



AMENDMENTS:

AMENDMENT	DATE	DESCRIPTION

NOTES:

**INTRODUCTION**


This Outline Development Plan (ODP) is for Area 4. This ODP provides an overarching urban design framework to guide future development of the land. It provides the principles and design intent that will underpin development of the masterplan. Development applications will need to demonstrate consistency with the final ODP that is formulated for the area.

The key structuring elements which underpin the ODP include the following:

- Road network: the interconnected road hierarchy of primary and secondary routes providing for vehicles, cyclists and pedestrians.
- Cycle and pedestrian network: the movement network for cyclists and pedestrians along road corridors and off-street linkages
- Stormwater network: the 'above ground' stormwater quality and management areas within existing ephemeral watercourses, and swales along road networks
- Open space network: reserves and parks offering a range of linkages and amenity for the site, as well as road corridors with significant tree avenues

Consistent with the Lincoln Structure Plan, the ODP supports the principles of strong connectivity to Birchs Road an integrated transport walking network, protection of valued drainage paths through the site.

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JOB TITLE:  
**B.H.L  
Lincoln**

SHEET TITLE:  
**Outline Development Plan  
Area 4**

DRAWING STATUS

SCALE: 1:2500@A1  
1:5000@A3

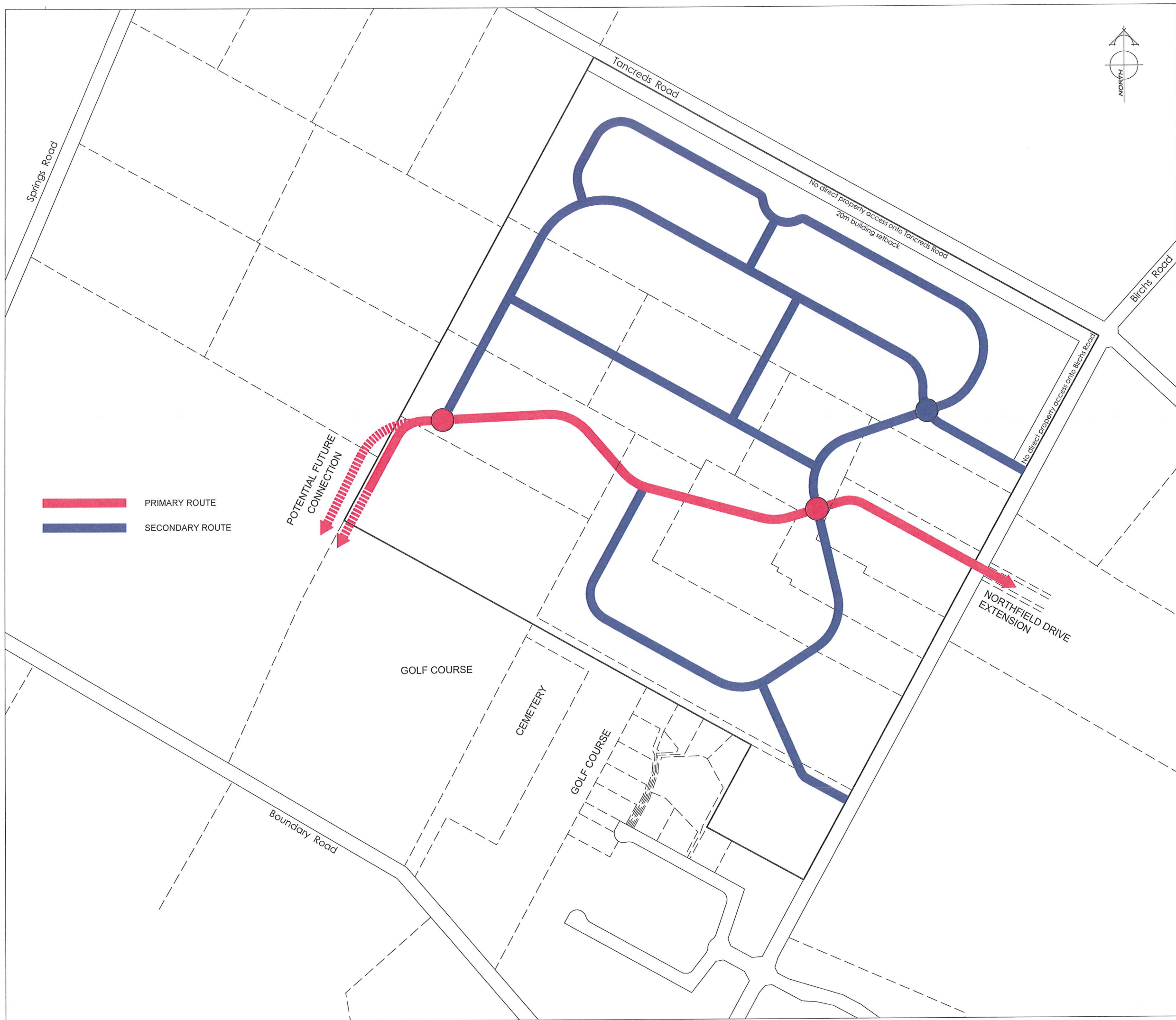
DATE: December 2009

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DRAWING No:  
**S.16509**

SHEET No:

REVISION:  
**R1**



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ROAD NETWORK

A legible hierarchy should be created for roads within Area 4, providing integrated connections to adjacent neighbourhoods and future development blocks. The primary routes should be consistent with the Lincoln Structure Plan and support strong connectivity to Birchs Road and Northfield Drive extension in Area 3. The road network should provide efficient and convenient connections between residential areas and destinations such as community facilities and neighbourhood centres as well as allowing for public transport. A logical choice of direct routes should be provided to disperse traffic volumes and reduce travel distances.

Provision of primary and secondary routes will enable safe pedestrian and cycle movements through the site, movement paths for stormwater, and green corridor connections that also provide areas of open space. Design strategies for these roads should be integrated while minimising conflict between vehicles, pedestrians and cyclists. The East - West orientated primary route will provide a collector road link across Area 4. It will cross and link with pedestrian, cycleway routes and areas of open space providing legible movement patterns that relate to natural site features and vistas. The connection to Boundary Road can be routed either through the Golf Course or the directly adjacent land, utilising existing tracks and/or unused parts of the Golf Course. All other local streets are not included in the ODP to ensure that the layout remains flexible and responds to built form.

There will be no direct property access onto Tancreds Road or Birchs Road. A portion of this frontage will also be subject to a 20m building setback as shown on this plan.

The length, location, geometry, width and overall design of these streets should reduce traffic speeds to allow for safe, pedestrian accessible environments. The patterns created by the streets and open space network should connect neighbourhoods, and in general this will mean that most roads will be through-routes, and any cul-de sacs should be limited in number and length.

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JOB TITLE:

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Outline Development Plan  
Lincoln Area 4  
Road Network Plan

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1:5000@A3

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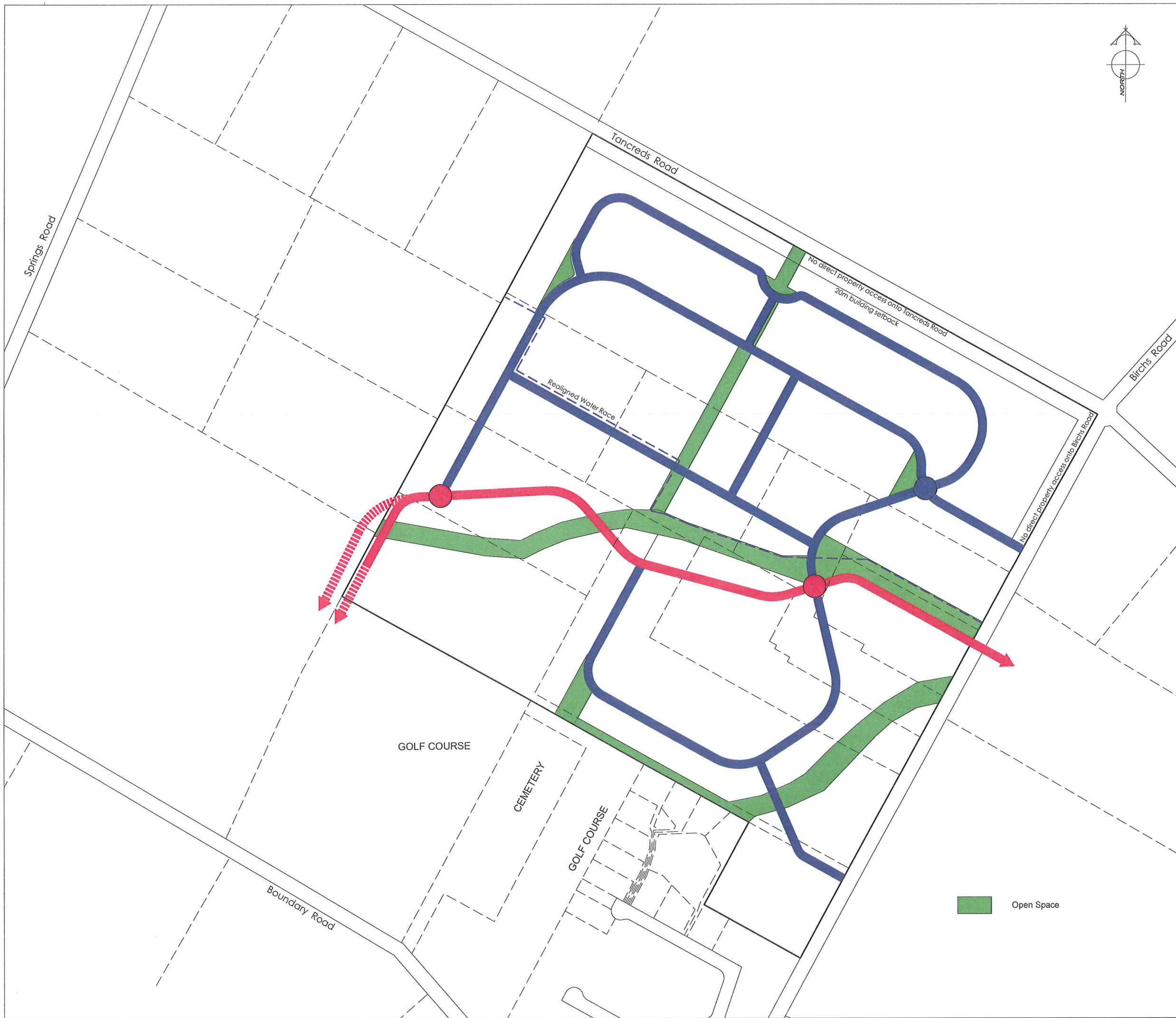
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**OPEN SPACE NETWORK**  
The existing natural drainage patterns underpin the connected system of accessible open space. Utilising and enhancing the natural features of the site will contribute to overall character and sense of place, offer more varied recreational experiences, and create a focal point for the community.  
Neighbourhood reserves will be developed as part of the overall subdivision design. These will integrate with drainage channels and natural features to provide for a high level of connectivity, recreational value and neighbourhood character. This will include the naturalisation of the water race which will be incorporated into the reserves and road network. The approximate location of the proposed water race is shown here.  
For the site as a whole, open space areas (including the stormwater reserves) should be accessible from primary and secondary routes within the site, and within a 400m walking distance of all new dwellings. In general, open space areas bounded by roads are more secure because of informal surveillance from passers-by and overlooking from windows and outdoor areas of nearby houses. Reserves that are clearly visible are likely to attract more users and be more valued by the community.  
The design of some streets will contribute to the overall character and connectedness of the public network of open space. Wide berms with footpaths and avenues of trees can create a boulevard effect to assist with navigation to open space areas. Vistas and key road junctions that terminate with open spaces and landscape features provide opportunities for good user legibility within the site. The junctions between primary routes should also be considered to ensure that long vistas terminate with landmark features such as trees within a landscape setback, and/or landmark buildings.  
Appropriate building setbacks should be provided along Tancreds Road and part of Birchs Road.  
Connection is also to be made to Golf Course and Cemetery.

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SHEET TITLE:

**Outline Development Plan  
Lincoln Area 4  
Open Space Network Plan**

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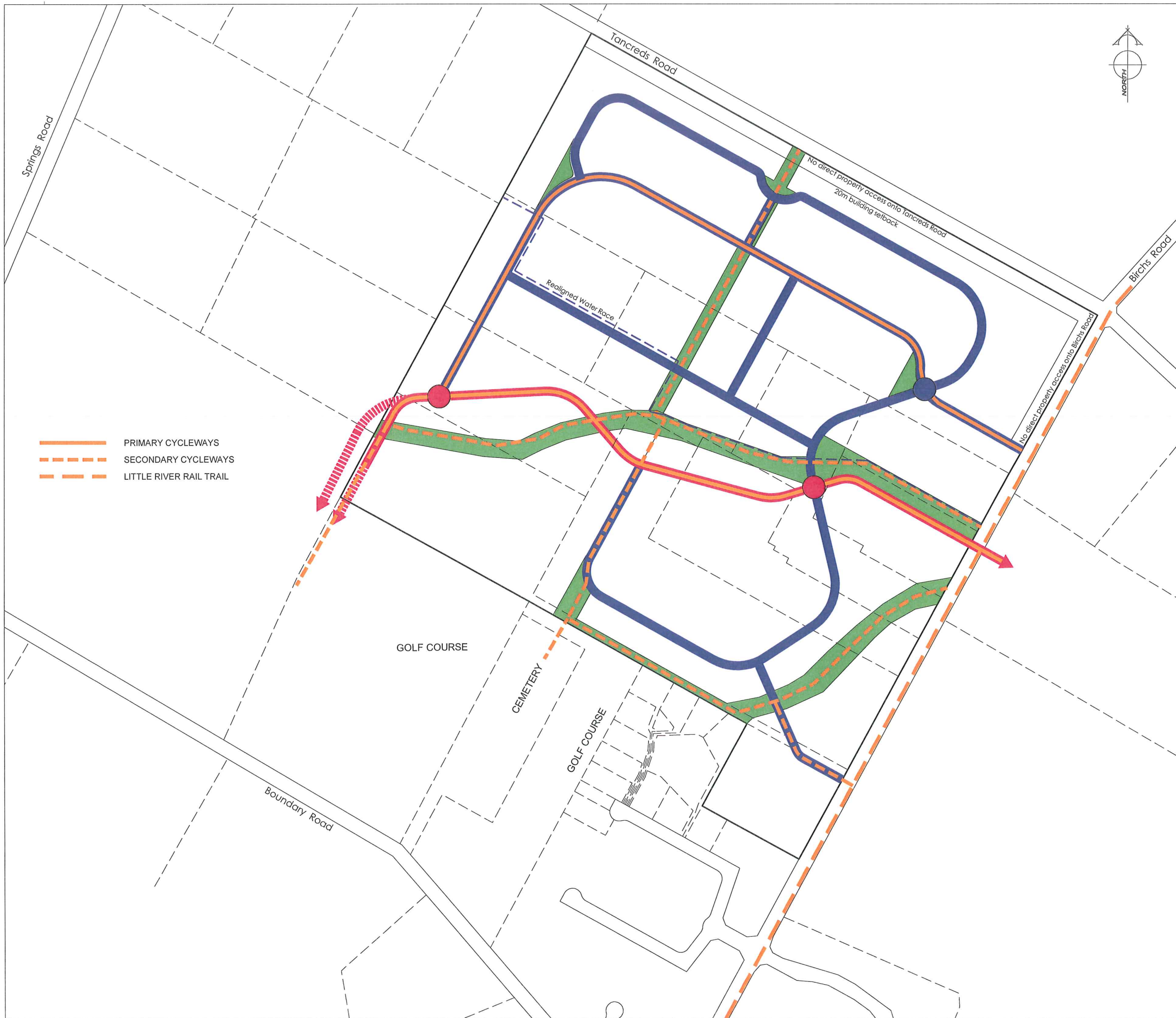
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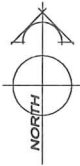
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**R1**





- PRIMARY CYCLEWAYS
- SECONDARY CYCLEWAYS
- LITTLE RIVER RAIL TRAIL



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**CYCLE AND PEDESTRIAN NETWORK**

The cycle and pedestrian network has a relationship with the underlying road pattern and open space network. Safe and convenient access for pedestrians and cyclists should be provided for all routes, including footpaths and in some cases cycle lanes. Footpaths should be provided on all roads, and widths may vary depending on the road hierarchy and anticipated use. Barrier free access should be provided along and across all roads for all users including those with mobility restrictions. Dedicated on-street cycle lanes should be provided along the primary route. All other streets should be slow speed, low traffic volume environments which are pedestrian and cycle friendly, and create liveable public spaces for the community. In some cases these may be shared surfaces.

Roads edging open spaces should provide safe and direct alternative routes for night use that complement open space movement patterns. These roads also provide passive surveillance and multiple entry and exit points to open space areas. These roads should adequately provide for walking and cycling and have good lighting. The street pattern should provide direct and convenient access so that a maximum 400m walking distance is provided between all dwellings and open space areas within the site. The open space corridor with direct connections to the Little River Rail Trail on Birchs Road is proposed in accordance with the Lincoln Structure Plan.

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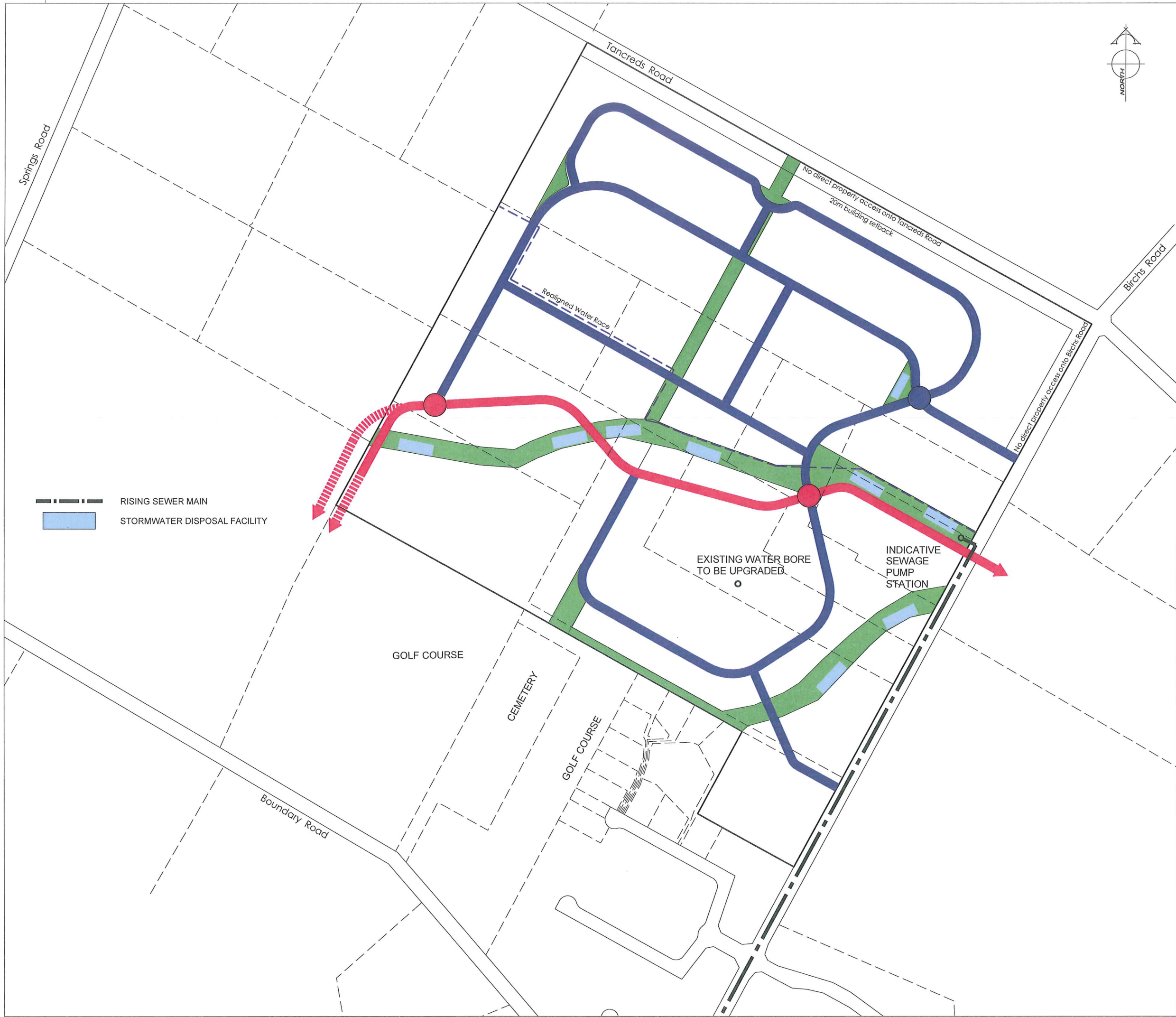
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**Outline Development Plan  
Lincoln Area 4  
Cycle & Pedestrian Network Plan**

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**SEWER NETWORK**  
A single sewer pump will be constructed on the site to service the whole Outline Development Area. A rising main will be constructed from the pump station to the Lincoln Sewage Facility.

**STORMWATER NETWORK**  
The stormwater network incorporates existing natural drainage patterns that meander east-west through the site, reserves and streets with above- ground stormwater management devices such as swales. Any requirements for stormwater detention and attenuation will generally be accommodated within the proposed development area. Stormwater reserves should also be designed and used for recreational purposes such as walking and cycling, and integrated with the open space network. Along with riparian management techniques, these reserves can create valuable ecological corridors and habitats, as well as significant amenity and localized character for the neighbourhood.

**WATER SUPPLY NETWORK**  
Additional water supplies will be obtained from the installation of new bores. These bores will be located to suit the required pressures and flows. all water mains will follow the road network or pedestrian routes. There is an existing bore on the site that could be upgraded to potable standard. The network would be connected into the existing Lincoln reticulation.

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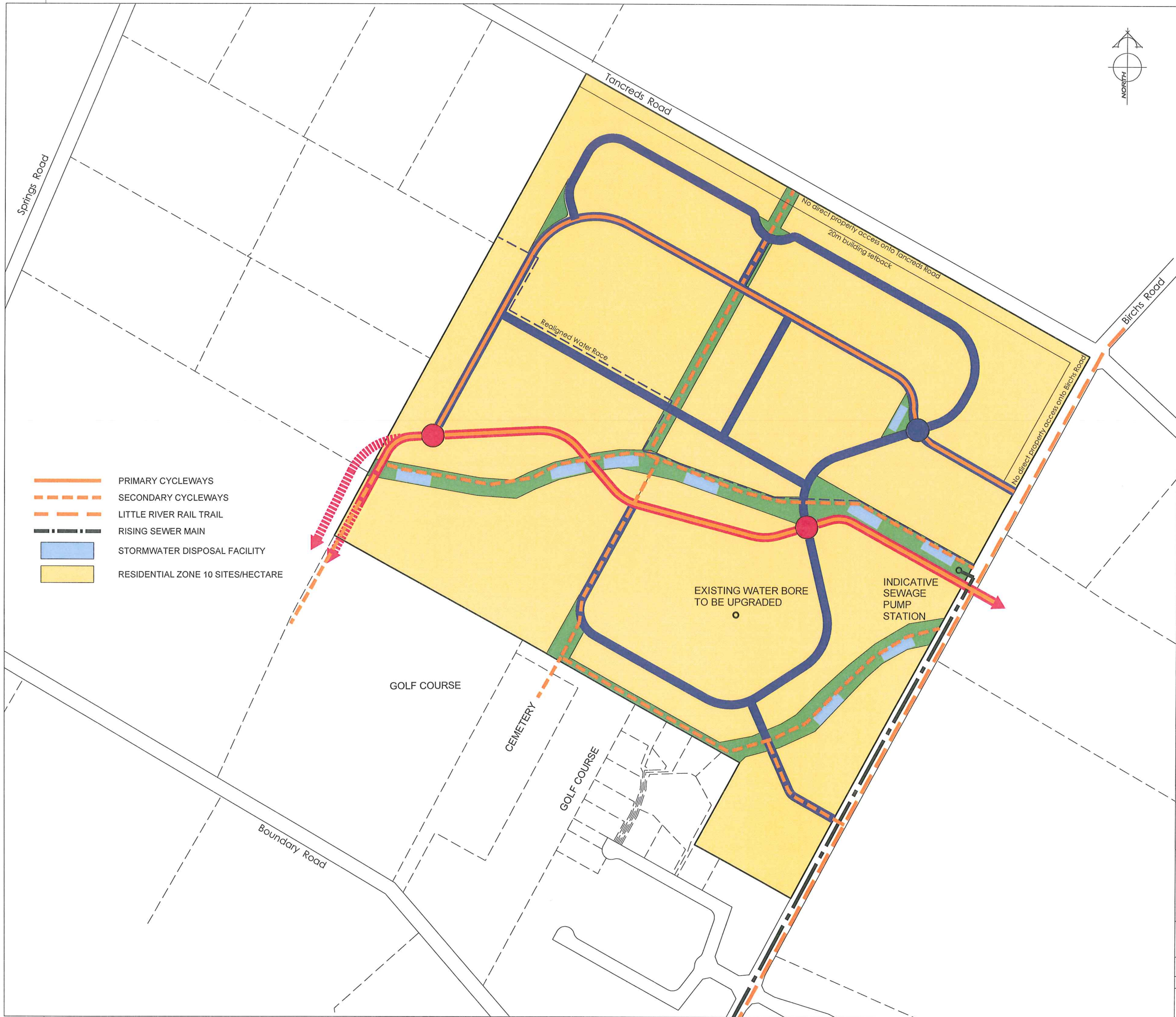
**Outline Development Plan  
Lincoln Area 4  
Sewer, Stormwater & Water  
Network Plan**

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- PRIMARY CYCLEWAYS
- SECONDARY CYCLEWAYS
- LITTLE RIVER RAIL TRAIL
- RISING SEWER MAIN
- STORMWATER DISPOSAL FACILITY
- RESIDENTIAL ZONE 10 SITES/HECTARE


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**DEVELOPMENT PATTERN**

The Area 4 Outline Development Plan area will be comprised entirely of conventional residential development in accordance with the Lincoln structure plan. Over the Outline Development Plan area a minimum net density of 10 households per hectare is to be achieved. The Northern extent of the Outline Development Plan area adjoining Tancred's Road, will have a 20m Building Setback requirement. The setback will provide a buffer between the residential development and adjoining rural area. This Setback will be extended onto Birchs Road as far as the first entrance into the area.

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Lincoln Area 4  
Development Pattern Plan**

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