reasons

Roads and railway lines traverse or adjoin parts of the rural area with special values. Constructing and maintaining them can affect the environment. For example: disturbing sites; placing sediment in wetlands and waterbodies; clearing vegetation; or visual effects on slopes. Roads and railway lines are managed by designations; so District Plan rules do not apply. The Council can use its powers under section 176(A) of the Act (to approve outline plans for designations) and advocacy, to encourage adverse effects to be mitigated.

Method

- Advocacy
- Discussions with network utility operators

CHRISTCHURCH INTERNATIONAL AIRPORT

Policy B2.1.28

Maintain residential density at a maximum of 1 house per 4 hectares, and avoid other noise sensitive activities in the rural area within the 50 dBA Ldn air-noise contour shown on the Planning Maps.

Policy B2.1.29

Require any new dwelling or extension to an existing dwelling within the area under the 55 dBA Ldn (or greater) air-noise contour shown on the Planning Maps, to be insulated for noise to the standards required in the Table contained in Appendix 20 of this Plan.

Explanation and Reasons

Noise contours, shown on the Planning Maps, are for noise from aircraft on the approach path to the Christchurch International Airport.

Christchurch International Airport is one of the few international airports which currently operate without any restrictions on the type of aircraft or times of flights, to manage effects of aircraft noise.

Christchurch International Airport Limited, the Airport Company, is anxious to maintain unrestricted operation in the future. Therefore, Christchurch International Airport Limited wants to prevent residential activities, or other activities which may be sensitive to aircraft noise, locating close to the airport and then lobbying for restrictions on the airport's operations. To avoid this situation, Christchurch International Airport Limited opposes residential development at densities greater than 1 house per 4 hectares on land inside the 50 dBA Ldn contour line.

New Zealand Standard NZS 6805:1992 recommends restrictions on residential development apply where aircraft noise exposure is 55 dBA Ldn or greater, but notes that greater protection may be appropriate in some areas. Christchurch International Airport Limited advocates for land use restrictions from 50 dBA Ldn. Overseas research shows people become annoyed by aircraft noise at levels lower than 55 dBA Ldn, so the risk of "reverse sensitivity" effects occurs before then. At 50 dBA Ldn, Christchurch International Airport Limited advocates restricting residential activities rather than requiring noise insulation. The reason is that the effects from aircraft noise at 50 dBA Ldn are mostly experienced outdoors or when windows are open. Policy B2.1.28 requires that residential density be maintained at a maximum of 1 house per 4 ha in rural areas within the 50 dBA Ldn air noise contour.

Policy B2.1.29 recognises that any risk of potential 'reverse sensitivity' effects on flight operations through Christchurch International Airport must be avoided because of the importance of the operation of the airport to the Region's and District's economy.

The noise contours are shown on the Planning Maps.

Indigenous Vegetation: means a plant community in which species indigenous to that part of New Zealand are important in terms of coverage, structure and/or species diversity. For these purposes, coverage by indigenous species or number of indigenous species shall exceed 30% of the total area or total number of species present, where structural dominance is not attained. Where structural dominance occurs (that is indigenous species are the tallest stratum and are visually conspicuous) coverage by indigenous species shall exceed 20% of the total area.

Industrial Activity: means any activity involving the manufacturing, production, processing, assembly, disassembly, packaging, servicing, testing, repair, direct handling, distribution and/or warehousing of any materials, goods, products, machinery or vehicles, but excludes mining, mineral exploration and quarrying and, for the avoidance of doubt, harvesting activities associated with plantation forestry. For the purpose of this definition an industrial activity is further defined as being either of the following:

(a) Rural Based Industrial Activity: means an Industrial Activity that involves the use of raw materials or primary products which are derived directly from the rural environment, including agricultural, pastoral, horticultural, forestry, viticultural and crops.

Or

(b) Other Industrial Activity: means any other Industrial Activity that is not defined as a "rural based industrial activity", as stated in (a) above.

Intensive Livestock Production: means the use of land and buildings for the commercial rearing and management of livestock where the viability of that activity is not dependant upon the soil fertility of the land on which that activity is undertaken.

Intensive Piggery Production Activity: means the use of land and buildings for the commercial rearing and management of pigs where the viability of the activity is not dependent upon the soil fertility of the land on which that activity is undertaken.

Internal Boundary: refer to Boundary.

K

Kitchen: means a room or indoor area, the principal purpose of which is the preparation and cooking of food. A kitchen will generally include a sink bench, and a means of cooking food such as a stove, gas or electric cooker, or microwave (or have the facilities for the installation of these).

L

Landscaping: means the visual improvement of an area through designed live planting of trees, shrubs and ground cover for amenity purposes and may include provision of physical features such as paving, art and seating. For the purposes of this definition, landscaping does not include the re-contouring of land by removing or displacement of earth or soil, or for any earthworks associated with the erection or installation of a wall.

 L_{10} : means the L_{10} exceedence level, in A-frequency-weighted decibels, which is equalled or exceeded ten percent of the total measurement time.

Ldn: means the night-weighted sound exposure level in dBA also known as the day-night average sound level and is the 24 hour sound exposure level in A-frequency weighted decibels (dBA) for any day with the period 10pm to 7am the following day "weighted" by 10 dB to represent the increased sensitivity of people to night-time noise. See NZS 6801:1999 Acoustics – Measurement of Environmental Sound.

Leq: means the sound level averaged over a stated time period which has the same A-weighted sound energy as the time varying sound during the same period. See NZS 6801:1999 Acoustics – Measurement of Environmental Sound.

L_{max}: means the maximum A-frequency-weighted sound level (dBA L_{max}) during a stated time period. See NZS 6801:1999 Acoustics – Measurement of Environmental Sound.

Local Road: (includes cul de sacs): means a road that is not intended to act as main through routes for traffic as their primary function is to provide property access, and they generally have lower traffic volumes. Any road in the district that is not a State Highway, Arterial or Collector road is a 'local road and not specifically identified in this Plan.

M

Mahinga Kai Site: includes any land, building or structure which is listed in Appendix 5 and shown on the Planning Maps as a Mahinga Kai Site.

Maintenance of gardens lawns or public spaces: means carrying out any work to preserve existing gardens lawns or public spaces in such a way that the scale or nature of the gardens lawns or public spaces is not altered. Maintenance does not include upgrading.

Mineral Exploration: has the same meaning as "exploration" in section 2 of the Crown Minerals Act 1991. *

Mining: has the same meaning as "mining" in section 2 of the Crown Minerals Act 1991. *

Monofill: includes any landfill which is designed and operated to accept waste associated with one specific activity or form of waste. For the purposes of Rule 7, a monofill does not include any landfill which accepts hazardous waste.

Municipal Solid Waste: includes any non-hazardous solid waste from any combination of domestic, commercial and industrial sources. It includes putrescible waste, garden waste, uncontaminated biosolids and contaminated waste, which is sterilised to a standard acceptable to the Department of Health and does not contain liquids or hazardous waste, which exceeds 200ml/tonne or 200g/tonne.

Network Infrastructure: has the same meaning as in section 197 of the Local Government Act 2002. *

N

Noise Limit: means a L_{10} , Leq or Lmax sound level in A-frequency-weighted decibels that is not to be exceeded during a measurement sample time in a specific time-frame. See NZS 6801:1999 Acoustics – Measurement of Environmental Sound.

Noise Sensitive Activities:

- Residential activities other than those in conjunction with rural activities that comply with the rules in the plan;
- Educational activities including pre-school places or premises;
- Travellers' accommodation except that which is designed, constructed and operated to a standard that mitigates the effects of noise on occupants;

Hospitals, healthcare facilities and elderly persons housing or complex.

Noticeboard: means a structure or device upon which notices can be placed or attached that are of community interest, which are intended to be read by people stopping at the notice board rather than by people passing by.

Notional Boundary: means a line 20m from any side of a rural dwelling or the legal boundary where this is closer to the dwelling.

NZTA: the New Zealand Transport Agency.

0

Operating Speed: The speed at which motor vehicles generally operate (85th percentile speed) on a particular road. Where operating speeds are unknown they are assumed to be the posted speed limit plus 10km/hr.

P

Papakainga Housing: includes any dwelling(s) which is/are erected to house members of the same family, iwi, or hapū, on land which is owned by that family, iwi or hapū, and which is Māori Land within the meaning of section 129 of Te Ture Whenua Māori Act 1993.

Parking Areas: means a continuous portion of a site, sites, allotment, allotments or part of any site or allotment on which parking for motor vehicles is provided and includes associated access.

Pathway: means a formed path for pedestrians and, or cyclists.

Pedestrian-cyclist link: means a green transport corridor for pedestrians and, or cyclists that for example links a road to a road, or a road to a reserve or facility. They are also known as 'walkway/cycleway links'. There is generally a path provided within the corridor for pedestrians and cyclists to share.

Place of Assembly: means any land and building used for the gathering of people. It does not include residential accommodation or places of work.

Planned Upgrade: includes any work proposed to form, install or upgrade any road or other utility which has been planned to occur within the next ten years, as outlined by the Council in the relevant Asset Management Plan(s) or Long Term Financial Strategy.

Plantation: means any group of trees planted on a site, whether intended to be harvested or not, which are not classed as amenity plantings or shelterbelts. A plantation may be comprised of exotic or indigenous species and includes but is not limited to: forests planted for harvesting, conservation, soil erosion control, pest or wilding tree management, and any orchard, vineyard or woodlot which does not comply with the definition of amenity planting (refer also to the definition of Existing Plantation).

Point Strip: includes any strip of land adjoining the site or end of a road, the purpose of which is to prevent access on to that road from land adjoining the point strip. Point strips may also be for the purpose of protecting the opportunity to achieve future transport links between adjoining blocks of land. A point strip is usually (though not exclusively) up to 200mm wide.

Posted Speed Limit: the legal and sign posted speed limit that applies to the road as per the Selwyn District Council Speed Limits Bylaw.

Principal Building: includes any building or buildings which is/are used as part of the primary activity or activities on the site. Principal buildings include dwellings but do not include accessory buildings.

Project: for the purposes of Rule 1.6.1.2 the term 'project' includes all earthworks undertaken as part of, or ancillary to, the completion of one particular activity or the construction of a building or structure, whether that activity is undertaken or the building or structure is erected continuously or in discrete stages; and whether it occurs in one continuous area or is separated by land which is not disturbed by earthworks. For example: the construction of one road or track from destination (a) to (b) is one project, as is the disturbance of soil for geological surveys over an identified area.

Property: means all land and adjoining land which is held in the same ownership. Any property may include one or more allotments.

Protected Tree: A tree, or group of trees, which are of significance to the District, and are listed in the protected tree schedule in Appendix 4.

These trees fall into two categories:

- Category A: trees that have particularly high levels of significance, particularly in relation to heritage values, and score the highest under the evaluation criteria.
- Category B: trees that have significance for a range of reasons (e.g. landscape importance) and have a moderate score under the evaluation criteria.

Public Waste Collection Service: includes any waste collection service which is provided by the local authority using its functions under the Local Government Act 1974.

Q

Quarrying: means to take, mine or extract, by whatever means, any rock, stone, gravel or sand existing in its natural state in land. "To quarry" has a corresponding meaning.

R

Radio Communication Facility: means any transmitting or receiving devices such as aerials, dishes, antenna, cables, lines, wires and associated equipment/apparatus, as well as support structures such as towers, masts and poles, and ancillary buildings.

Recreational Facility: includes the use of any land, building or structure for the primary purpose of recreation or entertainment and is available to be used by members of more than one household.

Relocated Building: includes any building that is removed from one site and relocated to another part, in whole or in parts. It does not include any new building which is designed for, or intended to be used on, a site but which is erected off the site, in whole or in parts, and transported to the site.

Research: means the use of land and buildings for the purpose of scientific research, inquiry or investigation, product development and testing, and consultancy and marketing of research information; and includes laboratories, quarantines, pilot plant facilities, workshops and ancillary administrative, commercial, conferencing, accommodation and retail facilities.

Residential Activity: means the use of land and buildings for the purpose of living accommodation and ancillary activities. For the purpose of this definition, residential activity shall include:

- a) Accommodation offered to not more than five guests for reward or payment where the registered proprietor resides on-site;
- b) Emergency and/or refuge accommodation; and
- c) Supervised living accommodation and any associated caregivers where the residents are not detained on the site.

'Residential Activity' does not include:

- (a) Travelling accommodation activities (other than those specified above); and
- (b) Custodial and/or supervised living accommodation where the residents are detained on the site.

Restaurant: means any land and/or buildings, or part thereof, principally used for the sale of meals or light refreshments to the general public and the consumption of those meals or light refreshments on the premises. Such premises may be licensed under the Sale of Liquor Act 1989.

Riparian Margin/Zone: means a strip of land of varying width adjacent to a waterbody which contributes to the natural functioning, quality and character of the waterbody, the land margin and their ecosystems.

Road: shall have the same meaning as defined in section 315 of the Local Government Act 1974. * and shall include roads to be vested.

Road Boundary: refer to Boundary.

Rural Activity: means the use of land or building(s) for the purpose of growing or rearing of crops or livestock, including forestry, viticulture and horticulture and intensive livestock production and may include a dwelling.

S

Secure: with respect to cycle parking, means that cyclists and their bicycles are not exposed to danger or harm; for example cycle parking is not located where manoeuvring vehicles could collide with a cyclist and or their parked bicycle; also that the cycle parks are not located where there is no visibility from a public space (i.e. a road or car parking area).

Sensitive Activity: includes any of the following activities:

- Residential Activity;
- Travelling Accommodation;
- Community Facility;
- Recreational Facility or Recreational Activity;
- Place of Assembly;
- Restaurant;
- Educational Facility;
- Camping Ground Facility;

but excludes Temporary Accommodation.

Shelterbelt: means any trees planted primarily to provide shelter for stock, crops or buildings from the prevailing wind(s). Shelterbelts are not more than 20 metres in width and are not clearfelled.

Sign: means any device or structure which is visible from any public space and is used to: identify any site or building; provide directions or information; or promote any goods, services, or forthcoming event. A sign does not include any window display, or any property identification signs which do not exceed an area of 0.2 m² (including rural numbers, dairy company numbers, street/road numbers/property names and property owners' names as long as those property identifiers do not include any advertising).

Silent File Area: includes any land which is listed in Appendix 5 and shown on the Planning Maps as a Silent File Area.

Site: means the area of land on which a particular activity and any ancillary activities by a person or party is/are undertaken. A site may include all or part of an allotment or more than one allotment.

Skyline: means the ridgelines of any mountain or hill range which are silhouetted against the sky and which do not have any other hill, mountain or mountain range behind them which is higher.

Solid Waste: includes any material which is discarded as being spent, useless, worthless or in excess, and includes liquid or gaseous waste which is stored in containers.

Sound Exposure Level: means the A-frequency weighted sound pressure level in decibels which, if maintained consistent for a period of 1 second, would convey the same sound energy to the receiver as is actually received from a given noise event over the same period. See NZS 6801:1999 Acoustics – Measurement of Environmental Sound.

State Highway: means any road that is identified as a State Highway in the road hierarchy classification as listed in Appendix 9 and managed by the New Zealand Transport Agency. They are high capacity roads of national importance providing inter-district and regional links between significant transport destinations such as towns, cities, ports and other places of significance. State Highways are maintained to high standards to ensure they operate efficiently, including managing both road and property access to them through the New Zealand Transport Agency's powers under the Government Roading Powers Act. They are also subject to access controls in this Plan.

Stopbank: means an embankment constructed for the purpose of preventing flooding.

T

Telecommunication Facility: means any telecommunication line, telephone exchange, telephone booth, telephone cabinet or pay phone, or any other structure, facility or apparatus intended for the purpose of effecting telecommunication.

Telecommunication Line: means a wire or wires or a conductor of any other kind (including a fibre optic cable) used or intended to be used for the transmission or reception of signs, signals, impulses, writing, images, sounds, or intelligence of any nature by means of any electromagnetic system; and includes any pole, insulator, casing fixture, tunnel, or other equipment or material used or intended to be used for supporting, enclosing, surrounding, or protecting any such wire or conductor and also includes any part of a line.

Temporary Accommodation: includes the use of any building to house any person for residential or business activities on a site, while construction work is being undertaken on the site.

Temporary accommodation maybe provided for persons occupying the site on which construction work occurs, or for persons involved in the construction work.

Temporary Activity: includes any activity which occurs on any site for a period of not more than 15 consecutive hours in any one time and occurs on no more than 12 times in any 12 month period; or any activity which does not last longer than a total of 7 consecutive days in any one time and occurs on not more than 3 times at any one site in any 12 month period. For the purposes of Rule 9.13.1 a temporary activity includes casual labour for seasonal activities such as crop harvesting and fruit picking, provided it complies with one of the time periods specified in the definition.

Temporary Military Training Activity: means a temporary activity undertaken for Defence Purposes. Defence purposes are those in accordance with the Defence Act 1990.

Temporary Sign: includes any sign erected to advertise an upcoming event of interest to the community, or any sign which advertises the products or services of a business or organisation which is associated with an activity on the site on which the sign is erected. Any temporary sign shall not be erected on any site for more than 6 consecutive calendar months at any one time, or for a collective period of more than 6 calendar months in any 2 year period, except that for any school or church anniversary event the temporary sign maybe displayed for up to 12 months prior to the event.

Tertiary Education: means the use of land and buildings for the purpose of facilitating tertiary education, training, development and instruction and/or related research and laboratories; and includes ancillary and accessory administrative, cultural, commercial, communal, conferencing, accommodation, retail and recreation facilities.

Township: means an urban area within the District which comprises a Living zone(s) and in some situations a Business zone(s).

Track: includes any area of land which is formed or used principally to traverse an area of land by foot, animals, or vehicles and includes any road. A track will have access to or from a formed road, or access to another track which has access to a formed road. For the purpose of Rule 1 - Earthworks, a track does not include multiple strips of land of up to 10m in width which run parallel to one another over a site.

Travelling Accommodation: means the use of land and buildings for temporary residential accommodation offered for a daily tariff which may involve the sale of liquor to in-house guests and the sale of food and liquor in conjunction with food to both the public and in-house guests. Travelling accommodation includes motels, holiday flats, motor and tourist lodges and hostels.

Tree: any woody perennial plant, typically with a distinct trunk (but sometimes multi-stemmed) from which branches arise well above ground level to form a crown, and includes other plants of a tree-like size and form such as palms.

U

Utility: includes the use of any structure, building or land for any of the following purposes:

- (a) The generation, transformation and/or transmission of energy;
- (b) Any telecommunication facility or telecommunication line;
- (c) Any radio communication facility;
- (d) The conveyance, storage, treatment or distribution of water for supply, including (but not limited to) irrigation and stockwater;

- (e) The drainage, reticulation or treatment of stormwater, waste water or sewage;
- (f) Transportation infrastructure, including (but not limited to) roads, accessways, railways, airports and navigational aids;
- (g) Work to mitigate potential natural hazards, including (but not limited to) stopbanks, groynes and gabions; or
- (h) Meteorological facilities for the observation, recording and communication of weather information.

Utility Building: includes any building or part of any building which is a utility or which is used principally to house or support a utility; and that building is $10m^2$ or more in gross floor area and 2.5m or greater in height.

Utility Structure: includes any device, equipment or other facility which is used principally to house or support a utility including any antenna, mast, pole or pylon; or any structure housing a utility which is less than 10m² in gross floor area or less than 2.5m in height.

V

Vehicle Accessway: means any area of land, the primary purpose of which is to provide access between the body of any allotment(s) or site(s) and any road reserve. Accessway includes any rights of way, access lot, access leg or private road.

Vehicle Crossing: means the area within the road reserve over which vehicles move from the carriageway to a site. The width of a vehicle crossing shall be defined as the formed width at the property boundary. The length of the crossing is the distance from the edge of the carriageway to the property boundary

Vehicle Movement: means a single motor vehicle journey to or from a particular site. "Vehicle trip" has the same meaning.

Vertebrate Toxic Agent: means any substance listed in Schedule 1 of the "Environmental Risk Management Authority Hazardous Substances (Vertebrate Toxic Agents) Transfer Notice 2004 (as amended)" and includes 1080 (Sodium fluoroacetate).

W

Wāhi Taonga Management Area: includes any land, building or structure which is listed in Appendix 5 and shown on the Planning Maps as a Wāhi Taonga Management Area.

Wāhi Taonga Site: includes any land, building or structure which is listed in Appendix 5 and shown on the Planning Maps as a Wāhi Taonga Site.

Walkway: See Pedestrian / Cycle Link

Waterbody: means fresh water or geothermal water in a river, lake, stream, pond (but excluding any artificial pond), wetland or aquifer or any part therefore that is not located within the coastal marine area. The terms "river", "lake" and "wetland" are also defined in this Plan.